



ADAMS COUNTY

COLORADO
BOARD OF COUNTY COMMISSIONERS

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STUDY SESSION AGENDA
TUESDAY
March 10, 2020

ALL TIMES LISTED ON THIS AGENDA ARE SUBJECT TO CHANGE

- | | | |
|-------------------|---------------------|--|
| 10:30 A.M. | ATTENDEE(S): | Adam Burg / Eliza Schultz / Elisabeth Rosen |
| | ITEM: | Legislative Update |
| 11:15 A.M. | ATTENDEE(S): | Kristin Sullivan / Jill Jennings Golich / Matt Rivera /
Brian Staley / Andrew Vidor |
| | ITEM: | Adams County Parking Study |
| 12:15 P.M. | ATTENDEE(S): | Jill Jennings Golich / Katie Keefe / Greg Dean / Ryan
Nalty / Keith Huck / Chane Finkenbinder |
| | ITEM: | Oil and Gas Update |
| 12:45 P.M. | ATTENDEE(S): | Raymond Gonzales |
| | ITEM: | Administrative Item Review / Commissioners
Communication |

(AND SUCH OTHER MATTERS OF PUBLIC BUSINESS WHICH MAY ARISE)

AGENDA IS SUBJECT TO CHANGE



STUDY SESSION AGENDA ITEM

DATE: March 10, 2020
SUBJECT: Adams County Parking Study
FROM: Kristin Sullivan, Director of Public Works
AGENCY/DEPARTMENT: Public Works
ATTENDEES: Kristin Sullivan, Jill Jennings Golich, Matt Rivera, Brian Staley, and Andrew Vidor
PURPOSE OF ITEM: Provide a summary of the results of the Adams County Parking Study completed in 2019
STAFF RECOMMENDATION: Informational session

BACKGROUND:

In this study session we will present the results of the Adams County Parking Study completed in 2019. This study was commissioned to review existing parking conditions for selected areas of public parking within 15 neighborhoods that make up the southwest portion of Adams County. The study neighborhoods include:

- | | |
|-------------------|------------------------|
| Arlington Meadows | Midtown at Clear Creek |
| Aloha Beach | North Pecos |
| Berkeley | Perl Mack |
| Fairview | Shaw Heights |
| Florado | Sherrelwood |
| Goat Hill | Utah Junction |
| Lakeshore Estates | Welby |
| Mapleton | |

The resulting report contains an analysis of existing Adams County parking policies, along with options on how to improve or streamline those policies. The report also formulates recommendations for parking management strategy options and the technology that can be used to implement those strategies.

AGENCIES, DEPARTMENTS OR OTHER OFFICES INVOLVED:

Community Safety & Wellbeing
 Community and Economic Development
 Public Works

ATTACHED DOCUMENTS:

PowerPoint Presentation
 Adams County Parking Study

FISCAL IMPACT:

Please check if there is no fiscal impact . If there is fiscal impact, please fully complete the section below.

Fund:

Cost Center:

	Object Account	Subledger	Amount
Current Budgeted Revenue:			
Additional Revenue not included in Current Budget:			
Total Revenues:			

	Object Account	Subledger	Amount
Current Budgeted Operating Expenditure:			
Add'l Operating Expenditure not included in Current Budget:			
Current Budgeted Capital Expenditure:			
Add'l Capital Expenditure not included in Current Budget:			
Total Expenditures:			

New FTEs requested: YES NO

Future Amendment Needed: YES NO

Additional Note:

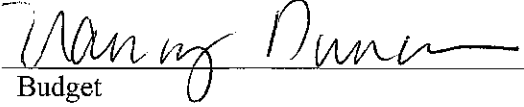
APPROVAL SIGNATURES:

Raymond H. Gonzales, County Manager



Deputy County Manager

APPROVAL OF FISCAL IMPACT:



Budget

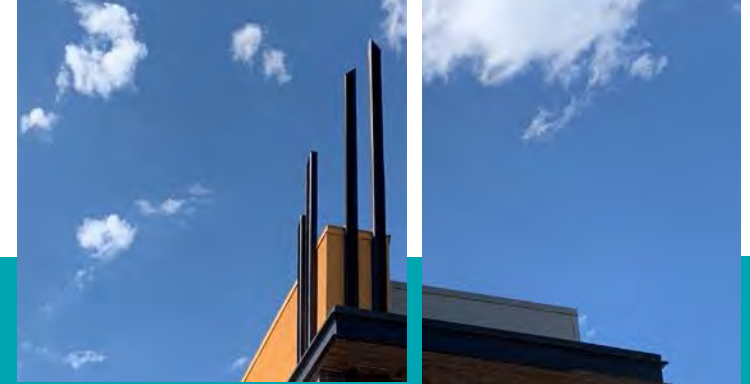


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ADAMS COUNTY SOUTHWEST COUNTY PARKING STUDY

County Commission Executive Briefing
March 10, 2020



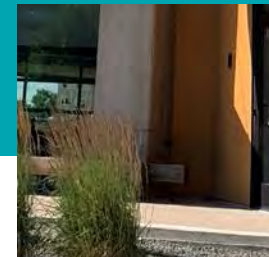
BUILDING ENVELOPE

CONSULTING

FORENSIC RESTORATION

PARKING DESIGN

PLANNING



Introduction
Purpose
Goals

Existing Conditions

Findings

Policy Review

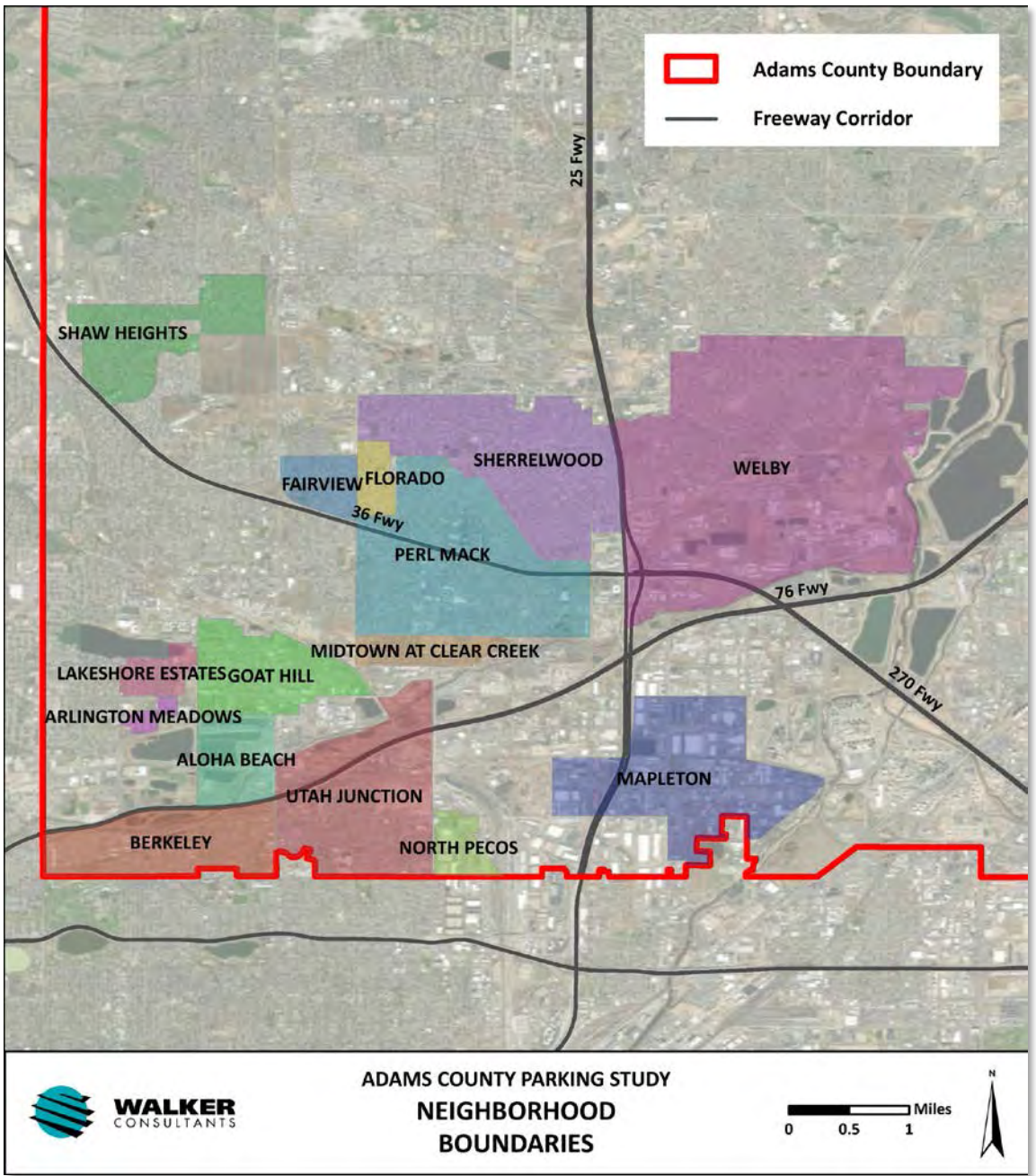
Technology &
Parking
Management
Strategies

INTRODUCTION

PURPOSE AND GOALS

- To provide a high-level overview of public on- and off-street parking demand in each neighborhood
- To utilize data collected to address the following areas of concern and identify when and where they occur:
 - Inconsistency and obsolescence of existing parking restrictions
 - Lack of enforcement & long-term parking of derelict vehicles
 - Spillover parking





Neighborhoods studied:

1. Aloha Beach
2. Arlington Meadows
3. Berkeley
4. Fairview
5. Florado
6. Goat Hill
7. Lakeshore Estates
8. Mapleton
9. Midtown at Clear Creek
10. North Pecos
11. Perl Mack
12. Shaw Heights
13. Sherrelwood
14. Utah Junction
15. Welby

INTRODUCTION

METHODOLOGY

- Data collected in 150 blocks of public on-street and 5 off-street public parking lots.
- Key corridors within each neighborhood were strategically chosen for study with an eye towards the purpose and goals of this study.
- Inventory, occupancy, and length of stay data was collected over the course of three weeks between mid-August and early-September. Weekday and weekend counts.
- Dates and times were chosen in order to maximize the likelihood of capturing peak parking demand in each neighborhood.



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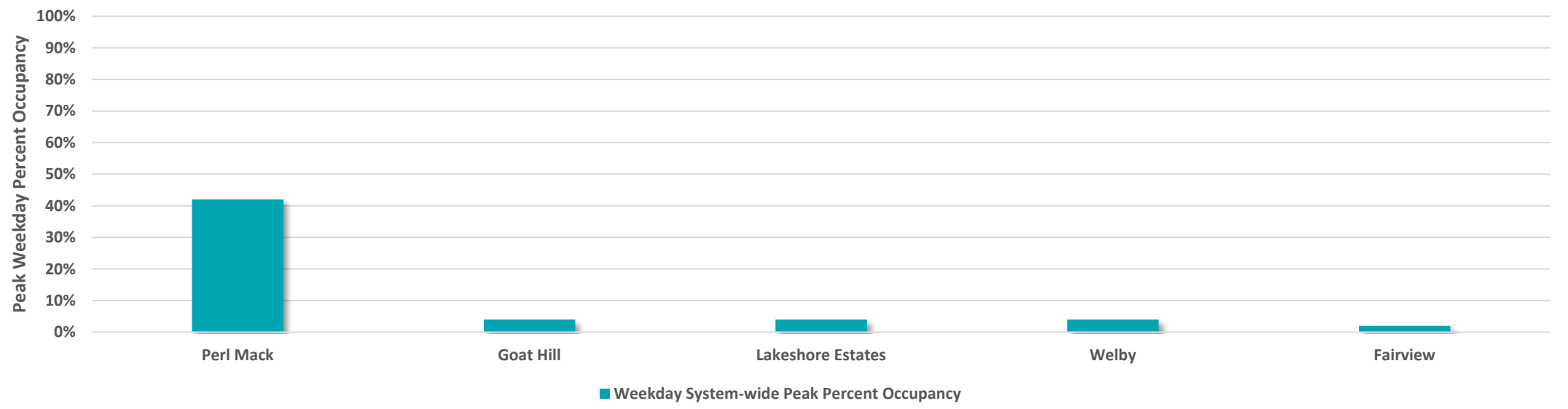
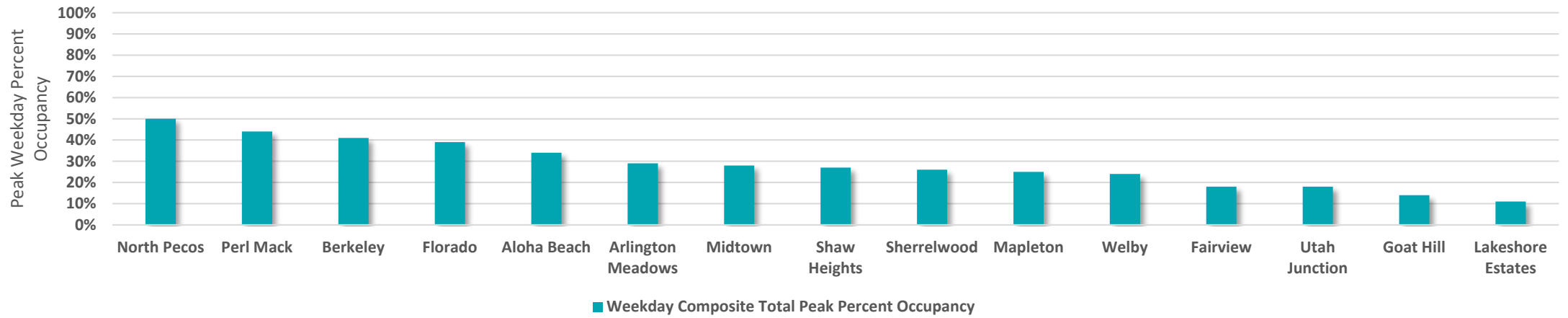
Policy Review

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EXISTING CONDITIONS INVENTORY & OCCUPANCY

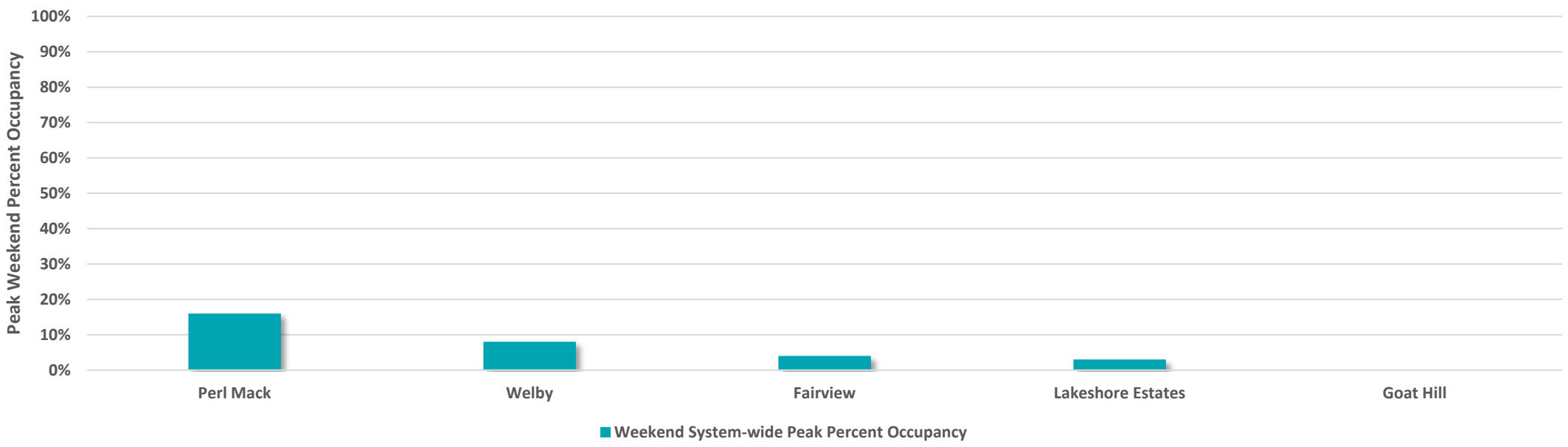
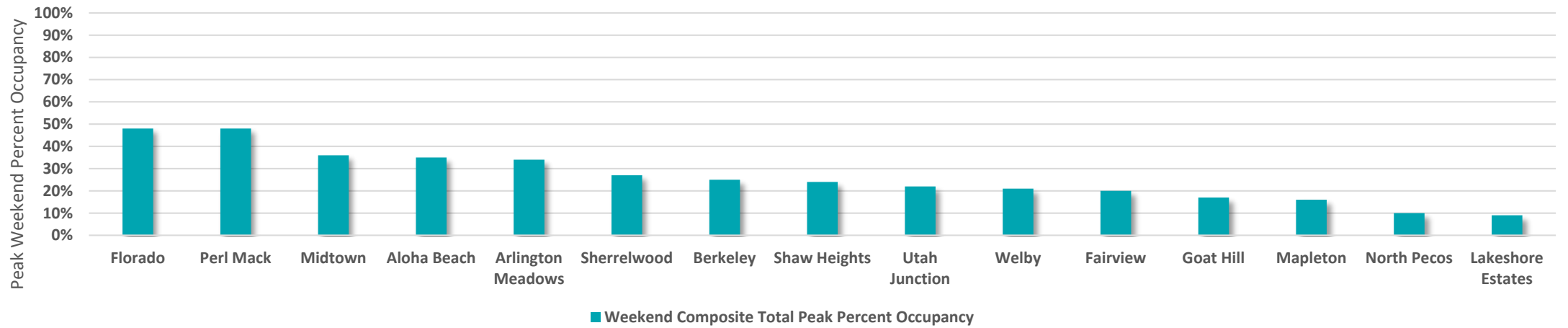
Weekdays





EXISTING CONDITIONS INVENTORY & OCCUPANCY

Weekends

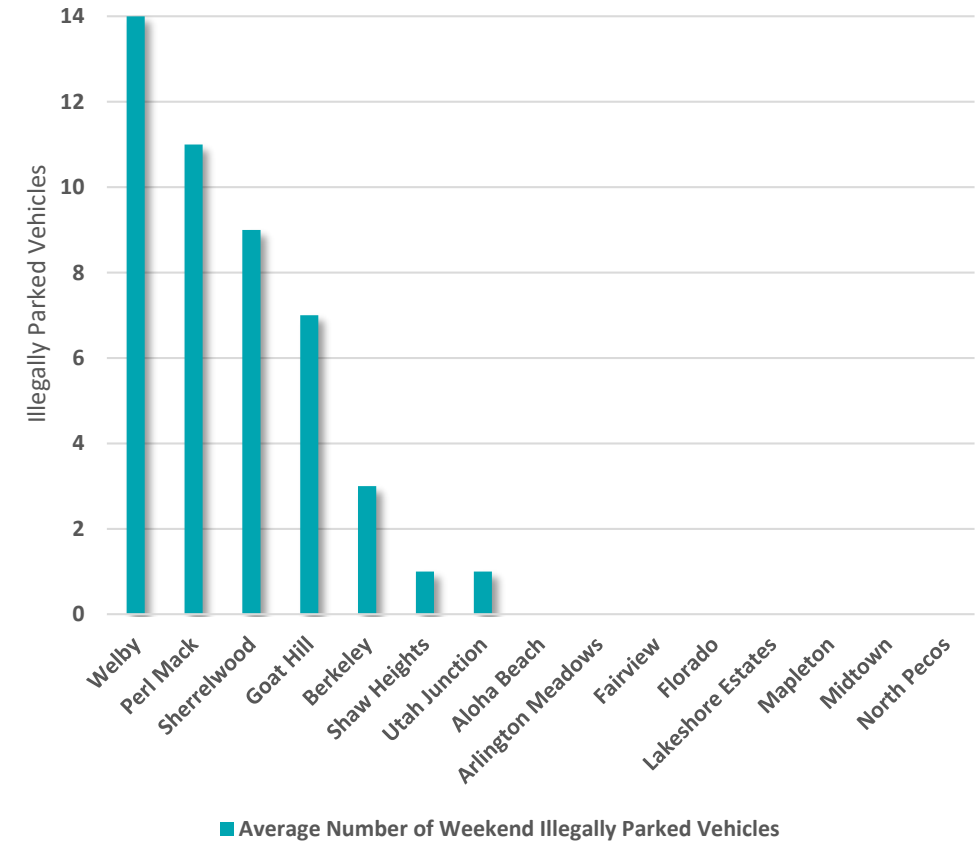
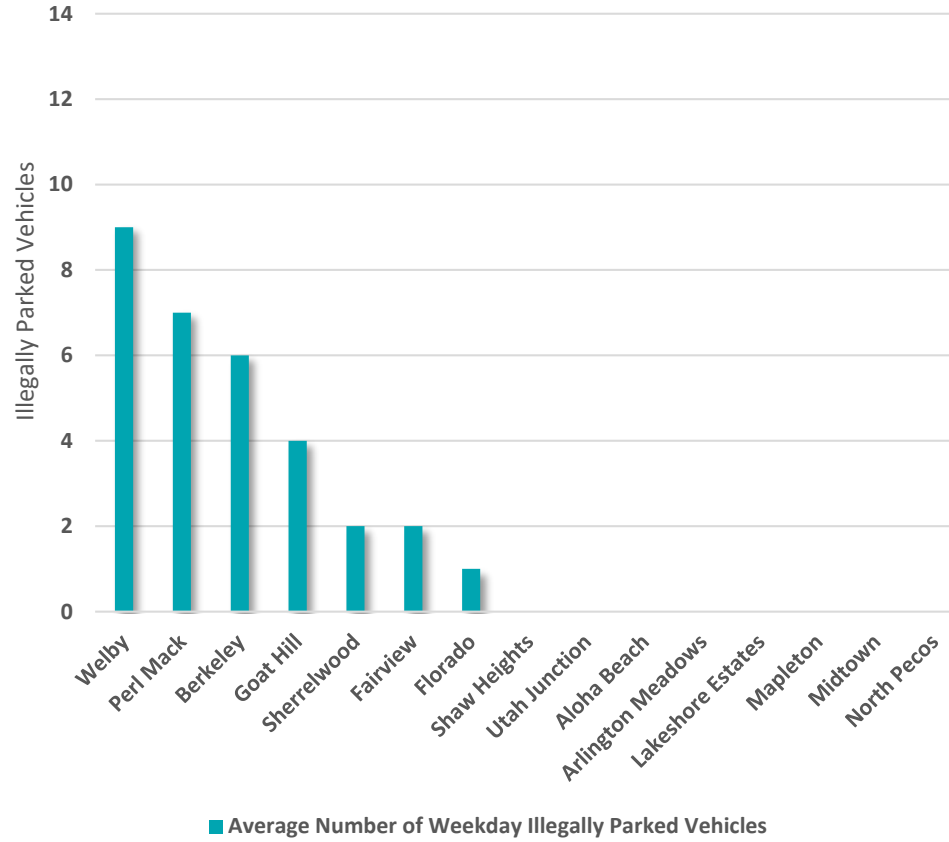


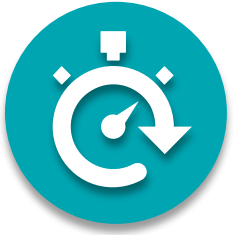


Weekdays

Weekends

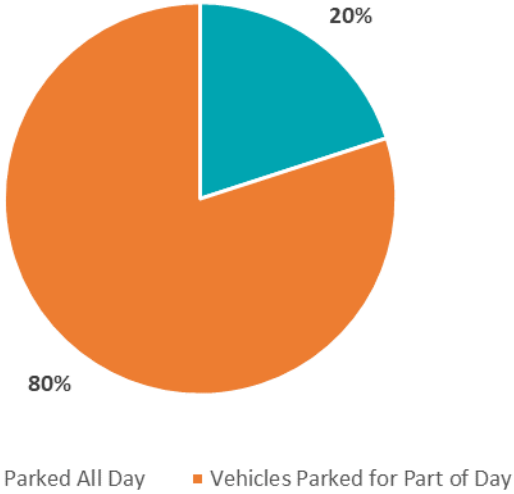
EXISTING CONDITIONS ILLEGALLY PARKED VEHICLES



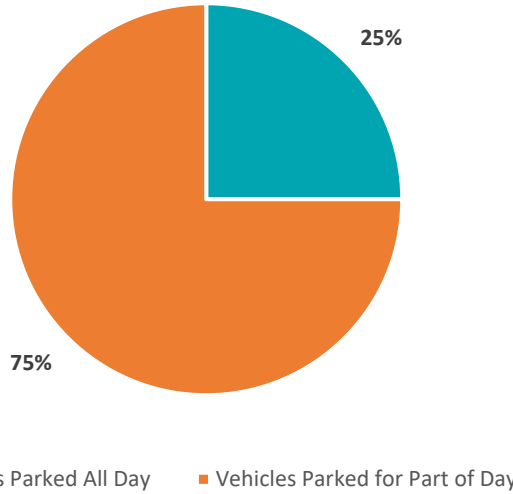


EXISTING CONDITIONS
LENGTH OF STAY

Weekdays



Weekends



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FINDINGS

ALL NEIGHBORHOODS

- Almost all on-street parking (where parking is allowed) is unrestricted.
- The following neighborhoods contained areas or corridors where a pattern of illegal parking was observed (defined as consistently having two or more illegally parked vehicles at most times):
 - Welby
 - Perl Mack
 - Berkeley
 - Goat Hill
 - Sherrelwood
 - Fairview

FINDINGS

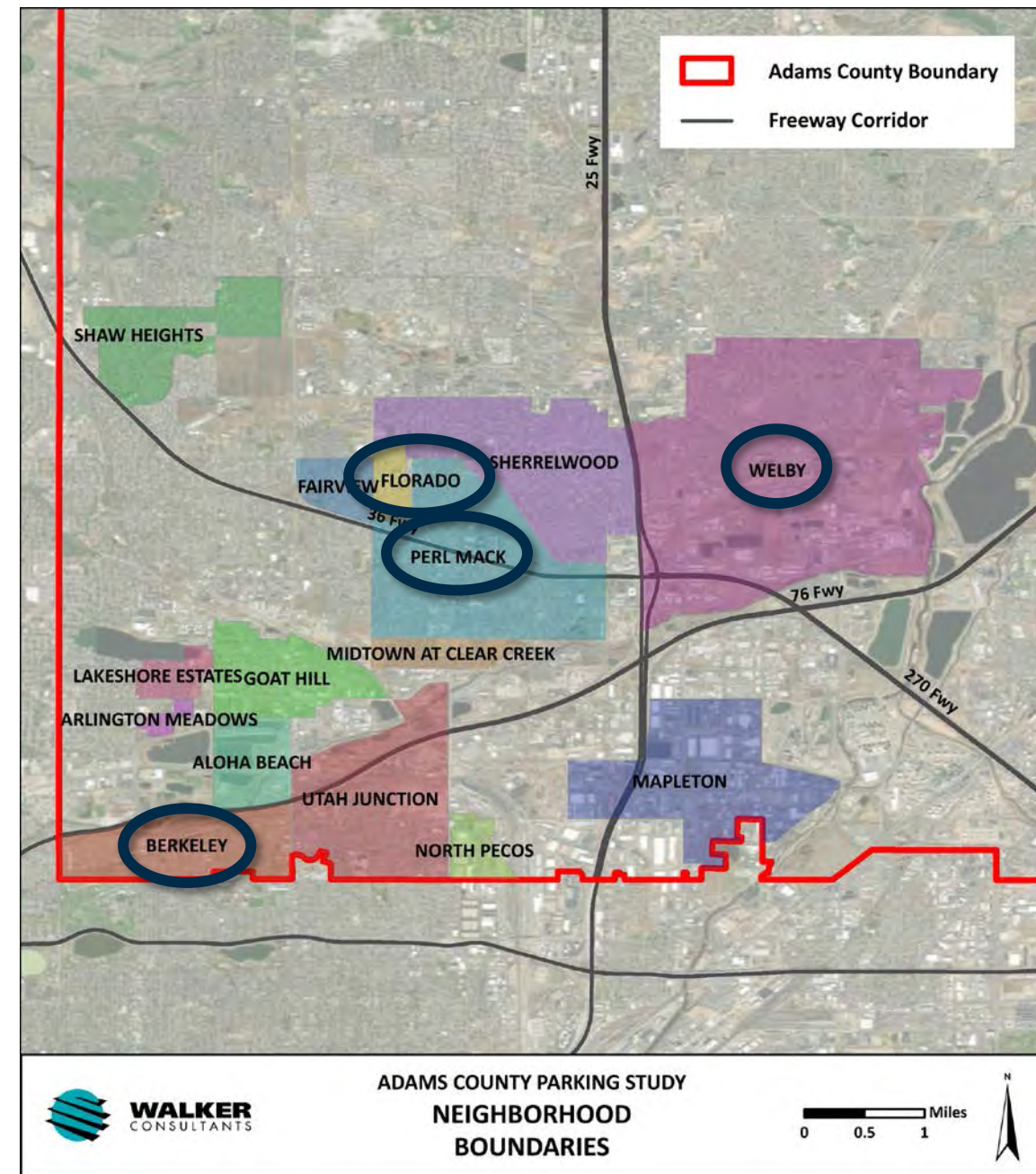
ALL NEIGHBORHOODS

- All neighborhoods studied have adequate on- and off-street parking supply to support the current parking demand.
- The following neighborhoods were ***not*** observed to have any overarching issues:
 - Aloha Beach
 - Arlington Meadows
 - Fairview
 - Goat Hill
 - Lakeshore Estates
 - Mapleton
 - Midtown at Clear Creek
 - North Pecos
 - Shaw Heights
 - Sherrelwood
 - Utah Junction

FINDINGS

ALL NEIGHBORHOODS

- The following neighborhoods may have localized areas with spillover parking and high on-street parking demand. These neighborhoods may be the first candidates for active parking management:
 - Berkeley
 - Florado
 - Perl Mack
 - Welby



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POLICY AND ORDINANCE REVIEW

Policy and ordinance review aims to enhance current ordinances to address concerns found from our analysis.

- Vehicle storage in right-of-way: additional language to further define limits and distances.
- Parking supply reductions and adjustments: intends to simplify parking reductions, create additional adjustment opportunities, and provide flexibility to consider future technology impacts.
- Reduced need populations: additional language suggested to right size parking supplies in projects serving low parking demand populations.

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TECHNOLOGY & OPERATIONS STRATEGIES

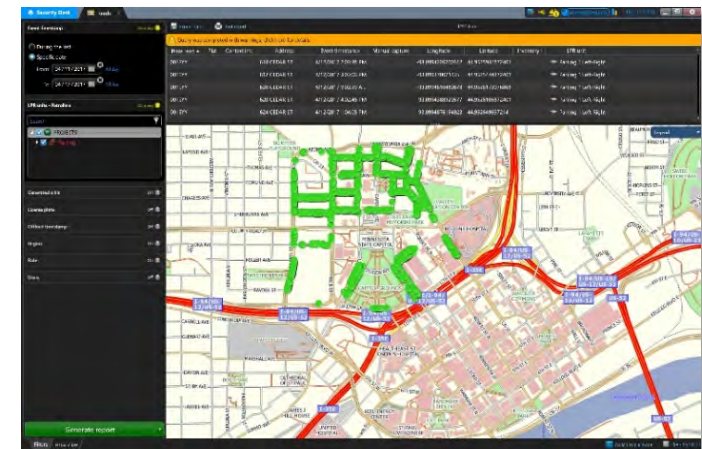
PARKING MANAGEMENT

- Active parking management addresses the identified areas of concern:
 - Illegal parking
 - Spillover parking
 - Overtime parking
- Active parking management promotes equitable use of public curb space.
- Active parking management requires new resources (\$10K-\$40K annually)
 - Department of Community Safety and Well-Being
 - Contracted through third-party parking management firm
- Requires establishment of residential permit programs (RPP) in specific areas as not to impact residents from adjacent spill over from businesses/institutions.

TECHNOLOGY & OPERATIONS STRATEGIES

LICENSE PLATE READER (LPR) TECHNOLOGY

- LPR technology is recommended (\$20K-\$50K)
- Benefits
 - Scalable, portable, and technologically advanced
 - Recent judicial actions may prohibit tire chalking
 - Minimal staff resources required
 - Data stored dynamically in the cloud
 - Advanced data analytics and technical support
 - Opportunity for synergy with law enforcement



MOVING FORWARD WITH ACTIVE PARKING MANAGEMENT

COST VERSUS BENEFIT: THE UNDERLYING CONCERNS

- Who, and how many, have voiced concerns
- What is the nature of concerns?
- Do County officials believe a problem exists?
- Do residents and business owners concur?
- Are concerns localized or dispersed across neighborhoods?
- Is lack of adequate on-site parking supply the problem?
- What is the qualitative severity versus the quantitative severity of concerns as shown in the data?
- What level of investment is in the strategic interest of all parties?



NEXT STEPS

Recommendations

- Active Parking Management
- Add 2 FTE's to Code Compliance to enforce parking regulations (2021)
- Public Education and Communication

Action Items

- Develop Parking Ordinances
- Job Description specific to the new positions
- Working with Public Works to evaluate current parking signage & locations

Future

- Parking Permit Zones
- Utilization of Technology (LPR)



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October 31st, 2019

Mr. Brian Staley
Deputy Director
Public Works
Adams County, Colorado
4430 S. Adams County Parkway, W2000B
Brighton, CO 80601

Re: Southwest Adams County Parking Study

Dear Mr. Staley,

Walker Consultants is pleased to submit this report outlining existing parking conditions for selected areas of public parking within the 15 neighborhoods that make up the southwest portion of the county. Also contained in this report is an analysis of existing parking policies, along with options on how to improve or streamline those policies. Finally, this report formulates recommendations for parking management strategy options and the technology that can be used to implement those strategies.

The ensuing analysis will provide county planning and public works staff with a sampling/overview of parking for these neighborhoods and help to inform decision making moving forward with regards to implementing parking management strategies in hot spot areas or along key corridors where conflicts or problems have been identified.

Sincerely,

WALKER CONSULTANTS

A handwritten signature in blue ink that reads "Andrew J. Vidor".

Andrew Vidor
Director of Planning

A handwritten signature in blue ink that reads "Drew Willsey".

Drew Willsey
Analyst

A handwritten signature in blue ink that reads "Christina Jones".

Christina Jones
Analyst



Southwest County Parking Study

Adams County, Colorado

October 31, 2019 - **DRAFT**

Prepared for:

Adams County Government – Department of Public Works

Project #: 23-7932



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CONSULTANTS

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EXECUTIVE SUMMARY

EXISTING CONDITIONS

Parking occupancy across all 15 neighborhoods reached an average of 25% peak occupancy during weekdays and 22% peak occupancy during weekends.

1. **Parking restrictions**

Time-limited parking was found in one neighborhood, Fairview. On-street ADA parking was found in Florado and Midtown. Off-street ADA parking was found in Fairview, Goat Hill, Lakeshore Estates, Perl Mack, and Welby. Note that this only represents restricted parking found within the corridors or lots identified for study in each neighborhood.

2. **Long-term and illegal parking**

Illegal parking activity was observed in seven neighborhoods. In order of prevalence, these are: Welby, Perl Mack, Berkeley, Goat Hill, Sherrelwood, Fairview, and Florado. Illegal parking was mostly in the form of parking along corridors where no parking is allowed at any time.

Across all neighborhoods, the average percentage of vehicles parked all day was 20% on weekdays and 25% on weekends. All-day parking during weekdays was highest, in terms of percent of total parkers, in Florado, Welby, Perl Mack, Mapleton, and Aloha Beach. All-day parking during weekdays was highest, in terms of percent of total parkers, in Shaw Heights, Sherrelwood, Perl Mack, Fairview, and Berkeley. Mapleton experienced the largest negative difference between weekday and weekend all-day parkers, and Shaw Heights experienced the largest positive difference.

3. **Spillover parking**

Though there were many individual block faces that experienced high parking demand during peak times in most neighborhoods, most of that high demand did not constitute spillover parking from nodes of parking demand other than immediate adjacent land uses. However, based on comparing composite peak occupancy data to system-wide peaks for each neighborhood, and analyzing surrounding land uses, spillover parking demand was likely occurring in Berkeley, Perl Mack, Welby, and potentially Florado.

The problem was most prevalent in Perl Mack, with multiple block faces along 76th Avenue, Samuel Drive, Dakin Street, Del Norte Street, and Bronco Road affected with high demand at peak hours on both the weekday and weekend. Prevalent weekday spillover parking in Berkeley was also observed, with the block faces closest to the Regis University campus (segments of King Court, 53rd Avenue, Irving Street, Meade Street, and Lowell Boulevard) affected. In Welby, block faces along Coronado Parkway near the intersection with Explorador Calle, Downing Drive, and Sheldon Drive were affected.

Spillover parking demand likely related to church activity may have occurred along block faces near church front entrances during Sunday mornings and afternoons in Welby and Florado, though this

parking activity may be related to the relative convenience of on-street parking in front of those churches, as opposed to actual spillover demand from mostly full church parking lots.

PARKING POLICY

Walker identified some areas of opportunity to streamline and improve parking policy as laid out in the county's Standards and Regulations. These opportunities include:

- Extending time limitations to apply to all vehicles, regardless of weight, habitation status, or whether the vehicle is derelict.
- Extending the maximum distance that off-site parking can be located from the site served by that parking.
- Removing the requirement for a residential component in shared parking agreements.
- Allowing reductions in the overall sum total parking requirement for component land uses within a shared parking agreement, regardless of whether the agreement falls within a TOD.
- Allowing more than two land uses to be included within a shared parking agreement.
- Allowing automatic parking requirement reductions for affordable housing.

TECHNOLOGY & OPERATIONS STRATEGIES

Walker identified four neighborhoods where both habitual illegal parking and possible spillover parking demand may be occurring: Berkeley, Perl Mack, Welby, and Florado. Of these neighborhoods, only Berkeley and Perl Mack were identified as being good candidates for a parking management program.

In Berkeley, Walker recommends that the county consider the following parking management options:

- Passive parking management, where "RESIDENT PARKING ONLY" signs are posted with no enforcement.
- Active parking management with enforcement provided by the county.
- Active parking management with enforcement provided by contract, possibly with Regis University.

For the latter two options, a resident parking permit program would have to be implemented within the area to be managed, which would encompass parking that falls within a certain radius of the university.

In Perl Mack, Walker identified two areas that are candidates for parking permit zones that would limit parking to residents only, due to potential spillover parking from certain multi-family complexes in the neighborhood. However, the exact nature and extent of spillover parking in these areas will need further analysis to determine its extent.

If a clear parking supply shortage exists in the off-street parking at these apartment complexes, Walker recommends the county consider facilitating a shared parking agreement with adjacent commercial/office land uses in two of the problem areas identified, in conjunction with or as an alternate standalone option to permit zones.

In the other two neighborhoods, Walker recommends potentially employing parking management in a limited fashion in order to alleviate frequent and habitual illegal parking activity.

If the county wishes to implement active parking management in any capacity, where enforcement operations would be conducted by the county, Walker recommends that the county consider procuring license plate reader (LPR) technology to be used by parking enforcement officers employed under the county's Department of Community Safety and Well-Being.

Such technology provides a scalable and technologically advanced method for enforcement of parking management restrictions, and is especially desirable in the wake of recent judicial action that prohibits tire chalking in some jurisdictions. Also, costs associated with LPR have decreased within the last five years, and such technology can be employed in a portable fashion with minimal staff resources required. Data collected by LPR technology is stored dynamically in the cloud, and data analytics and technical support are included with modern LPR software platforms.

Finally, data collected by Parking Enforcement Officers (PEOs) using LPR technology can be valuable for law enforcement purposes as well as parking management purposes. There is a natural opportunity for synergy between the Sheriff's Department and parking enforcement that can be leveraged to the mutual benefit of both entities.


COST VERSUS BENEFIT

Under any of the parking management options described in this report, including the option of maintain the current status quo of no active or passive parking management in any of the neighborhoods studied, the county must consider the relative costs and benefits. Even though data contained within this report outlines some specific areas of interest in some of the neighborhoods, further study would be required to determine the exact extent to which parking issues exist. A large part of that exercise would involve determining which constituencies, and how many people within those constituencies, have voiced concerns, as well as the nature and scope of those concerns. Does the county believe a problem exists? Or do residents and businesses believe a problem exists? Or is there wide and established consensus amongst all stakeholders?

For instance, county staff or leadership may be under the impression that on-street parking overflow is a significant problem in Berkeley based on the complaints of a small number of citizens who may or may not live within the neighborhood or within the identified area of interest. Or, complaints about lack of parking supply coming from the Perl Mack neighborhood may be primarily coming from residents of the apartment complexes within the areas of interest who are frustrated with having been crowded out of off-street private parking, and not necessarily from owners of nearby single-family homes who feel that apartment parking overflow demand is actually spilling over onto their street. Cost/benefit calculations for proposed parking management options will differ accordingly.

Also, the qualitative severity of a parking problem may vary from the quantitative severity as borne out in the data. It may or may not be in the strategic and best interest of decision makers to implement parking management solutions to problems that are at least partially perceived.

In any case, when considering both planned and proposed opportunities for higher-density, mixed-use and transit-orientated development in the approval pipeline, it is in the interest of the county to have the framework for parking management strategies and options in place, to be implemented as needed.



01 Introduction &
Background

SECTION 1 – INTRODUCTION & BACKGROUND

Adams County, Colorado is located immediately to the north of the City and County of Denver and encompasses much of Denver’s northern metropolitan area. The county is one of the most populated counties in the state, with a population recently surpassing half a million. The county’s approximately 1,200 square miles stretches out over a narrow, rectangular area that is generally composed of rural and agricultural land. The westernmost quarter of the county comprises the majority of the populated and urban areas within the county.

The area of Adams County that falls within the Denver metro area is partially divided up into multiple municipalities: Arvada, Westminster, Commerce City, Thornton, Northglenn, Federal Heights, and the county seat of Brighton. However, there are significant urbanized/developed parts within the area that are unincorporated, which fall directly under the jurisdiction of the county. Within those unincorporated areas, the county has defined a number of neighborhoods, primarily used for planning purposes. It is important to note that some unincorporated land, according to Adams County’s existing neighborhood definitions, does not fall within any neighborhood.

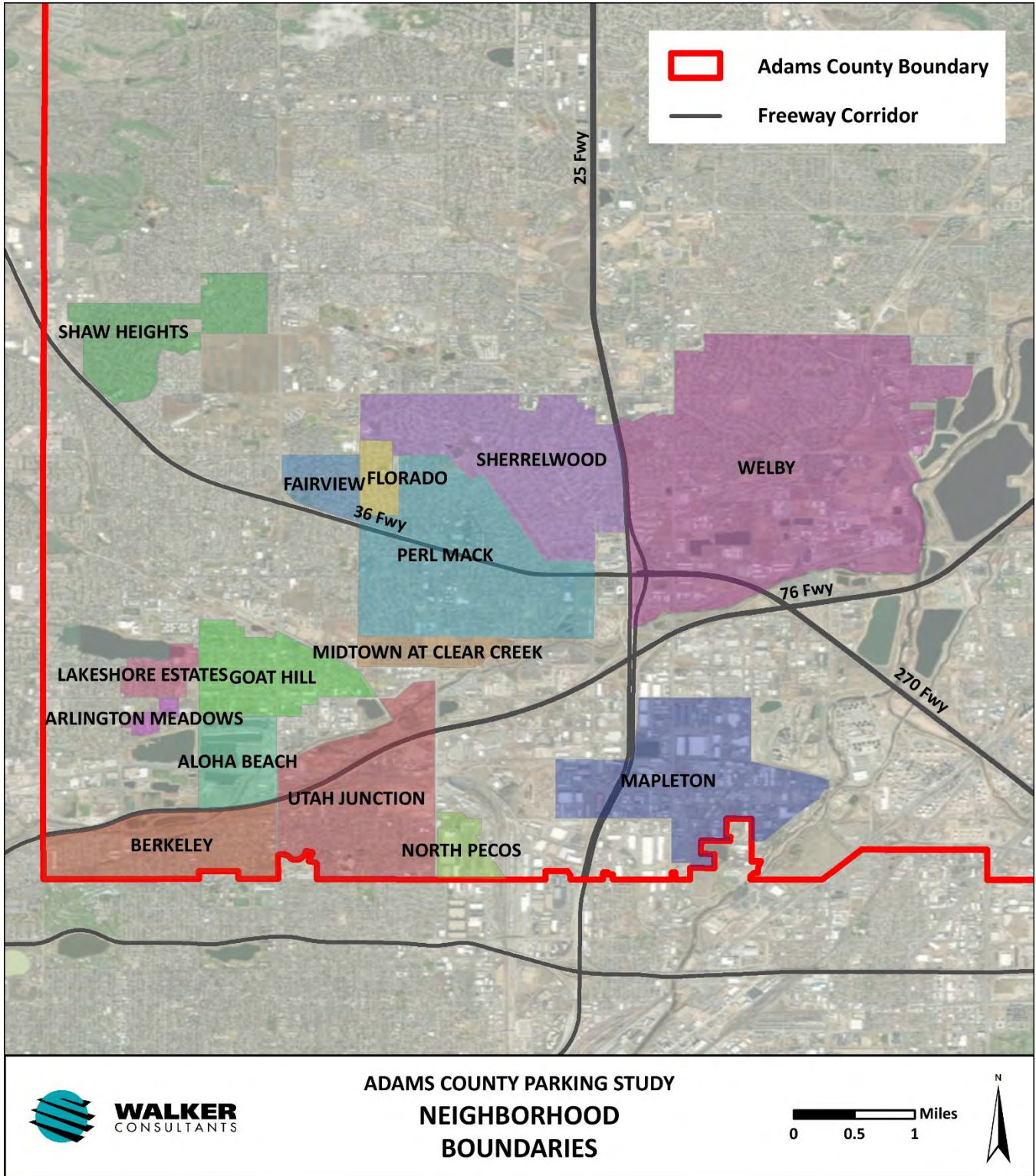
The south westernmost part of the unincorporated area in the county, south of 92nd Avenue and west of Colorado Boulevard, is considered the “inner ring” of unincorporated, developed land, and consists of fifteen neighborhoods. These neighborhoods are:

1. Aloha Beach
2. Arlington Meadows
3. Fairview
4. Florado
5. Goat Hill
6. Lakeshore Estates
7. Mapleton
8. Midtown at Clear Creek
9. North Pecos
10. Perl Mack
11. Shaw Heights
12. Sherrelwood
13. Utah Junction
14. Welby

The nature of each neighborhood differs greatly, though all neighborhoods grade from being suburban to rural in nature. Aloha Beach, Arlington Meadows, Berkeley, Fairview, Florado, Lakeshore Estates, Midtown, and Shaw Heights are primarily composed of residential land uses, mostly single-family detached homes. Goat Hill, Perl Mack, Sherrelwood, Utah Junction, and Welby have a mix of residential, commercial, and industrial land uses. Mapleton and North Pecos are primarily composed of industrial land uses.

The figure below depicts the boundaries of each neighborhood within southwestern Adams County included in this study.

Figure 1: Neighborhoods of Southwestern Adams County



Source: Adams County, Walker Consultants

Arlington Meadows, Fairview, Florado, Lakeshore Estates, Midtown, Shaw Heights, and Sherrelwood were developed to municipal suburban standards, with regular, master-planned lot subdivision patterns and urban infrastructure such as curbs, gutters, and street lighting. Some of the subdivisions that make up these neighborhoods cross into incorporated limits of neighboring municipalities. For instance, the Shaw Heights subdivision partially falls within City of Westminster limits and partially within unincorporated areas. Out of these, Arlington Meadows is the only covenant-controlled community. According to County records, these neighborhoods were generally built out by 1980, with the notable exception of Midtown, which is an entirely new-urbanist, mixed-use infill development.

The remaining neighborhoods have been largely developed to unincorporated/rural standards. Some of these neighborhoods, notably the sections of Berkeley north of Regis University, are in the process of having their infrastructure upgraded to urban standards. In these neighborhoods, significant tracts of rural, undeveloped land exist, and industrial land uses are interspersed throughout.



Berkeley

PURPOSE AND GOALS

The current mix of public parking in most of these neighborhoods is almost exclusively in the form of unmanaged on-street parking, though there are a handful of small, off-street surface lots for community centers and parks (for the purposes of this study, public school parking lots were not considered to be public parking). For these neighborhoods, Adams County has not previously conducted a formal, comprehensive public parking study. The lack of data has inhibited the county's understanding of its public parking system and made moving forward with parking management and enforcement strategies, along with incorporating parking discussions into county planning efforts, difficult. In order to address this lack of data, Adams County commissioned Walker to conduct a formal parking study.

The intent of this study is to obtain a representative sample of quantitative parking data for each of the identified neighborhoods, in the form of inventorying public parking supply (on street and for selected off-street public surface lots) and documenting occupancy rates on typical weekdays and weekends, as well as determining average length of stay for parked vehicles.

Adams County staff has identified a handful of key areas of focus where existing problems are known to exist, and Walker has conducted its study analysis with an eye towards these areas of concern. These areas of concern are as follows:

1. **Inconsistency and obsolescence of existing parking restrictions**

The majority of parking is unrestricted throughout the study areas. Where parking restrictions exist, parking is prohibited at any time. Though most prohibited on-street parking exists along arterials and collectors, there are a handful of examples across the neighborhoods of "legacy" parking restrictions that were put in place on a case-by-case

Walker was tasked with investigating restrictions in order to formulate an understanding of what they are and where they are located. Recommendations for updates and revisions are provided where appropriate based on field observations.

2. **Lack of enforcement & long-term parking of derelict vehicles**

The county does not currently employ staff dedicated to parking enforcement, and Adams County Sheriff's Deputies typically do not issue parking infractions, especially when there's not a clear and present threat to safety. The county itself does not prohibit parking of derelict passenger vehicles; if a vehicle's registration is up to date, it may be parked indefinitely along any street where parking is permitted. However, it should be noted that neighborhoods such as Arlington Meadows that have an active homeowner's association and/or are covenant controlled may have their own restrictions.

Walker was tasked with examining existing parking policy and code requirements. Recommendations are provided to update parking management and enforcement policies moving forward.

3. Spillover parking

The majority of parking complaints received by Adams County staff for the neighborhoods that are part of this study come from three hot spots. One hot spot is found in the Berkeley neighborhood. Significant spillover parking is generated by students of Regis University, which is located within the City and County of Denver. A combination of constrained off-street supply and costly student parking permits incentivize students to park off-site. Public, on-street parking immediately surrounding Regis to the south, east, and west is restricted and routinely enforced by the City and County of Denver. Therefore, spillover parking is known to happen to the north, across 52nd Avenue into Adams County, where parking is currently unrestricted.

Another hot spot is found in the Perl Mack neighborhood, near the interchange of Pecos Street and the 36 Freeway. The Boulder Crossroads Apartments is one of the largest multi-family developments within any neighborhood included in this study. Limited resident parking here contributes to residents parking along neighboring streets, as well as potentially in adjacent private parking lots located to the west across Dakin Street.

Other localized hot spots of spillover parking across the neighborhoods are reported throughout the day along residential streets immediately adjacent to schools. In particular, the F.M. Day Elementary School, also in the Perl Mack neighborhood, was specifically identified.

Walker was tasked with determining the extent to which spillover parking is an issue in the identified hot spots as well as to identify any other problem areas that may exist in any of the other neighborhoods selected for study that are not presently known. This will help to inform and refine Walker's management and enforcement recommendations and support the development of specific, localized strategies that may be appropriate in each hot spot identified.



Utah Junction

These three areas of concern informed Walker’s strategy for selecting representative corridors and parking lots for study in each neighborhood that would yield meaningful, informative data. The number of corridors selected, and the size of each neighborhood’s study area, differed based on a number of factors. For instance, some neighborhoods were very small in geographical size and consisted of a single type of land use or development pattern. One neighborhood, North Pecos, consists primarily of industrial uses, with virtually no public parking outside of a stretch of 52nd Avenue. Additionally, neighborhood corridors were considered based on the frequency of known areas of concern. In neighborhoods with few to no reported occurrences of concern, a small selection of corridors was considered sufficient to establish a meaningful representative sample of parking for that study area.

Other neighborhoods were geographically very large in size, and featured a diversity of development intensity and land uses. These neighborhoods, such as Welby, required a larger selection of corridors for study throughout the neighborhood. Large, contiguous areas of study were selected for study in PerI Mack and Berkeley based on known spillover hot spots described above.

Several off-street parking for community centers and parks within each neighborhood were also selected for study.

STUDY AREAS

The following series of figures depicts the areas of study within each neighborhood. It should be noted that some corridors studied fall outside of the approximate neighborhood boundaries. These corridors were selected for study because they are *de facto* part of their respective neighborhood and would yield meaningful data in accordance with the metrics discussed in this section.

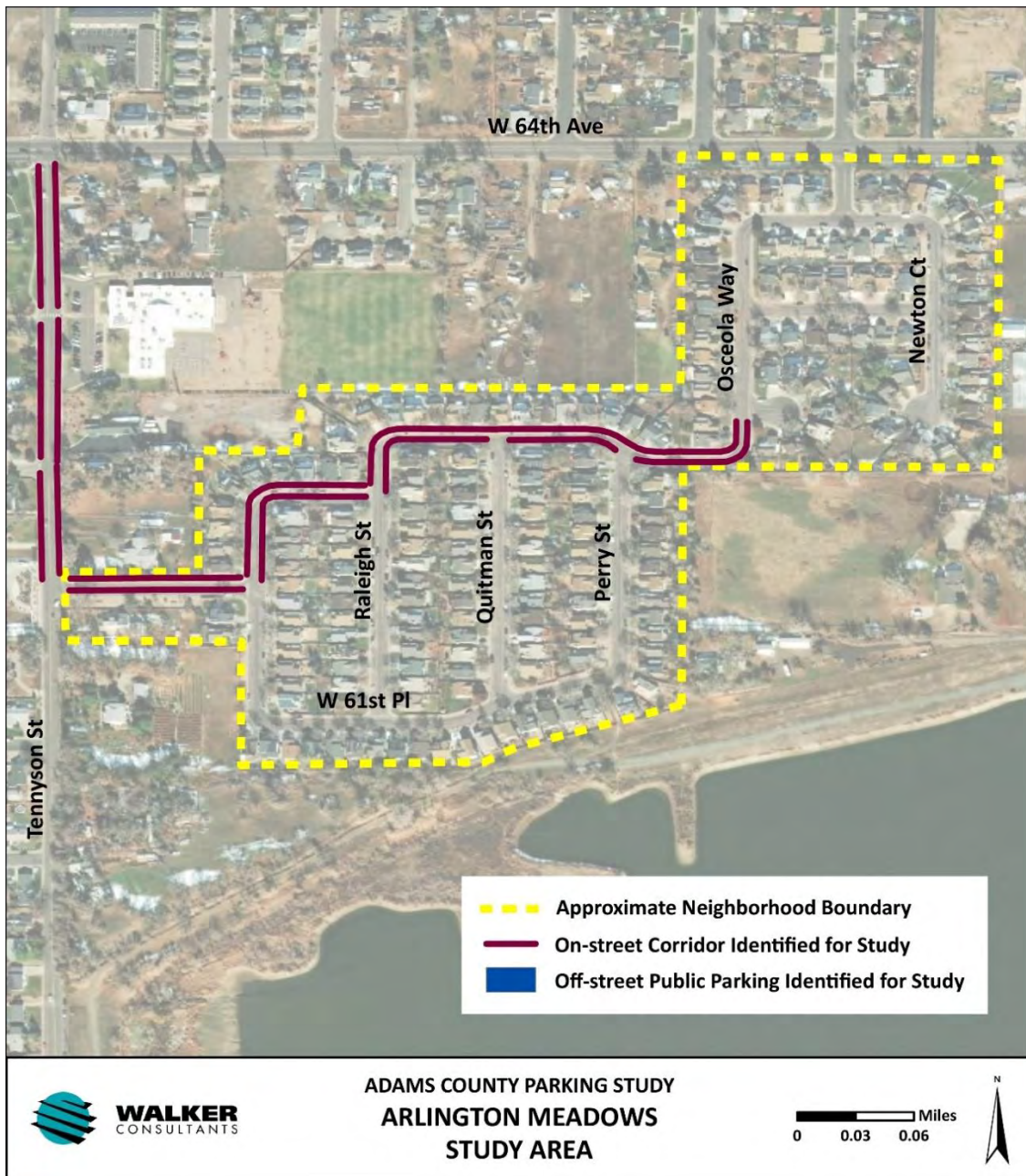


North Pecos

ARLINGTON MEADOWS

Arlington Meadows is a covenant-controlled residential community that is generally bounded by Tennyson Street to the west, 64th Avenue to the north, Lowell Boulevard to the east, and Jim Baker Reservoir to the south. The neighborhood is located completely within unincorporated Adams County. Notable facilities in the neighborhood include the Tennyson Knolls School and Elliott Gardens, an outdoor botanical facility.

Figure 2: Arlington Meadows Study Area



Source: Walker Consultants

ALOHA BEACH

Aloha Beach is a predominantly residential neighborhood roughly bounded by Lowell Boulevard to the west, 64th Avenue to the north, Federal Boulevard to the east, and the 76 Freeway to the south. It should be noted that a significant number of Aloha Beach’s homes line an access-controlled segment of 62nd Avenue east of Lowell; this corridor was therefore excluded from this study. The neighborhood contains some commercial and light industrial uses along Federal Boulevard, though there is no on-street parking in the vicinity of these land uses. Though the northern boundary for Aloha Beach was given as 63rd Avenue, because 62nd Avenue is controlled access, Walker staff determined in the field that Julian Street south of 64th Avenue and a portion of 63rd Avenue was representative of the residential neighborhood characteristics of this study area.

Figure 3: Aloha Beach Study Area

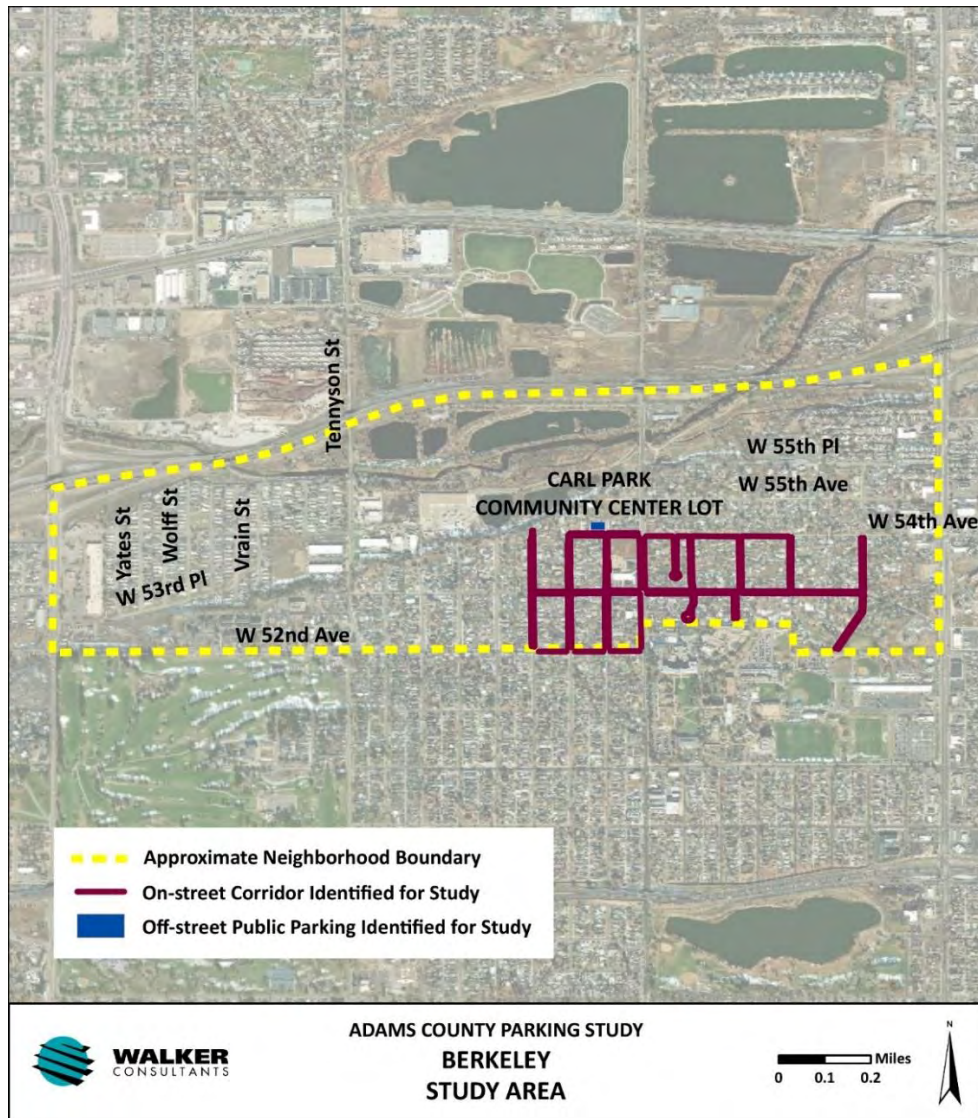


Source: Walker Consultants

BERKELEY

Berkeley is a predominantly residential neighborhood that is generally bounded by Sheridan Boulevard to the west, Federal Boulevard to the east, 52nd Avenue to the south, and the 76 Freeway to the north. There are several commercial establishments along 52nd Avenue and mixed commercial and warehouse developments along Federal Boulevard, Sheridan Boulevard, Tennyson Street, and Lowell Boulevard. There is a large mobile home community on the west side of the study area, and a smaller one mobile home community off of Federal Boulevard, both having private street networks; these streets were not studied. To study the impacts of reported spillover parking from Regis University to the surrounding neighborhood, the entire street network within an approximate 3-block radius of the campus was identified for study.

Figure 4: Berkeley Study Area

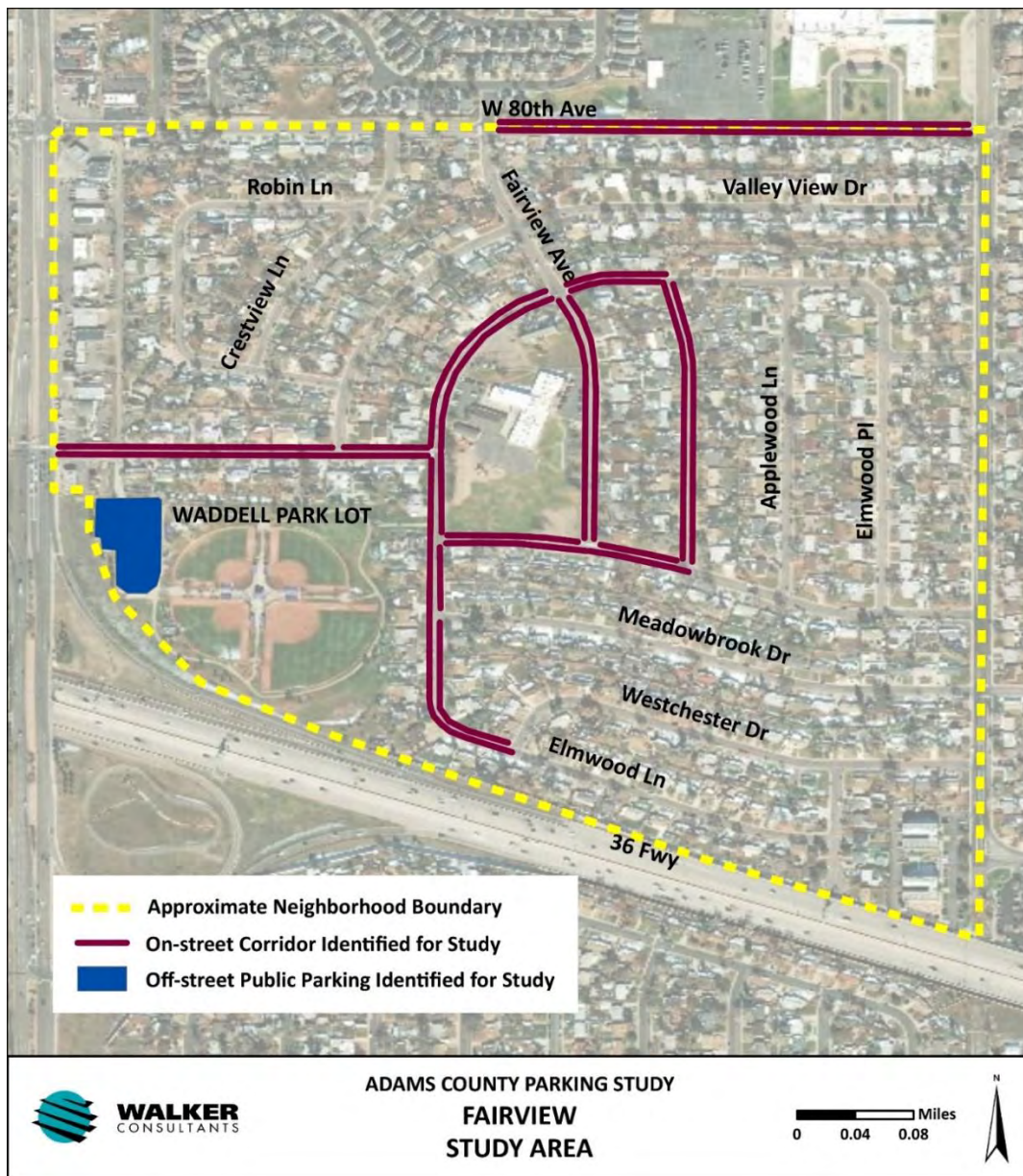


Source: Walker Consultants

FAIRVIEW

Fairview is residential neighborhood roughly bounded by 80th Avenue to the north, Federal Boulevard to the west, Zuni Street to the east, and the 36 Freeway to the south. Non-residential land uses within this neighborhood include a strip of commercial retail along Federal Boulevard, an elementary school, a church, and Waddell Park. The school, church, and park were considered potential causes of spillover parking demand, and corridors adjacent to these uses were identified for study.

Figure 5: Fairview Study Area

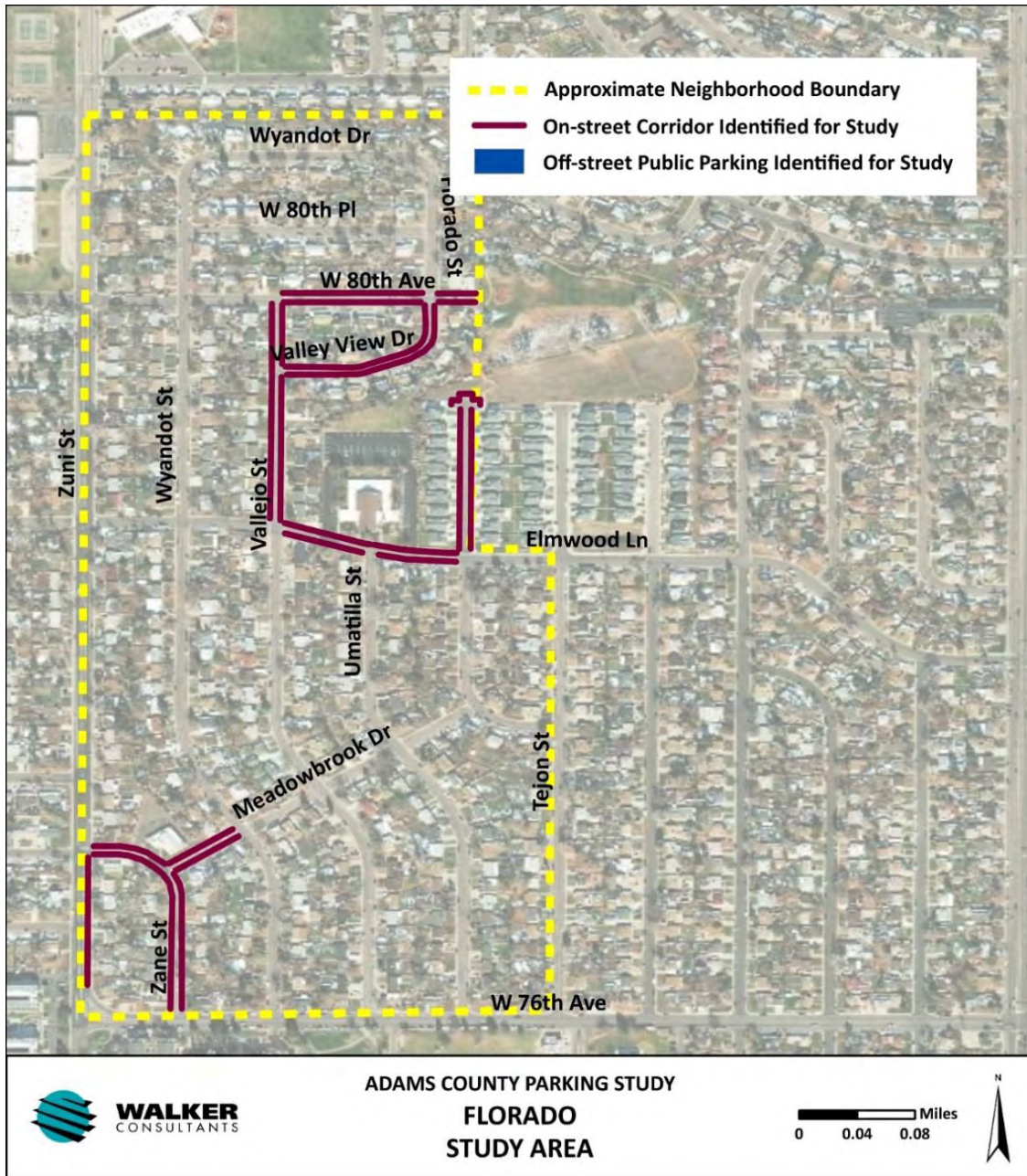


Source: Walker Consultants

FLORADO

Florado is a predominantly residential neighborhood that is immediately adjacent to and bounded by Fairview and Zuni Street to the west, 76th Avenue to the south, 80th Avenue to the north, and Tejon Street to the east. The Florado neighborhood includes a church and new infill housing north of Elmwood Lane.

Figure 6: Florado Study Area



Source: Walker Consultants

GOAT HILL

Goat Hill features a variety of land uses and is bisected by Federal Boulevard. It is bounded by Lowell Boulevard to the west, 64th Avenue to the south, and RTD’s B Line commuter rail and railroad tracks to the north. Notable facilities include the Baker Community Center, whose lot was selected for study, Hodgkins Leadership Academy, as well as several churches that line 65th Avenue. Numerous corridors throughout this neighborhood were identified as potentially having both spillover parking issues as well as outdated parking restrictions. These corridors, identified in Figure 7, were included for study. The section of 63rd Avenue and Clay Street that extends outside of the given neighborhood boundary was identified by Walker staff as being effectively characteristic of Goat Hill.

Figure 7: Goat Hill Study Area



Source: Walker Consultants

LAKESHORE ESTATES

Lakeshore Estates is a residential neighborhood located immediately north of Arlington Meadows and is bounded by 64th Avenue to the south, Hidden Lake and the City of Westminster to the north, Tennyson Street and the City of Arvada to the west, and Lowell Boulevard to the east. There is a multi-family development and church along Tennyson Street, and Hidden Lake Park is located at 66th Avenue and Lowell Boulevard. The parking lot for Hidden Lake Park was identified for inclusion in this study. For this study, 66th Avenue, as well as the adjacent cul-de-sacs to the north of 66th Avenue were identified for study due their proximity to the lake, potentially leading to recreational vehicle/boat parking by lake-goers outside of the neighborhood.

Figure 8: Lakeshore Estates Study Area

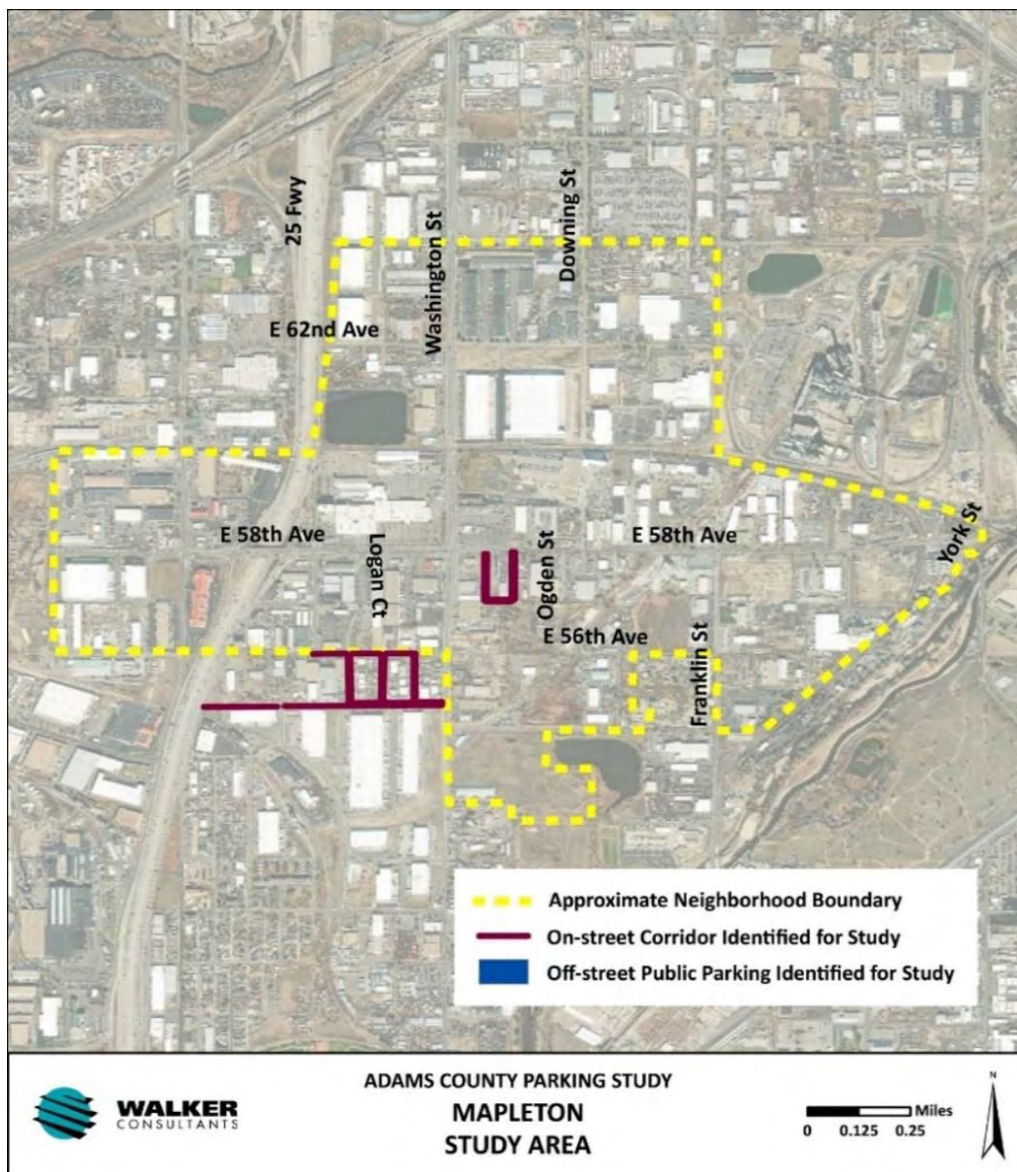


Source: Walker Consultants

MAPLETON

Mapleton is an industrial neighborhood that is not contiguous with any other neighborhood. It is approximately bounded by 56th Avenue and the City and County of Denver to the south, Delaware Street to the west, the South Platte River to the east, and 64th Avenue to the north. There are a handful of homes in the neighborhood interspersed with the industrial uses. The industrial street corridors to the south of 56th Avenue between the Merchandise Mart and the Grizzly Rose, although outside of defined neighborhood boundaries, were identified as having the highest potential for on-street parking demand spillover and were therefore included for study.

Figure 9: Mapleton Study Area

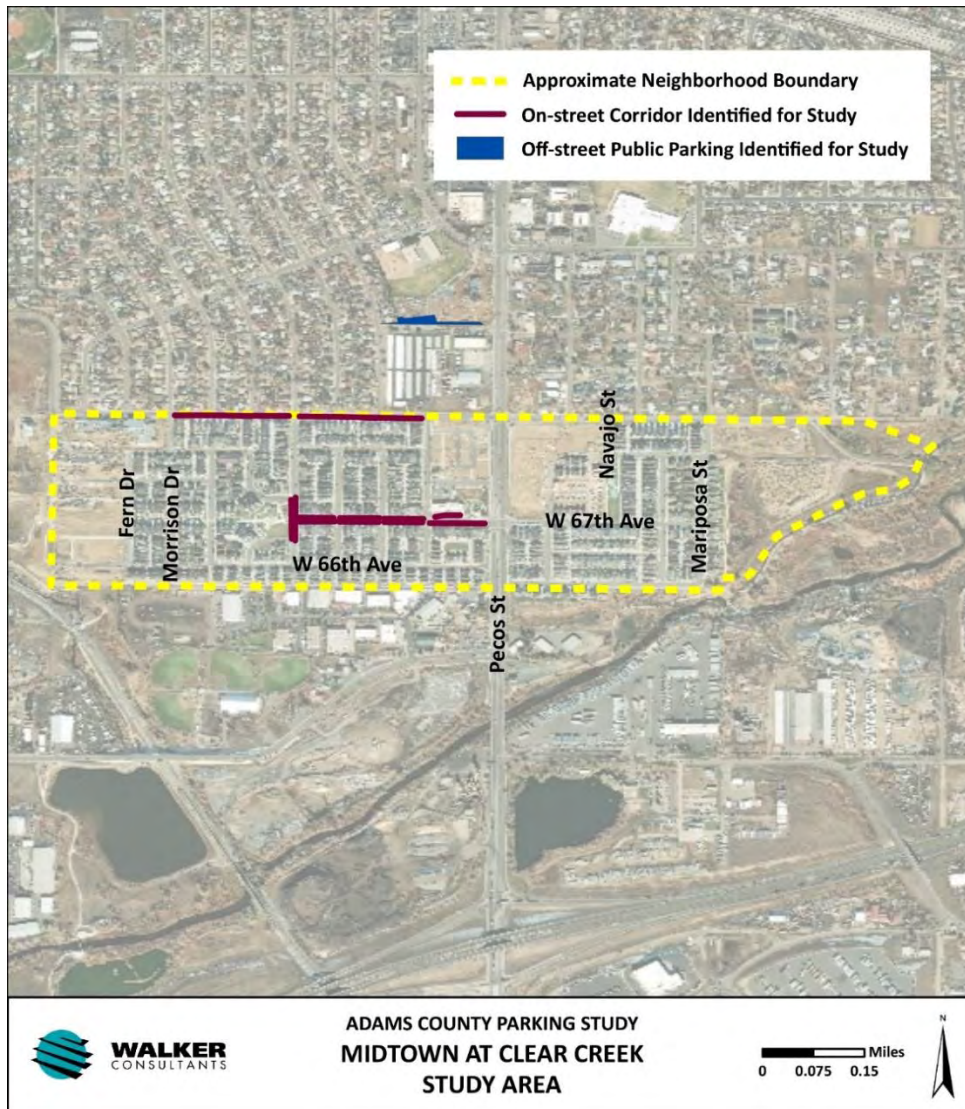


Source: Walker Consultants

MIDTOWN AT CLEAR CREEK

Midtown at Clear Creek (“Midtown”) is a new urban infill development bisected by Pecos Street between 68th Avenue to the north and 66th Avenue to the south, being bounded by the Lower Clear Creek Canal to the east and Federal Boulevard to the west. The given boundaries of Midtown do not currently include the newest phases of development currently under construction west of Zuni. The neighborhood is unique in this study area as it is composed of new construction, mixed-use development that is denser in nature than most other areas included in this study. Though 68th Avenue straddles the boundary between Perl Mack and Midtown, all of 68th Avenue was included with Midtown to reflect the dense, attached housing development along 68th Avenue that is anticipated to generate parking demands in this area.

Figure 10: Midtown Study Area

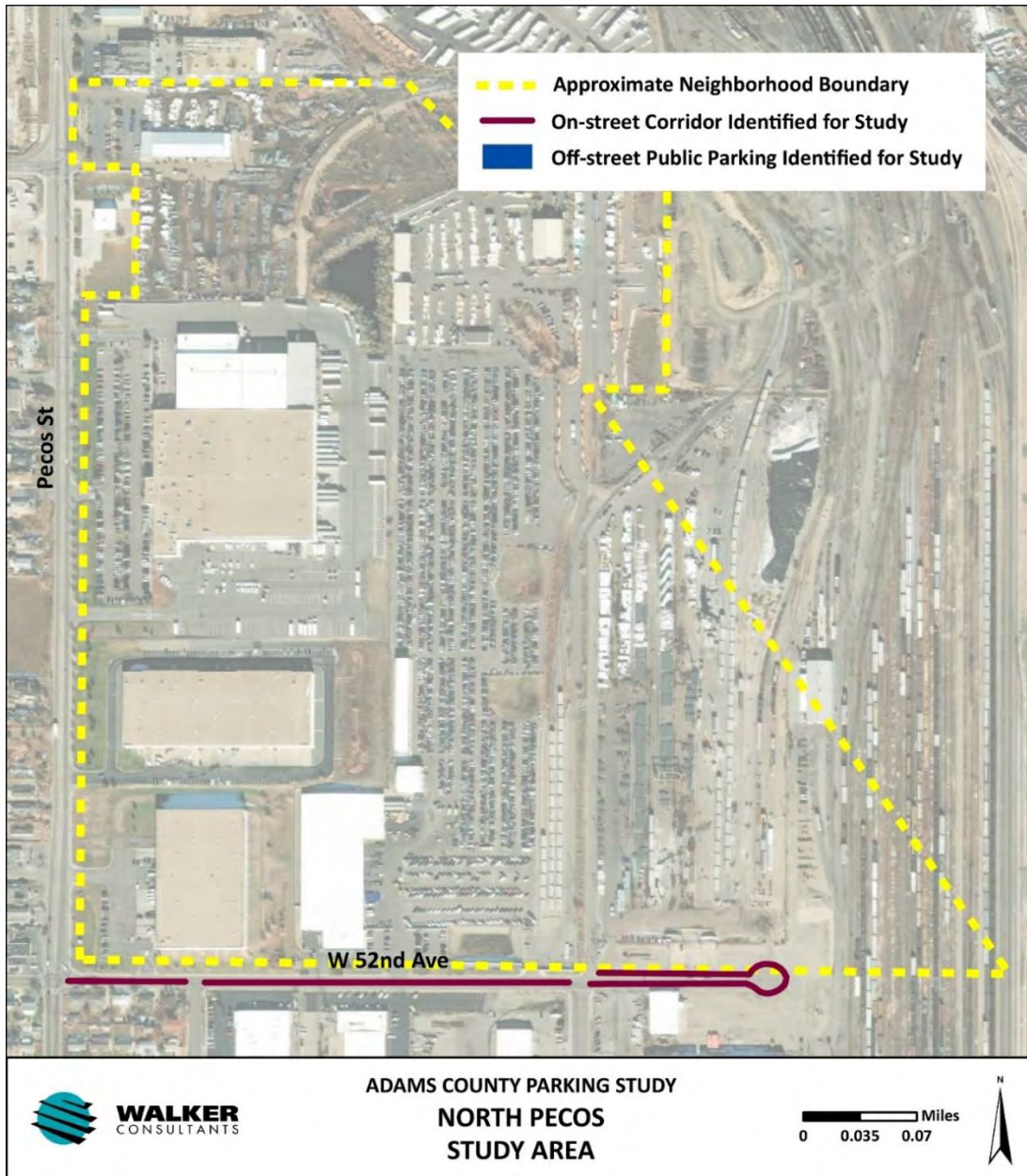


Source: Walker Consultants

NORTH PECOS

The North Pecos neighborhood is located north of 52nd Avenue and east of Pecos Street. It consists of largely of railroad tracks and heavy industrial land uses, including a Coors distribution center. There is no on-street parking permitted along Pecos Street, though there are no posted restrictions along the north side of 52nd Avenue. These are the only two public streets that fall within given boundaries.

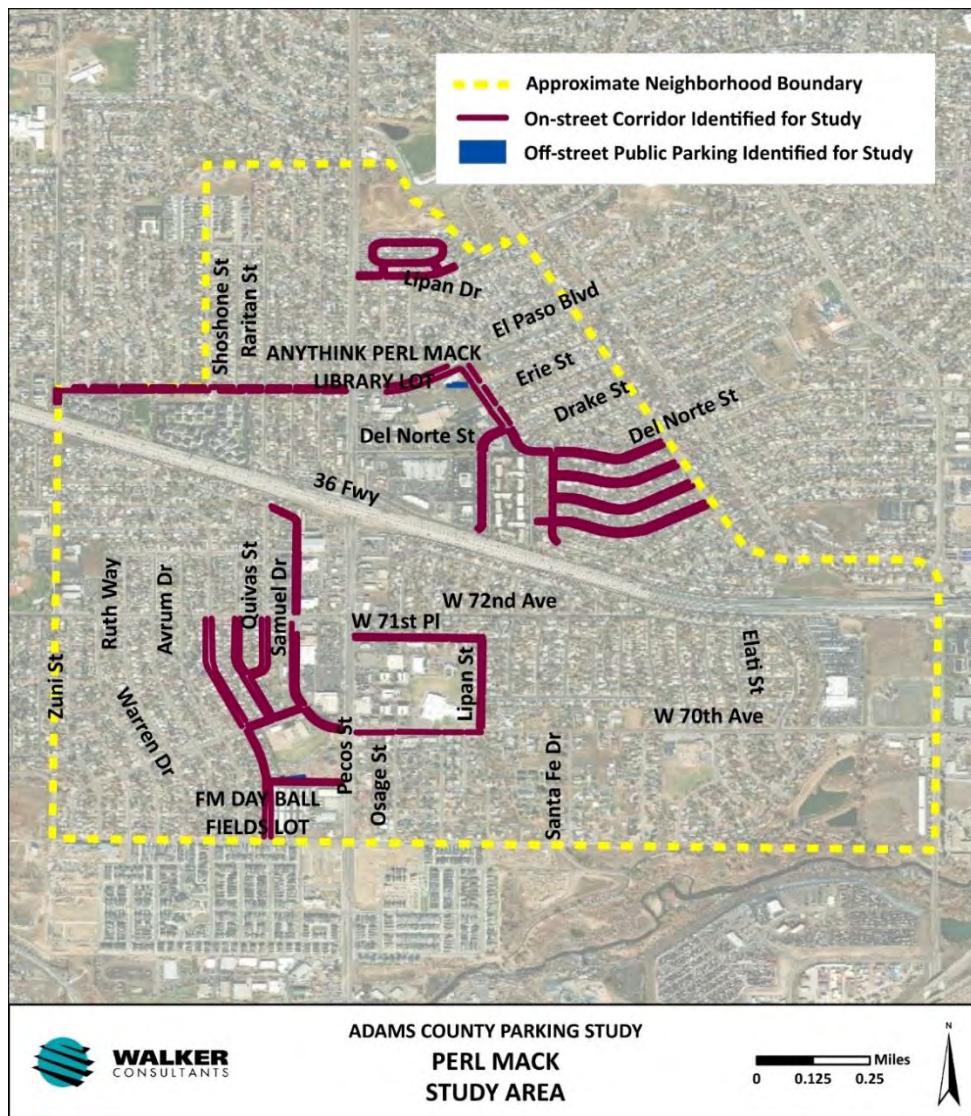
Figure 11: North Pecos Study Area



Source: Walker Consultants

PERL MACK

Perl Mack is one of the largest and most diverse, in terms of land use, neighborhoods that comprise this study. It is approximately bounded by Zuni Street to the west, 68th Avenue to the south, Broadway to the east, and Greenwood Boulevard and the Florado and Sherrelwood neighborhoods to the north. Two of the three areas of concern identified by Adams County for this study occur in the Perl Mack neighborhood. Accordingly, corridors adjacent to the respective activity centers within an approximate 4-block radius were selected for study, in addition to representative corridors in other geographical areas of the neighborhood. Both spillover parking and potential parking conflict amongst competing land uses on either side of Pecos are potential issues. Two off-street lots were also identified for study, at the Anythink Perl Mack Library and the FM Day Ball Fields.

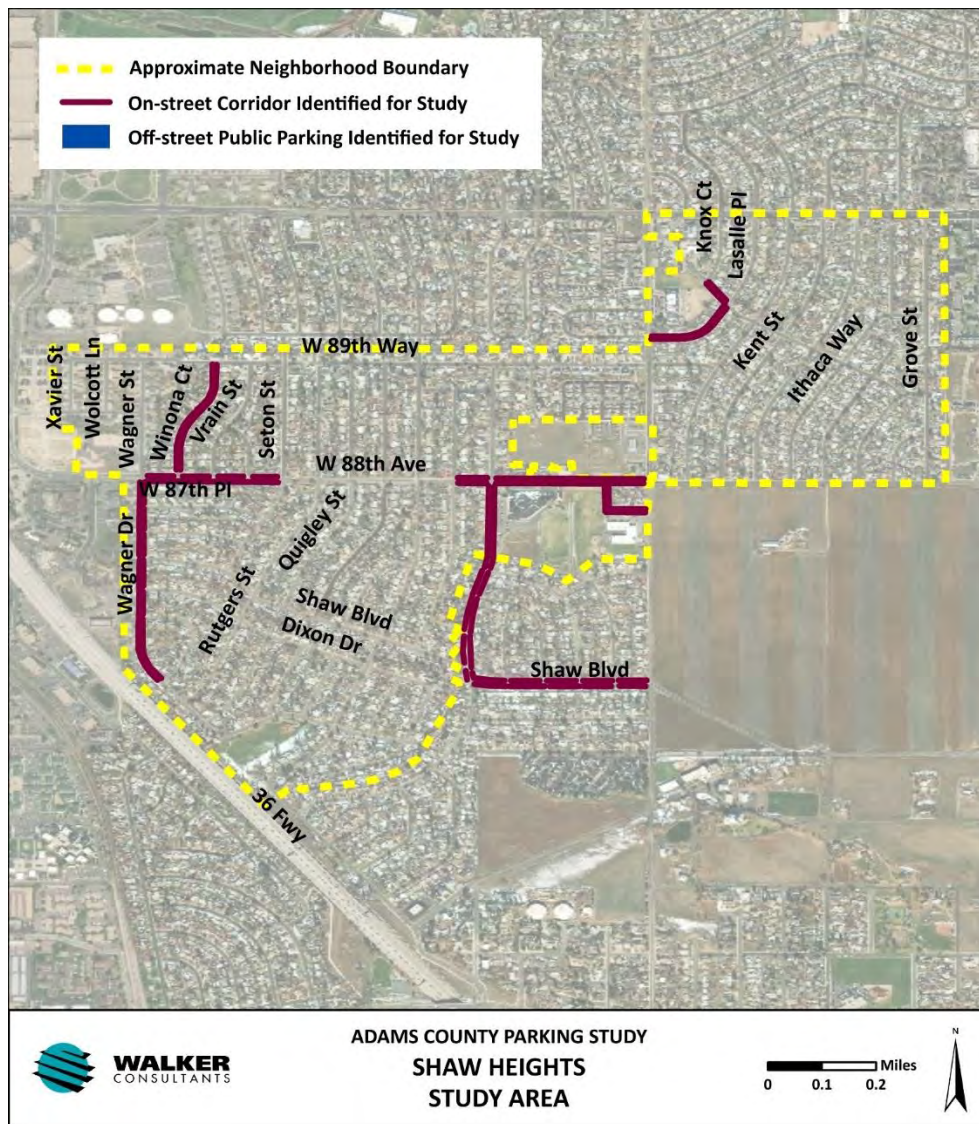
Figure 12: Perl Mack Study Area


Source: Walker Consultants

SHAW HEIGHTS

Shaw Heights is a neighborhood that forms an enclave of unincorporated Adams County completely surrounded by the City of Westminster. It is an irregular area that is bisected by Lowell Boulevard between 84th Avenue and 92nd Avenue, with the 36 Freeway forming the southwest boundary. Part of the neighborhood is unincorporated and part of it is within Westminster city limits. It is predominantly composed of single-family detached residences, though there are three schools within the unincorporated areas and some hotel uses immediately to the west of Wagner Drive. A representative sample of corridors were identified for study. These include the streets that are adjacent to Mesa Elementary and Shaw Heights Middle School, as well as Wagner Drive, due to potential spillover parking from the hotels.

Figure 13: Shaw Heights Study Area



Source: Walker Consultants

SHERRELWOOD

Sherrelwood is a large, residential neighborhood that is generally bounded by 84th Avenue to the north, the 25 Freeway to the east, and Greenwood Boulevard to the southwest, with a section west of Pecos Street. The neighborhood is unique in that its street network largely does not follow the Denver metro grid naming system. There are a handful of parks and schools located throughout the neighborhood, for which adjacent corridors were identified for study given the relatively higher potential for spillover parking. Also, Conifer Road and Greenwood Boulevard were selected for study, which are “main streets” in Sherrelwood. Though the southwest side of Greenwood Boulevard falls within the boundaries of the Perl Mack neighborhood, both sides were included with Sherrelwood for continuity in analysis and to reflect observed field conditions in how the parking demands in this area were perceived to be generated.

Figure 14: Sherrelwood Study Area



Source: Walker Consultants

UTAH JUNCTION

Utah Junction is a mixed neighborhood roughly bounded by 52nd Avenue and the City and County of Denver to the south, Pecos Street to the east, Clear Creek and 64th Avenue to the north, and Federal Boulevard to the west. Approximately the southern half is mostly residential in nature, with mixed levels of infrastructure development. The northern half is generally defined by light industrial and warehouse land uses, with a significant amount of undeveloped or floodplain lands. Corridors identified for study include the streets adjacent to Berkeley Hills Park, Zuni Street fronting the AT&T/CenturyLink north metro switchboard complex, and 52nd Place, due to the interface between residential and non-residential uses in this vicinity.

Figure 15: Utah Junction Study Area

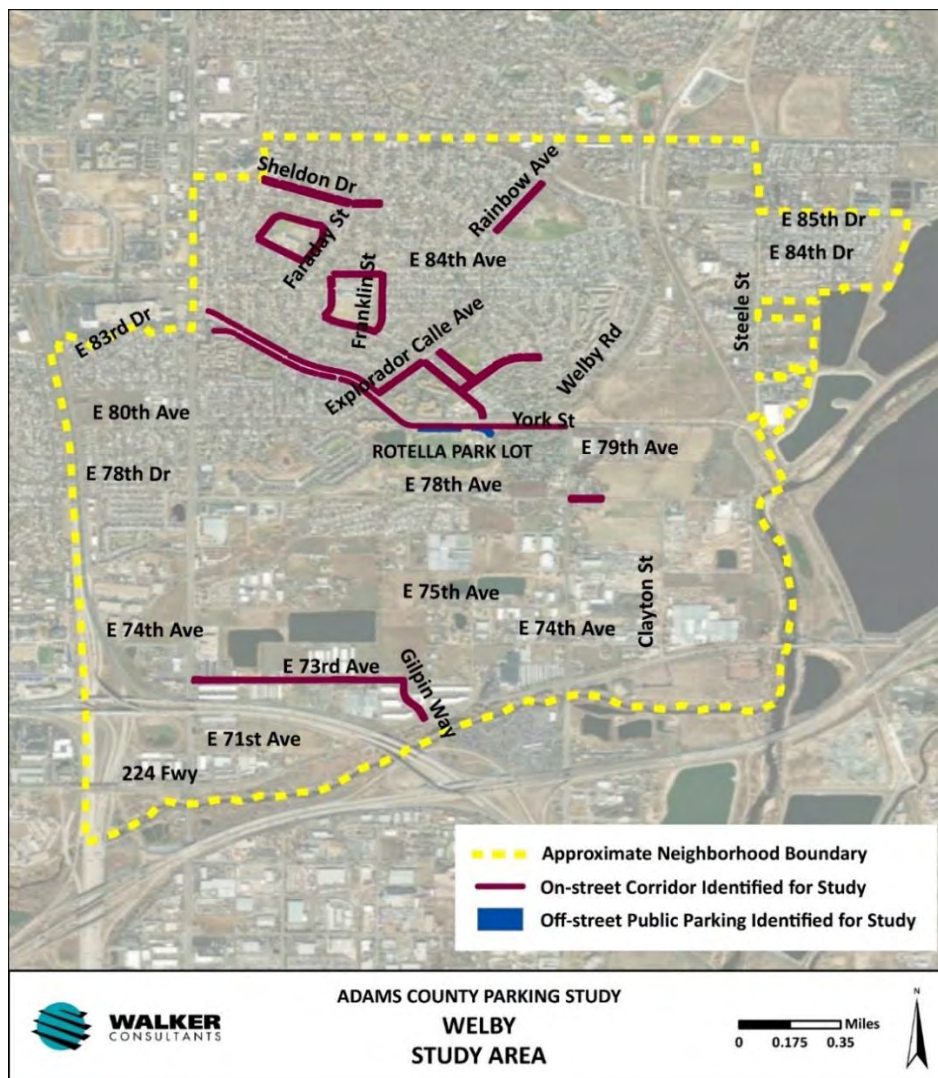


Source: Walker Consultants

WELBY

Welby is by far geographically the largest neighborhood, according to given boundaries, in this study. It comprises nearly all unincorporated areas north of Highway 224, east of the 25 Freeway, south of 88th Avenue, and west of the Thornton Gravel retention ponds. Just as with Utah Junction, Utah Junction is roughly equally comprised of residential and half non-residential land uses, with 78th Avenue being the approximate dividing line. Schools in Welby include Monterey Community School, Coronado Hills Elementary, and McElwain Elementary. Parks of note include Rotella Park, a regional park, as well as City View Park. Corridors identified for study are mostly within the residentially developed areas adjacent to schools and parks, though a stretch of 73rd Avenue and Gilpin Way was selected to represent the industrial land uses to the south, as well as Coronado Parkway, the “main street” of the Coronado Heights sub-division within Welby.

Figure 16: Welby Study Area



Source: Walker Consultants

DEFINITION OF TERMS

Several terms or jargon are used in this report that have unique meanings when used in the parking industry. To help clarify these terms and enhance understanding by the reader, the following definitions are presented.

- **Adequacy** - The difference between the effective parking supply and parking space demand.
- **Design Day** - The day that represents the level of parking demand that the parking system is designed to accommodate. Based on industry standards, this level of activity is typically equal to the 85th to 95th percentile of absolute peak activity. Although we will occasionally design to a higher-than-typical design standard, such as a standard by which demand will exceed supply only one day per month, or even the absolute peak level of demand, we do not typically design to these extreme conditions because the result is an abundance of spaces that remain unused most of the time.
- **Effective Supply** - The total supply of parking spaces, adjusted to reflect the cushion needed to provide for vehicles moving in and out of spaces, spaces unavailable due to maintenance, and to reflect the time necessary for parking patrons to find the last few available spaces. The effective supply varies as to the user group and type of parking, but typically the effective supply is 85 percent to 95 percent of the total number of spaces. The adjustment factor is known as the Effective Supply Factor.
- **Inventory** - The total number of marked parking spaces within the Study Area.
- **Parking Demand** - The number of spaces required by various user groups. Parking demand representing design day conditions is compared with effective supply to determine the adequacy of a parking system.
- **Parking Supply** – The total number of marked parking spaces within the study area.
- **Patron or User** - Any individual parking in a study area.
- **Peak Hour** - The peak hour represents the busiest hour of the day for parking demand.
- **Survey Day** - The day that occupancy counts within a study area are recorded. This day should represent a typical busy day.



02 Methodology

SECTION 2 - METHODOLOGY

Walker Consultants retained Idax Data Solutions, LLC (“Idax”), as a sub-contractor for the data collection phase of this project. Idax worked with Walker to identify and strategically select corridors within each neighborhood for study and create a data collection plan. Idax used proprietary, cloud based data collection software that they created in accordance with Walker’s specifications. In addition to collecting data on parking inventory and occupancy, duration (length of stay) data was collected concurrently. Length of stay data was collected manually, with technicians recording license plate or vehicle makes/models with their corresponding location and time of each observation. Technicians also noted any qualitative observations they made about parking issues or conflicts while in the field, as well as inconsistencies between Adams County’s GIS data and field observations, particularly with regard to the location and presence of parking restriction signage.

WEEKS SELECTED FOR STUDY

The weeks of August 18th and August 25th, 2019 were selected for primary data collection. Idax deployed teams of technicians into each neighborhood with assigned routes to follow. The data collection schedule – the order in which neighborhoods were selected for study -- was determined according to the fall semester schedules of Regis University and the four school districts that fall within the 15 neighborhoods.

Collecting data during active school hours maximizes the likelihood of capturing an accurate, representative peak of morning and afternoon parking demand related to these institutions. This also captures potential spillover parking or parking conflicts that may be occurring throughout the school day. In the case of Regis University, spillover parking was specifically identified as an area of concern for this study. However, data collection was avoided during the first week of school due to abnormal parking, loading, and unloading behavior that occurs during this time, which may not be representative of typical conditions.

DAYS SELECTED FOR STUDY

For each neighborhood, one weekday and one weekend day were selected for data collection. Because parking activity during a typical work week peaks between Tuesday and Thursday, Mondays and Fridays were avoided. Data collection was conducted for an entire neighborhood on a single day where possible.

For the weekend, either Saturday or Sunday was selected based on the particular land use mix of the corridors and areas identified for study within each neighborhood. For areas and corridors that were either predominantly residential or industrial in nature, or that fall within a transitional zone between residential and commercial retail, office, or industrial, Saturday was the preferred weekend day of study. This is due to weekend parking activity typically peaking on Saturdays for these land uses. However, for areas immediately surrounding churches, Sunday was the preferred weekend day. In those areas, weekend afternoon and evening parking demand would be expected to peak primarily or exclusively due to churchgoing activity.

TIMES SELECTED FOR STUDY

There were three observation periods throughout the morning and afternoon and two observation periods during the evening for both weekdays and weekends, with data collection occurring every two hours. For all weekdays, the morning/afternoon data collection window was 9 AM to 1 PM. This was intended to capture peak parking demand generated by schools as well as by government, industrial, and office land uses. The evening data collection window was 9 PM to 11 PM. This was selected to capture peak residential parking demand, as residents are typically home after 9 PM on weekdays. For all weekend days, the same 9 AM to 1 PM window applied. However, the evening window was earlier, between 7 PM and 9 PM. This earlier window was intended to capture peak demand generated by visitors in residential areas and by retail, entertainment, and restaurant customers in commercial areas.

NOTES ABOUT INVENTORY AND OCCUPANCY DATA

The system peak is the time of day which is observed to experience the highest overall parking demand across all corridors and parking lots identified for study in a particular neighborhood. However, particular street segments throughout the day may have experienced localized parking demand that peaked at another time during the day. For instance, a residential neighborhood on the weekday likely experiences its parking peak at 9 PM or 11 PM, as residents are most likely to be home for the evening. However, the street segments immediately surrounding a school or park within that neighborhood may peak at 1 PM or 3 PM due to student activity and parent pickups.

For each neighborhood, Walker identified both the system peak and the “composite” peak on the weekday and weekend day. Occupancy heat maps show the “composite” peak, which is the highest parking demand observed across the day for each street segment or off-street parking lot. In accordance with the goals and purpose of this study, this will visually assist in the identification of problem areas and localized parking “hot spots” that would not otherwise necessarily appear in the system-wide peak.

It should be noted that the individual inventory and occupancy figures for a particular street segment and block face are typically quite low. On a street segment with a parking supply of 5, demand increasing by just one vehicle would represent a 20% increase for that segment. Therefore, large percent increases or decreases in demand observed for a particular segment across time and day may not be statistically significant and should consider additional factor before drawing conclusions.

As discussed in Section 1 of this report, corridors and lots selected for study were selected based on potential “hot spots” of high parking demand within each neighborhood. Therefore, it should be noted that both the system-wide and composite parking demand figures given in this report may over- or under-represent what aggregate parking demand levels are for all public parking within each neighborhood, including the corridors that were not selected for study. In other words, if every single on-street parking space in the neighborhood were studied, aggregate neighborhood demand at all observation times would likely be lower than that of the representative sample areas contained in this study.

The inventory and occupancy figures contained in this report only constitute representative samples of each neighborhood’s public parking system, and shall not be construed to represent a complete accounting of all public parking within a neighborhood.

NOTES ABOUT LENGTH OF STAY DATA

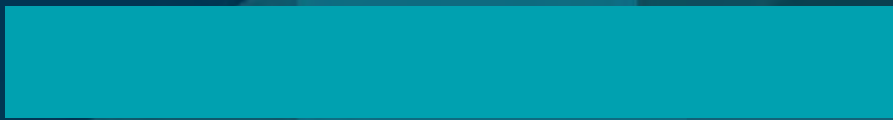
Length of stay data was collected concurrently with inventory and occupancy. Because the intervals between counts were irregular and data was not collected across consecutive days, conclusions that can be drawn from length of stay data are limited, especially in residential neighborhoods or areas where a significant percentage of observed vehicles were likely to be parked overnight.

For instance, Walker staff observed many unique vehicles recorded at 11 PM only, or during the 9 and 11 PM counts but not at other times. Typically, these vehicles would be recorded as being parked for less than 2 hours and for between 2 and 4 hours, respectively. These vehicles observed at either one or both evening data collection times were likely to be resident overnight parkers, and not short-term (under 4 hour) visitor parkers.

Therefore, and in accordance with the purpose and goals of this study identified in the Introduction, Walker staff grouped length of stay data into two categories: vehicles parked all day, whose license plate occurred during every data collection time, and vehicles that left for at least part of the day, whose license plate did not occur during at least one data collection time. This data can potentially be used to extrapolate conclusions about each neighborhood. For instance, a higher percentage of all-day parkers suggests that more on-street parking may be used for long-term vehicle storage. In residential neighborhoods, it could also suggest that residents of that neighborhood are less active than other neighborhoods, or that fewer homes in the neighborhood have driveways.

When comparing the ratio of all-day parkers from the weekday to the weekend, other conclusions can be assumed. For instance, if the number of all-day parkers on the weekend is much higher than on the weekday in a residential neighborhood, it is suggestive that the vast majority of residents in that neighborhood work during the week, and that parking activity from non-residential uses is low. However, if the weekend percentage of all day parkers is much lower than the weekday number, it is suggestive that there is a notable amount of non-residential-related parking activity going on during weekends that is likely short-term in nature. In a non-residential neighborhood, it indicates that on-street parking is likely not being used for long-term vehicle storage, even if the number of all-day parkers is high during weekdays.

The length of stay data contained in this report can be used to provide context to results and conclusions derived from inventory and occupancy data, though it cannot be definitively used to make judgements or calculations about the exact nature of parking activity or exact lengths of stays along a particular corridor or in a neighborhood.



03 Inventory & Occupancy

SECTION 3 – INVENTORY & OCCUPANCY

INVENTORY & PEAK OCCUPANCY (ALL NEIGHBORHOODS)

Across all 15 neighborhoods, Walker surveyed approximately 7,071 public on-street parking spaces and 479 public off-street parking spaces. The following table summarizes weekday and weekend on-street parking inventory and peak occupancy by neighborhood.

Table 1: Weekday On-street Parking Inventory and Peak Occupancy by Neighborhood

Neighborhood	Number of On Street Parking Spaces Surveyed	Weekday System-wide Peak Percent Occupancy	Weekday Composite Total Peak Percent Occupancy
North Pecos	20	50%	50%
Perl Mack	1,337	40%	44%
Berkeley	629	33%	41%
Florado	237	39%	39%
Aloha Beach	154	31%	34%
Arlington Meadows	113	27%	29%
Midtown	157	24%	28%
Shaw Heights	533	20%	27%
Sherrelwood	934	21%	26%
Mapleton	306	23%	25%
Welby	1,234	18%	24%
Fairview	450	14%	18%
Utah Junction	277	17%	18%
Goat Hill	389	10%	14%
Lakeshore Estates	301	7%	11%
Total (Weekday)	7,071	25%	29%

Source: Walker Consultants

Table 2: Weekend On-street Parking Inventory and Peak Occupancy by Neighborhood

Neighborhood	Number of On Street Parking Spaces Surveyed	Weekend System-wide Peak Percent Occupancy	Weekend Composite Total Peak Percent Occupancy
Florado	237	40%	48%
Perl Mack	1,337	41%	48%
Midtown	157	27%	36%
Aloha Beach	154	34%	35%
Arlington Meadows	113	27%	34%
Sherrelwood	934	22%	27%
Berkeley	629	22%	25%
Shaw Heights	533	20%	24%
Utah Junction	277	19%	22%
Welby	1,234	16%	21%
Fairview	450	17%	20%
Goat Hill	389	13%	17%
Mapleton	306	13%	16%
North Pecos	20	10%	10%
Lakeshore Estates	301	6%	9%
Total (Weekend)	7,071	22%	26%

Source: Walker Consultants

The following two tables show weekend and weekday off-street parking and peak occupancy by neighborhood.

Table 3: Weekday Off-street Parking Inventory and Peak Occupancy by Neighborhood

Neighborhood	Number of Off Street Parking Spaces Surveyed	Weekday System-wide Peak Percent Occupancy
Perl Mack	33	42%
Goat Hill	23	4%
Lakeshore Estates	121	4%
Welby	159	4%
Fairview	143	2%
Total (Weekday)	479	11%

Source: Walker Consultants

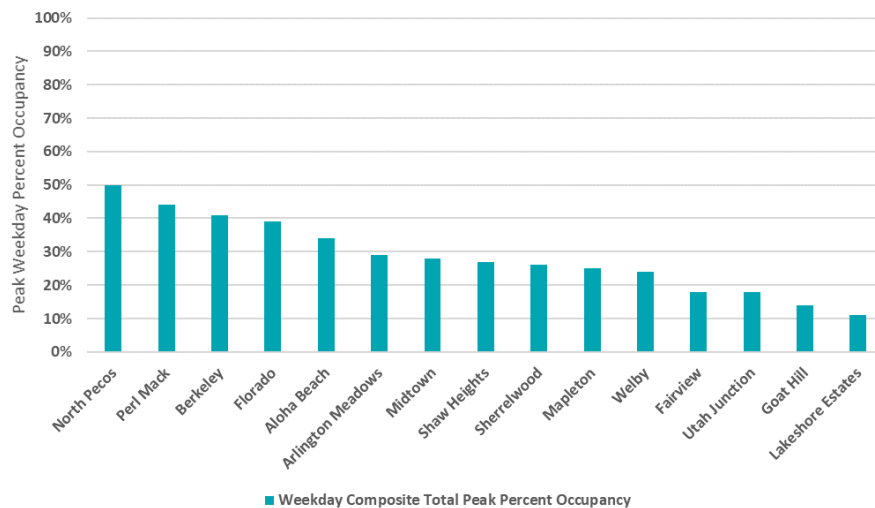
Table 4: Weekend Off-street Parking Inventory and Peak Occupancy by Neighborhood

Neighborhood	Number of Off Street Parking Spaces Surveyed	Weekend System-wide Peak Percent Occupancy
Perl Mack	33	16%
Welby	159	8%
Fairview	143	4%
Lakeshore Estates	121	3%
Goat Hill	23	0%
Total (Weekend)	479	6%

Source: Walker Consultants

Composite peak weekday on-street demand ranged from 50% occupied in North Pecos to 11% in Lakeshore Estates, as shown in the following figure, with the average peak composite weekday occupancy being 27%.

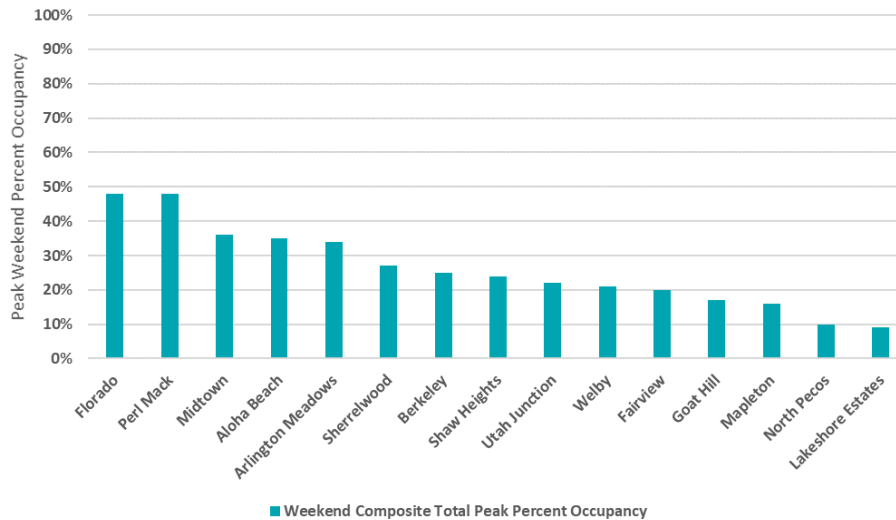
Figure 17: Weekday On-street Composite Peak Occupancy by Neighborhood



Source: Walker Consultants

Composite peak weekend on-street demand ranged from 48% occupancy in Florado to 9% in Lakeshore Estates, as shown in the following figure, with an average peak composite weekend occupancy of 26%.

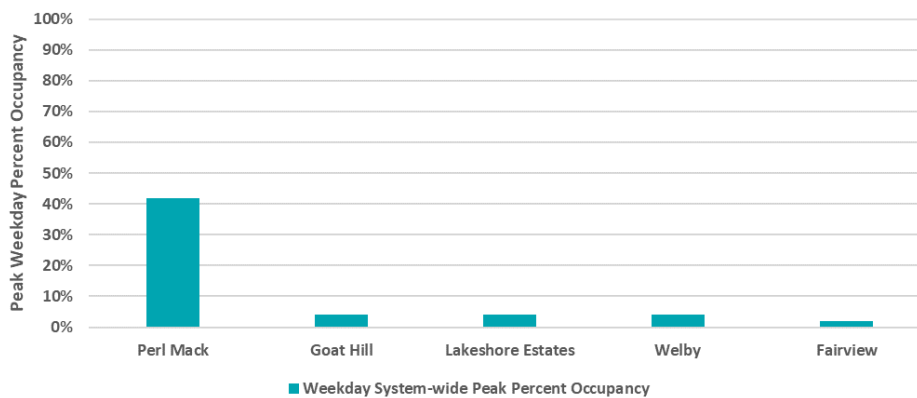
Figure 18: Weekend On-street Composite Peak Occupancy by Neighborhood



Source: Walker Consultants

Peak weekday off-street demand ranged from 2% occupancy in Fairview to 42% in Perl Mack, as shown in the following figure, with the average peak composite weekday occupancy being 11%.

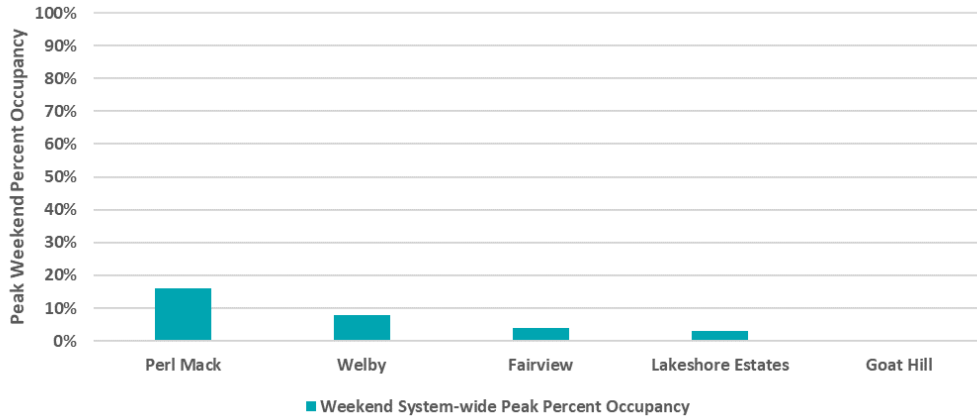
Figure 19: Weekday Off-street Peak Occupancy by Neighborhood



Source: Walker Consultants

Peak weekend off-street demand ranged from 0% occupancy in Goat Hill to 16% in Perl Mack, as shown in the following figure, with the average peak composite weekend occupancy being 6%.

Figure 20: Weekend Off-street Peak Occupancy by Neighborhood

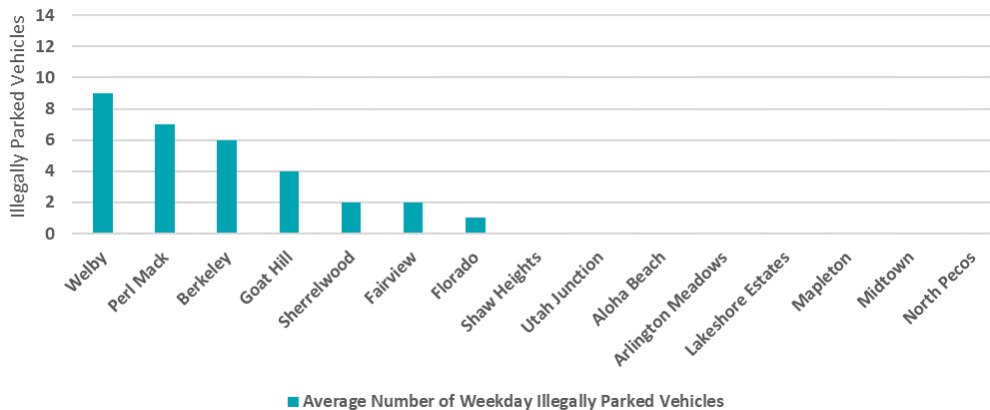


Source: Walker Consultants

ILLEGALLY PARKED VEHICLES (ALL NEIGHBORHOODS)

The average number of weekday illegally parked vehicles along all corridors ranged from zero in eight neighborhoods to nine in the Welby neighborhood. The average number of weekday illegally parked vehicles per neighborhood was two. The figure below shows average number of weekday illegally parked vehicles by neighborhood.

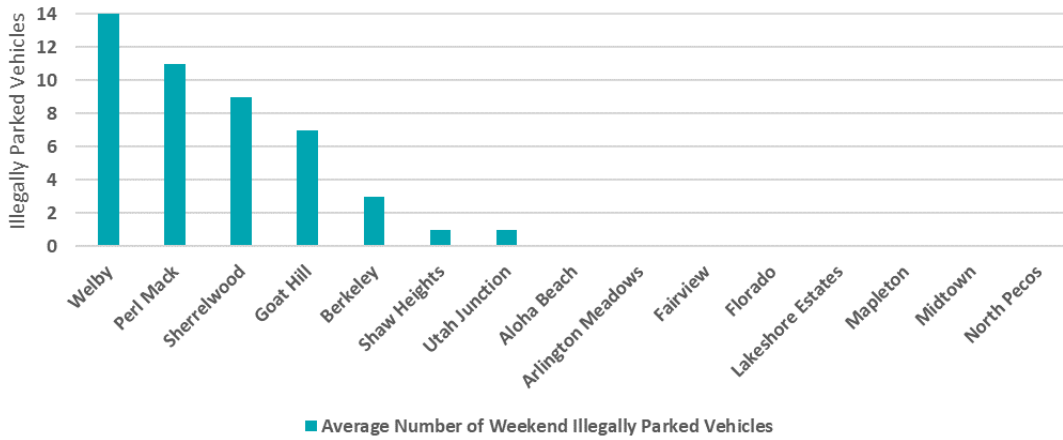
Figure 21: Average Number of Weekday Illegally Parked Vehicles by Neighborhood



Source: Walker Consultants

The average number of weekend illegally parked vehicles along all corridors ranged from zero in eight neighborhoods to 14 in the Welby neighborhood. The average number of weekend illegally parked vehicles per neighborhood was three. The figure below shows average number of weekend illegally parked vehicles by neighborhood.

Figure 22: Average Number of Weekend Illegally Parked Vehicles by Neighborhood



Source: Walker Consultants

ALOHA BEACH

A total of 9 block faces were surveyed across four different streets.

INVENTORY

Walker staff observed a total of approximately 154 public on-street parking spaces along all corridors identified for study in Aloha Beach. The table below summarizes parking supply by block face:

Table 5: Aloha Beach Observed Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
Julian St	Alley	W 63rd Ave	W	12	0	0	12
	Alley	W 64th Ave	E	11	0	0	11
	W 63rd Ave	Alley	E	1	0	0	1
	W 64th Ave	Alley	W	13	0	0	13
Lowell Blvd	Dead End	W 62nd Ave	E	0	0	0	0
	W 62nd Ave	Dead End	W	0	0	0	0
W 63rd Ave	Federal Blvd	Julian St	N	33	0	0	33
	Julian St	Federal Blvd	S	84	0	0	84
W 64th Ave	Knox St	Lowell Blvd	N	0	0	0	0
Total On Street Parking				154	0	0	154

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the Aloha Beach neighborhood was performed on Wednesday, August 28, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. Parking demand for the neighborhood ranged from 32 vehicles (21% occupied) at 9 AM to a system-wide peak of 48 vehicles (31% occupied) at 9 PM and 11 PM.

The table below summarizes parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

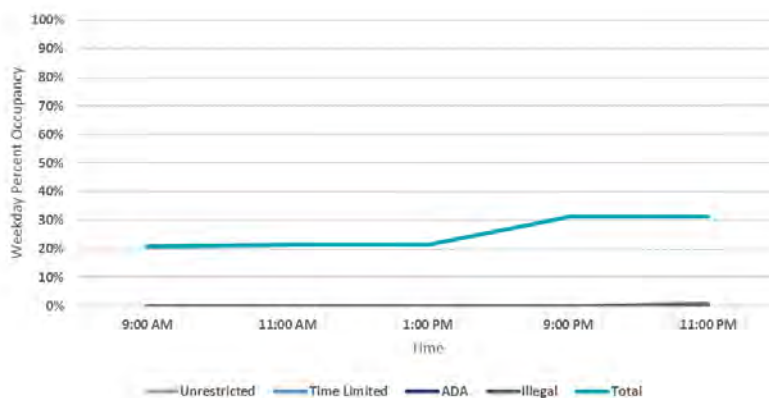
Table 6: Aloha Beach Observed Weekday Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	154	32	21%	0	0%	0	0%	0	0%	32	21%
11:00 AM	154	33	21%	0	0%	0	0%	0	0%	33	21%
1:00 PM	154	33	21%	0	0%	0	0%	0	0%	33	21%
9:00 PM	154	48	31%	0	0%	0	0%	0	0%	48	31%
11:00 PM	154	48	31%	0	0%	0	0%	1	1%	48	31%
Average	154	39	25%	0	0%	0	0%	0	0%	39	25%

Source: Walker Consultants

The figure below shows total weekday parking occupancy by time for the Aloha Beach neighborhood.

Figure 23: Aloha Beach Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 11 PM, demand by block face ranged from 0% occupied (multiple segments) to 100% occupied on Julian Street between 63rd Avenue and 64th Avenue. Overall, there were four block faces with occupancies of 0% at the system-wide weekday peak and two block faces that were 85% or above, with 85% occupancy typically being the level of demand at which parking is considered to be “full.” Parking demand during the system-wide weekday peak was generally concentrated along Julian Street.

One illegally parked vehicle was observed along Julian Street between 64th Avenue and the alley and was only observed during the 11 PM collection.

The following table summarizes parking occupancy by block face at 11 PM.

Table 7: Aloha Beach Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					Total Weekday Demand (System-wide Peak)	Weekday % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
Julian St	Alley	W 63rd Ave	W	12	2	0	0	0	2	17%	
	Alley	W 64th Ave	E	11	11	0	0	0	11	100%	
	W 63rd Ave	Alley	E	1	1	0	0	0	1	100%	
	W 64th Ave	Alley	W	13	11	0	0	1	11	0%	
Lowell Blvd	Dead End	W 62nd Ave	E	0	0	0	0	0	0	0%	
	W 62nd Ave	Dead End	W	0	0	0	0	0	0	0%	
W 63rd Ave	Federal Blvd	Julian St	N	33	12	0	0	0	12	36%	
	Julian St	Federal Blvd	S	84	11	0	0	0	11	13%	
W 64th Ave	Knox St	Lowell Blvd	N	0	0	0	0	0	0	0%	
Total (Weekday System-wide Peak)				154	48	0	0	1	48	31%	

Source: Walker Consultants

Table 5 represents a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (11 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

Two block faces were observed with parking demand peak occupancies greater than they exhibited during the system-wide peak. For the weekday, the overall composite peak was approximately the same as the system-wide peak.

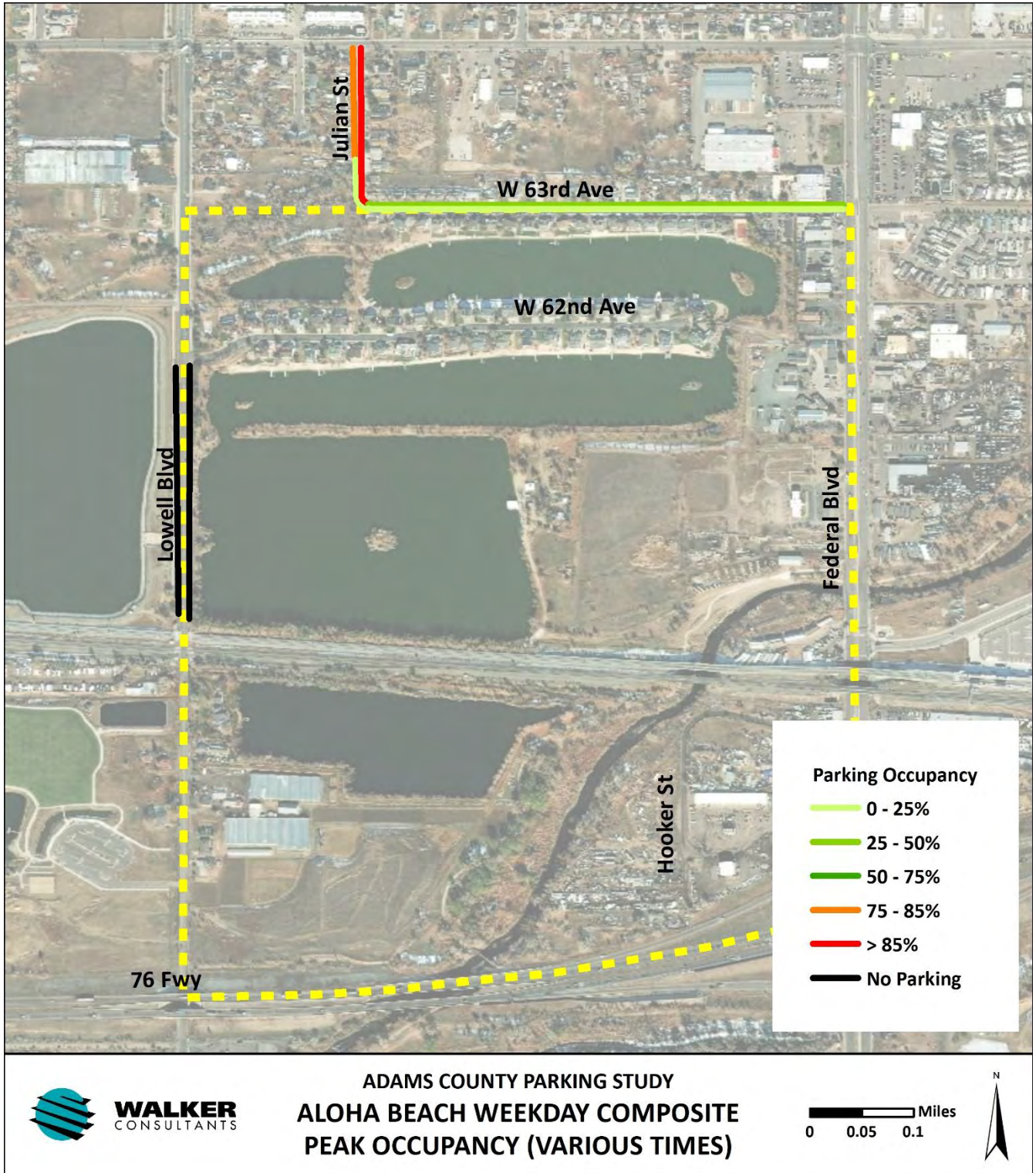
Table 8: Aloha Beach Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)						Time that Peak Occurred
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Peak % Occupancy (Across All Times)	
Julian St	Alley	W 63rd Ave	W	12	3	0	0	0	3	25%	9:00 PM
	Alley	W 64th Ave	E	11	11	0	0	0	11	100%	11:00 PM
	W 63rd Ave	Alley	E	1	1	0	0	0	1	100%	11:00 PM
	W 64th Ave	Alley	W	13	6	0	0	0	6	0%	
Lowell Blvd	Dead End	W 62nd Ave	E	0	0	0	0	0	0	0%	
	W 62nd Ave	Dead End	W	0	0	0	0	0	0	0%	
W 63rd Ave	Federal Blvd	Julian St	N	33	13	0	0	0	13	39%	9:00 PM
	Julian St	Federal Blvd	S	84	13	0	0	0	13	15%	11:00 PM
W 64th Ave	Knox St	Lowell Blvd	N	0	0	0	0	0	0	0%	
Total (Weekday Composite)				154	47	0	0	0	47	31%	11:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekday parking demand per block face across all collection periods.

Figure 24: Aloha Beach Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the Aloha Beach neighborhood was performed on Saturday, August 31, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 33 vehicles (21% occupied) at 7 PM to the system-wide peak of 52 vehicles (34% occupied) at 7 PM.

The table below summarizes parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

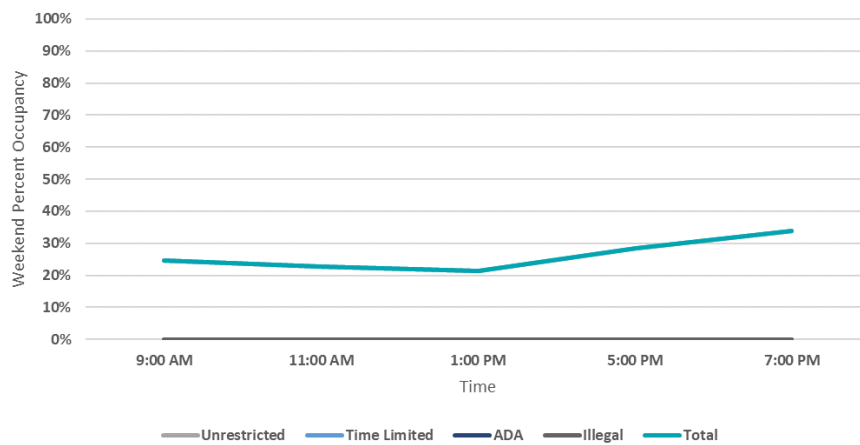
Table 9: Aloha Beach Observed Weekend Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	154	38	25%	0	0%	0	0%	0	0%	38	25%
11:00 AM	154	35	23%	0	0%	0	0%	0	0%	35	23%
1:00 PM	154	33	21%	0	0%	0	0%	0	0%	33	21%
5:00 PM	154	44	29%	0	0%	0	0%	0	0%	44	29%
7:00 PM	154	52	34%	0	0%	0	0%	0	0%	52	34%
Average	154	40	26%	0	0%	0	0%	0	0%	40	26%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the Aloha Beach neighborhood.

Figure 25: Aloha Beach Total Weekend Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekend peak at 7 PM, demand by block face ranged from 0% occupied (multiple segments) to 100% occupied along 63rd Avenue. Overall, there were four block faces with occupancies of 0% during the system-wide weekend peak and one segment, Julian Street, that was at or above 85% occupied. Parking demand during the system-wide weekday peak was largely concentrated along the same streets as observed during weekday collections.

No illegally parked vehicles were observed during weekend collections in the Aloha Beach neighborhood.

The following table summarizes parking occupancy by block face at 7 PM on Saturday.

Table 10: Aloha Beach Weekend System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					
					Un-restricted	Time Limited	ADA	Illegal	Total Weekend Demand (System-wide Peak)	Weekend Peak % Occupancy (System-wide Peak)
Julian St	Alley	W 63rd Ave	W	12	0	0	0	0	0	0%
	Alley	W 64th Ave	E	11	8	0	0	0	8	73%
	W 63rd Ave	Alley	E	1	1	0	0	0	1	100%
	W 64th Ave	Alley	W	13	10	0	0	0	10	77%
Lowell Blvd	Dead End	W 62nd Ave	E	0	0	0	0	0	0	0%
	W 62nd Ave	Dead End	W	0	0	0	0	0	0	0%
W 63rd Ave	Federal Blvd	Julian St	N	33	8	0	0	0	8	24%
	Julian St	Federal Blvd	S	84	25	0	0	0	25	30%
W 64th Ave	Knox St	Lowell Blvd	N	0	0	0	0	0	0	0%
Total (Weekend System-wide Peak)				154	52	0	0	0	52	34%

Source: Walker Consultant

Table 5 represents a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during system-wide peak (7 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

There was one block face, 63rd Avenue between Federal Boulevard and Julian Street at 5 PM, with an individual peak occupancy greater than its exhibited occupancy during the system-wide peak of 7 PM. During weekend observations, the overall composite peak was approximately 1% higher than the system peak.

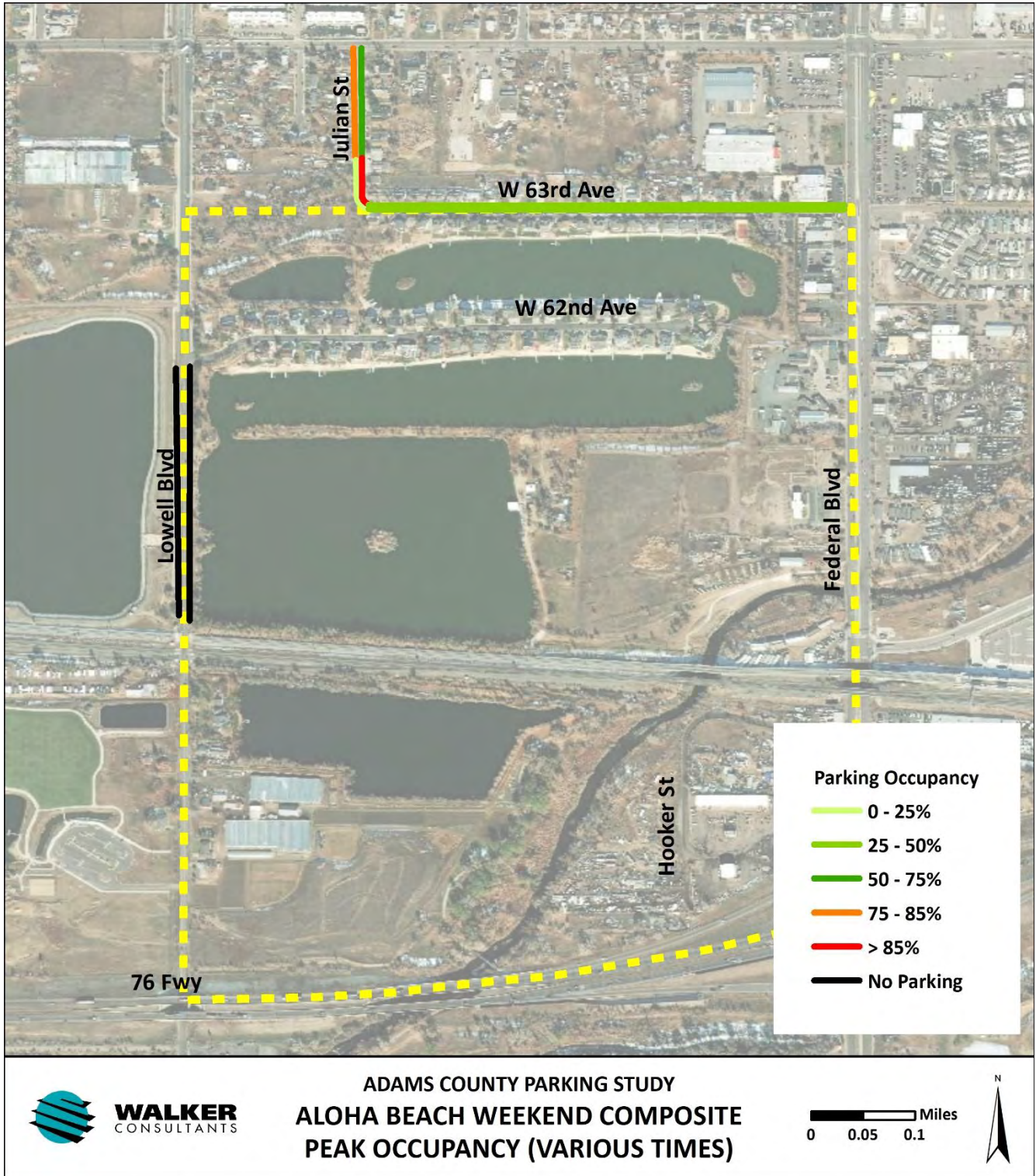
Table 11: Aloha Beach Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)	Time that Peak Occurred
Julian St	Alley	W 63rd Ave	W	12	0	0	0	0	0	0%	
	Alley	W 64th Ave	E	11	8	0	0	0	8	73%	11:00 PM
	W 63rd Ave	Alley	E	1	1	0	0	0	1	100%	11:00 PM
	W 64th Ave	Alley	W	13	10	0	0	0	10	77%	11:00 PM
Lowell Blvd	Dead End	W 62nd Ave	E	0	0	0	0	0	0	0%	
	W 62nd Ave	Dead End	W	0	0	0	0	0	0	0%	
W 63rd Ave	Federal Blvd	Julian St	N	33	10	0	0	0	10	30%	5:00 PM
	Julian St	Federal Blvd	S	84	25	0	0	0	25	30%	11:00 PM
W 64th Ave	Knox St	Lowell Blvd	N	0	0	0	0	0	0	0%	
Total (Weekend Composite)				154	54	0	0	0	54	35%	11:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekend parking demand per block face for all collection periods.

Figure 26: Aloha Beach Heat Map of Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

ARLINGTON MEADOWS

A total of 16 block faces were surveyed across four streets.

INVENTORY

Walker staff observed a total of approximately 113 public on-street parking spaces along all corridors identified for study in Arlington Meadows. The table below summarizes parking supply by street and block face:

Table 12: Arlington Meadows Observed Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
Stuart St	W 62nd Ave	Raleigh St	S	10	0	0	10
Tennyson St	W 62nd Ave	W 63rd Ave	E	8	0	0	8
	W 63rd Ave	W 62nd Ave	W	0	0	0	0
	W 63rd Ave	W 63rd Dr	E	9	0	0	9
	W 63rd Dr	W 63rd Ave	W	0	0	0	0
	W 63rd Dr	W 64th Ave	E	0	0	0	0
	W 64th Ave	W 63rd Dr	W	0	0	0	0
W 62nd Ave	Stuart St	Tennyson St	N	16	0	0	16
	Tennyson St	Stuart St	S	16	0	0	16
W 62nd Pl	Osceola Way	Perry St	N	6	0	0	6
	Perry St	Osceola Way	S	14	0	0	14
	Perry St	Quitman St	N	4	0	0	4
	Quitman St	Perry St	S	10	0	0	10
	Quitman St	Raleigh St	N	7	0	0	7
	Raleigh St	Quitman St	S	7	0	0	7
	Raleigh St	W 62nd Ave	N	6	0	0	6
Total On Street Parking				113	0	0	113

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the Arlington Meadows neighborhood was performed on Wednesday, August 28, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. Parking demand for the neighborhood ranged from 15 vehicles (13% occupied) at 11 AM to the system-wide peak of 30 vehicles (27% occupied) at 11 PM.

The table below summarizes parking occupancy by user group per collection period for the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

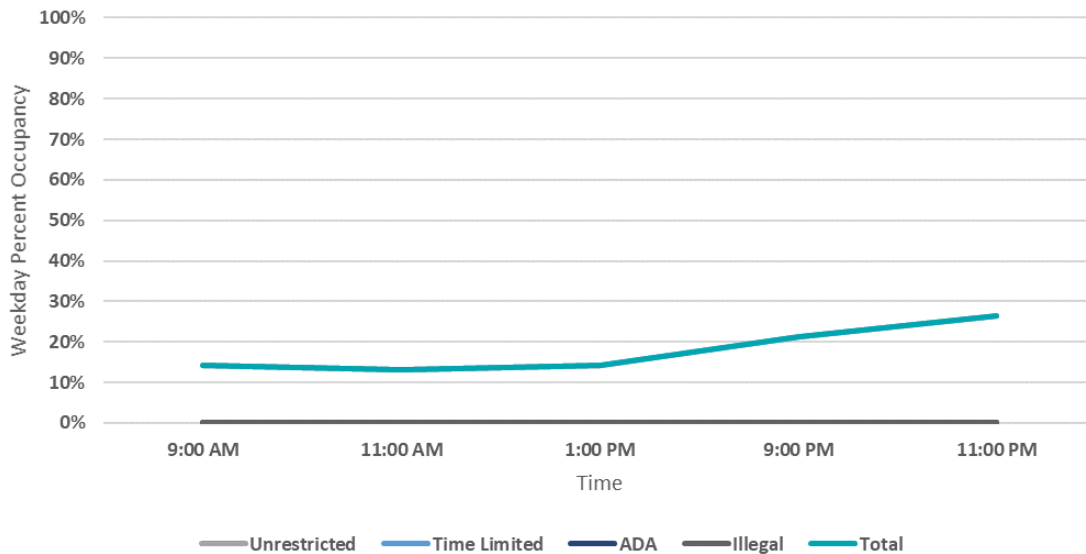
Table 13: Arlington Meadows Observed Weekday Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	113	16	14%	0	0%	0	0%	0	0%	16	14%
11:00 AM	113	15	13%	0	0%	0	0%	0	0%	15	13%
1:00 PM	113	16	14%	0	0%	0	0%	0	0%	16	14%
9:00 PM	113	24	21%	0	0%	0	0%	0	0%	24	21%
11:00 PM	113	30	27%	0	0%	0	0%	0	0%	30	27%
Average	113	20	18%	0	0%	0	0%	0	0%	20	18%

Source: Walker Consultants

The figure below shows total weekday parking occupancy per collection period for the Arlington Meadows neighborhood.

Figure 27: Arlington Meadows Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 11 PM, demand by block face ranged from 0% occupied (multiple segments) to 100% occupied on Stuart Street between 62nd Avenue and Raleigh Street. Eight block faces experienced occupancies of 0% during the system-wide weekday peak and one block face that was 85% occupied or above. While not yet exceeding it, 62nd Place between Raleigh and 62nd Avenue is approaching effective supply, observed at 83% occupancy during the system-wide peak. Parking demand during the system-wide weekday peak was generally concentrated along Stuart Street and 62nd Place. No vehicles were observed parked along Tennyson Street throughout the study period.

No illegally parked vehicles were observed in the Arlington Meadows neighborhood during the weekday.

The following table summarizes parking occupancy by block face during the system-wide peak of 11 PM.

Table 14: Arlington Meadows Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					Weekday % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal	Total Weekday Demand (System-wide Peak)	
Stuart St	W 62nd Ave	Raleigh St	S	10	10	0	0	0	10	100%
Tennyson St	W 62nd Ave	W 63rd Ave	E	8	0	0	0	0	0	0%
	W 63rd Ave	W 62nd Ave	W	0	0	0	0	0	0	0%
	W 63rd Ave	W 63rd Dr	E	9	0	0	0	0	0	0%
	W 63rd Dr	W 63rd Ave	W	0	0	0	0	0	0	0%
	W 63rd Dr	W 64th Ave	E	0	0	0	0	0	0	0%
W 62nd Ave	W 64th Ave	W 63rd Dr	W	0	0	0	0	0	0	0%
	Stuart St	Tennyson St	N	16	2	0	0	0	3	13%
W 62nd Pl	Tennyson St	Stuart St	S	16	2	0	0	0	2	13%
	Osceola Way	Perry St	N	6	0	0	0	0	0	0%
	Perry St	Osceola Way	S	14	0	0	0	0	0	0%
	Perry St	Quitman St	N	4	2	0	0	0	2	50%
	Quitman St	Perry St	S	10	3	0	0	0	3	30%
	Quitman St	Raleigh St	N	7	1	0	0	0	1	14%
Raleigh St	Raleigh St	Quitman St	S	7	4	0	0	0	4	57%
	Raleigh St	W 62nd Ave	N	6	5	0	0	0	5	83%
Total (Weekday System-wide Peak)				113	29	0	0	0	30	27%

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (11 PM) are highlighted in blue. Block faces with 0% occupancy across all times are highlighted in green.

Two block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. Both of these occurred along 62nd Place. For the weekday, the overall composite peak was approximately 2% higher than the system peak.

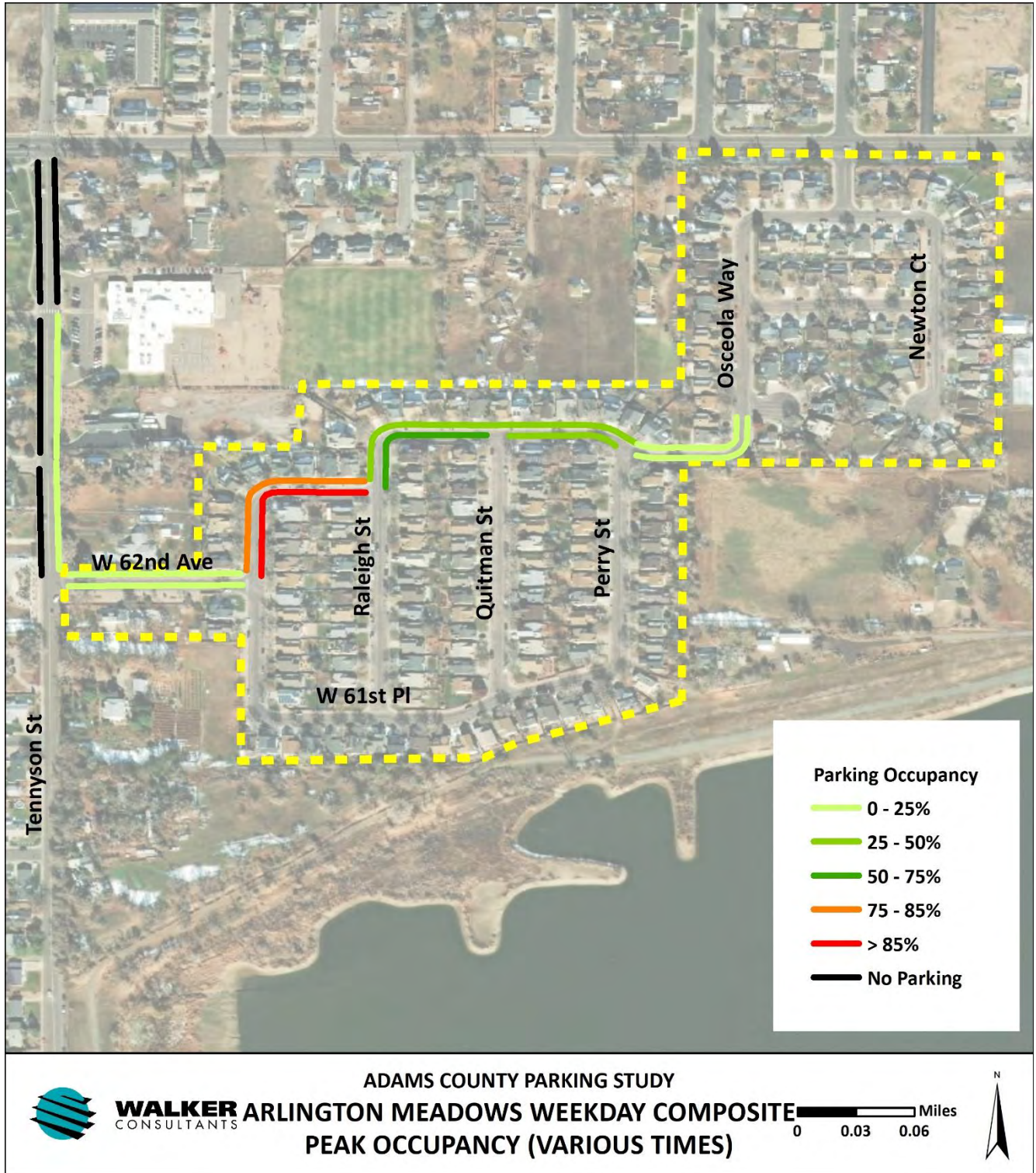
Table 15: Arlington Meadows Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)							
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Weekday Peak % Occupancy (Across All Times)	Time that Peak Occurred	
Stuart St	W 62nd Ave	Raleigh St	S	10	10	0	0	0	0	10	100%	11:00 PM
Tennyson St	W 62nd Ave	W 63rd Ave	E	8	0	0	0	0	0	0	0%	
	W 63rd Ave	W 62nd Ave	W	0	0	0	0	0	0	0	0%	
	W 63rd Ave	W 63rd Dr	E	9	0	0	0	0	0	0	0%	
	W 63rd Dr	W 63rd Ave	W	0	0	0	0	0	0	0	0%	
	W 63rd Dr	W 64th Ave	E	0	0	0	0	0	0	0	0%	
W 62nd Ave	W 64th Ave	W 63rd Dr	W	0	0	0	0	0	0	0	0%	
	Stuart St	Tennyson St	N	16	2	0	0	0	0	3	13%	11:00 PM
W 62nd Pl	Tennyson St	Stuart St	S	16	2	0	0	0	0	2	13%	11:00 PM
	Osceola Way	Perry St	N	6	0	0	0	0	0	0	0%	
	Perry St	Osceola Way	S	14	2	0	0	0	0	2	14%	11:00 AM
	Perry St	Quitman St	N	4	2	0	0	0	0	2	50%	11:00 PM
	Quitman St	Perry St	S	10	3	0	0	0	0	3	30%	11:00 PM
	Quitman St	Raleigh St	N	7	2	0	0	0	0	2	29%	9:00 AM
	Raleigh St	Quitman St	S	7	4	0	0	0	0	4	57%	11:00 PM
Raleigh St	W 62nd Ave	N	6	5	0	0	0	0	5	83%	11:00 PM	
Total (Weekday Composite)				113	32	0	0	0	0	33	29%	11:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekday parking demand by block face for all collection periods.

Figure 28: Arlington Meadows Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the Arlington Meadows neighborhood was performed on Saturday, August 31, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 21 vehicles (19% occupied) at 7 PM to the system-wide peak of 30 vehicles (27% occupied) at 7 PM.

The table below summarizes parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

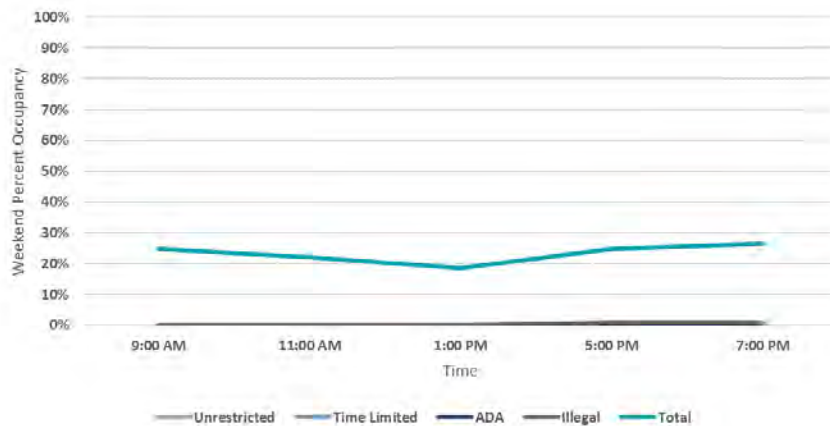
Table 16: Arlington Meadows Observed Weekend Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	113	28	25%	0	0%	0	0%	0	0%	28	25%
11:00 AM	113	25	22%	0	0%	0	0%	0	0%	25	22%
1:00 PM	113	21	19%	0	0%	0	0%	0	0%	21	19%
5:00 PM	113	28	25%	0	0%	0	0%	1	1%	28	25%
7:00 PM	113	30	27%	0	0%	0	0%	1	1%	30	27%
Average	113	26	23%	0	0%	0	0%	0	0%	26	23%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the Arlington Meadows neighborhood.

Figure 29: Arlington Meadows Total Weekend Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekend peak at 7 PM, demand by block face ranged from 0% occupied (multiple segments) to 100% occupied along 63rd Avenue. Overall, there were six segments with occupancies of 0% during the system-wide weekend peak and two segments that were 85% or above. Parking demand during the system-wide weekend peak was largely concentrated along the same streets as during the weekday.

There was one illegally parked vehicle along Tennyson Street between 63rd Drive and 64th Avenue.

The following table summarizes parking occupancy by block face at 7 PM on Saturday.

Table 17: Arlington Meadows Weekend System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					
					Un-restricted	Time Limited	ADA	Illegal	Total Weekend Demand (System-wide Peak)	Weekend % Occupancy (System-wide Peak)
Stuart St	W 62nd Ave	Raleigh St	S	10	10	0	0	0	10	91%
Tennyson St	W 62nd Ave	W 63rd Ave	E	8	0	0	0	0	0	0%
	W 63rd Ave	W 62nd Ave	W	0	0	0	0	0	0	0%
	W 63rd Ave	W 63rd Dr	E	9	0	0	0	0	0	0%
	W 63rd Dr	W 63rd Ave	W	0	0	0	0	0	0	0%
	W 63rd Dr	W 64th Ave	E	0	0	0	0	1	0	0%
W 62nd Ave	W 64th Ave	W 63rd Dr	W	0	0	0	0	0	0	0%
	Stuart St	Tennyson St	N	16	3	0	0	0	3	19%
W 62nd Pl	Tennyson St	Stuart St	S	16	1	0	0	0	1	6%
	Osceola Way	Perry St	N	6	1	0	0	0	1	17%
	Perry St	Osceola Way	S	14	1	0	0	0	1	7%
	Perry St	Quitman St	N	4	2	0	0	0	2	50%
	Quitman St	Perry St	S	10	4	0	0	0	4	40%
	Quitman St	Raleigh St	N	7	1	0	0	0	1	14%
	Raleigh St	Quitman St	S	7	1	0	0	0	1	14%
Raleigh St	W 62nd Ave	N	6	6	0	0	0	6	100%	
Total (Weekend System-wide Peak)				113	30	0	0	1	30	27%

Source: Walker Consultant

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (7 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

Four block faces were observed with individual peak occupancies greater than they exhibited during the system-wide peak. Three of these four block faces occur along 62nd Place. For the weekend, the overall composite peak was approximately 7% higher than the system peak.

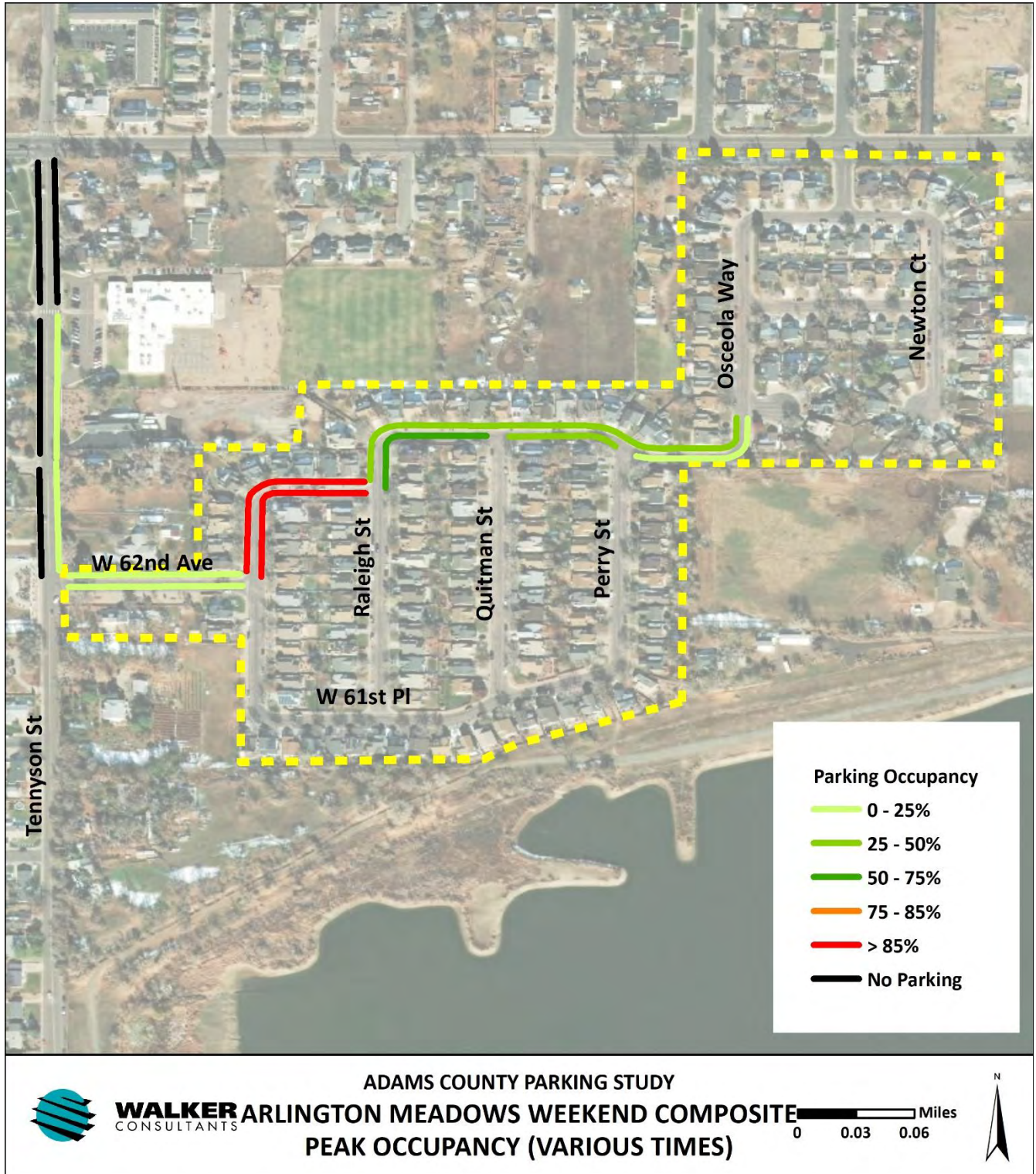
Table 18: Arlington Meadows Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)							
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)	Time that Peak Occurred	
Stuart St	W 62nd Ave	Raleigh St	S	10	10	0	0	0	0	10	91%	7:00 PM
Tennyson St	W 62nd Ave	W 63rd Ave	E	8	0	0	0	0	0	0	0%	
	W 63rd Ave	W 62nd Ave	W	0	0	0	0	0	0	0	0%	
	W 63rd Ave	W 63rd Dr	E	9	0	0	0	0	0	0	0%	
	W 63rd Dr	W 63rd Ave	W	0	0	0	0	0	0	0	0%	
	W 63rd Dr	W 64th Ave	E	0	0	0	0	0	0	0	0%	
	W 64th Ave	W 63rd Dr	W	0	0	0	0	0	0	0	0%	
W 62nd Ave	Stuart St	Tennyson St	N	16	3	0	0	0	0	3	19%	7:00 PM
	Tennyson St	Stuart St	S	16	3	0	0	0	0	3	19%	9:00 AM
W 62nd Pl	Osceola Way	Perry St	N	6	2	0	0	0	0	2	33%	5:00 PM
	Perry St	Osceola Way	S	14	1	0	0	0	0	1	7%	7:00 PM
	Perry St	Quitman St	N	4	2	0	0	0	0	2	50%	7:00 PM
	Quitman St	Perry St	S	10	4	0	0	0	0	4	40%	7:00 PM
	Quitman St	Raleigh St	N	7	3	0	0	0	0	3	43%	1:00 PM
	Raleigh St	Quitman St	S	7	4	0	0	0	0	4	57%	5:00 PM
	Raleigh St	W 62nd Ave	N	6	6	0	0	0	0	6	100%	7:00 PM
Total (Weekend Composite)				113	38	0	0	0	0	38	34%	11:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekend parking demand per block face across all collection periods.

Figure 30: Arlington Meadows Heat Map of Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

BERKELEY

A total of 56 block faces were surveyed across 12 streets, all immediately to the west, north, and east of Regis University.

INVENTORY

Walker staff observed a total of approximately 629 public on-street parking spaces along all corridors identified for study in the Berkeley neighborhood. The table below summarizes parking supply by street and block face:

Table 19: Berkeley Observed Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
Grove St	Dead End	W 53rd Ave	W	18	0	0	18
	W 53rd Ave	Dead End	E	0	0	0	0
	W 53rd Ave	W 54th Ave	W	0	0	0	0
	W 54th Ave	W 53rd Ave	E	0	0	0	0
Irving St	W 53rd Ave	W 54th Ave	W	6	0	0	6
	W 54th Ave	W 53rd Ave	E	13	0	0	13
Julian St	Cul de sac	W 53rd Ave	E	15	0	0	15
	W 53rd Ave	Cul de sac	W	12	0	0	12
	W 53rd Ave	W 54th Ave	E	9	0	0	9
	W 54th Ave	W 53rd Ave	W	12	0	0	12
King Ct	Cul de sac	W 54th Ave	E	20	0	0	20
	W 54th Ave	Cul de sac	W	17	0	0	17
King St	Cul de sac	W 53rd Ave	E	7	0	0	7
	W 53rd Ave	Cul de sac	W	3	0	0	3
	W 53rd Ave	W 54th Ave	E	4	0	0	4
	W 54th Ave	W 53rd Ave	W	12	0	0	12
Lowell Blvd	W 53rd Ave	W 52nd Ave	W	17	0	0	17
	W 53rd Ave	W 54th Ave	E	10	0	0	10
	W 54th Ave	W 53rd Ave	W	0	0	0	0
Meade St	W 52nd Ave	W 53rd Ave	E	17	0	0	17
	W 53rd Ave	W 52nd Ave	W	16	0	0	16
	W 53rd Ave	W 54th Ave	E	24	0	0	24
	W 54th Ave	W 53rd Ave	W	23	0	0	23
Newton St	W 52nd Ave	W 53rd Ave	E	17	0	0	17
	W 53rd Ave	W 52nd Ave	W	15	0	0	15
	W 53rd Ave	W 54th Ave	E	18	0	0	18
Osceola St	W 54th Ave	W 53rd Ave	W	19	0	0	19
	Dead End	W 53rd Ave	W	26	0	0	26
	W 52nd Ave	W 53rd Ave	E	19	0	0	19
	W 53rd Ave	Dead End	E	22	0	0	22
W 52nd Ave	W 53rd Ave	W 52nd Ave	W	13	0	0	13
	Meade St	Lowell Blvd	S	9	0	0	9
	Newton St	Meade St	S	10	0	0	10
W 53rd Ave	Osceola St	Newton St	S	8	0	0	8
	Grove St	Irving St	S	13	0	0	13
	Irving St	Grove St	N	0	0	0	0
	Irving St	Julian St	N	5	0	0	5
	Julian St	Irving St	S	5	0	0	5
	Julian St	King St	N	8	0	0	8
	King St	Julian St	S	6	0	0	6
	King St	Lowell Blvd	N	14	0	0	14
	Lowell Blvd	King St	S	6	0	0	6
	Lowell Blvd	Meade St	N	9	0	0	9
	Meade St	Lowell Blvd	S	9	0	0	9
	Meade St	Newton St	N	8	0	0	8
	Newton St	Meade St	S	10	0	0	10
	Newton St	Osceola St	N	10	0	0	10
	Osceola St	Newton St	S	11	0	0	11
	W 54th Ave	Julian St	Irving St	S	16	0	0
King Ct		King St	S	4	0	0	4
King St		Julian St	S	17	0	0	17
Lowell Blvd		King Ct	S	7	0	0	7
Lowell Blvd		Meade St	N	10	0	0	10
Meade St		Lowell Blvd	S	12	0	0	12
Meade St		Newton St	N	8	0	0	8
Newton St		Meade St	S	10	0	0	10
Total On Street Parking				629	0	0	629

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the Berkeley neighborhood was performed on Wednesday, August 28, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. Parking demand for the neighborhood ranged from 155 vehicles (25% occupied) at 9 PM to the system-wide peak of 208 vehicles (33% occupied) at 9 AM.

The table below summarizes parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

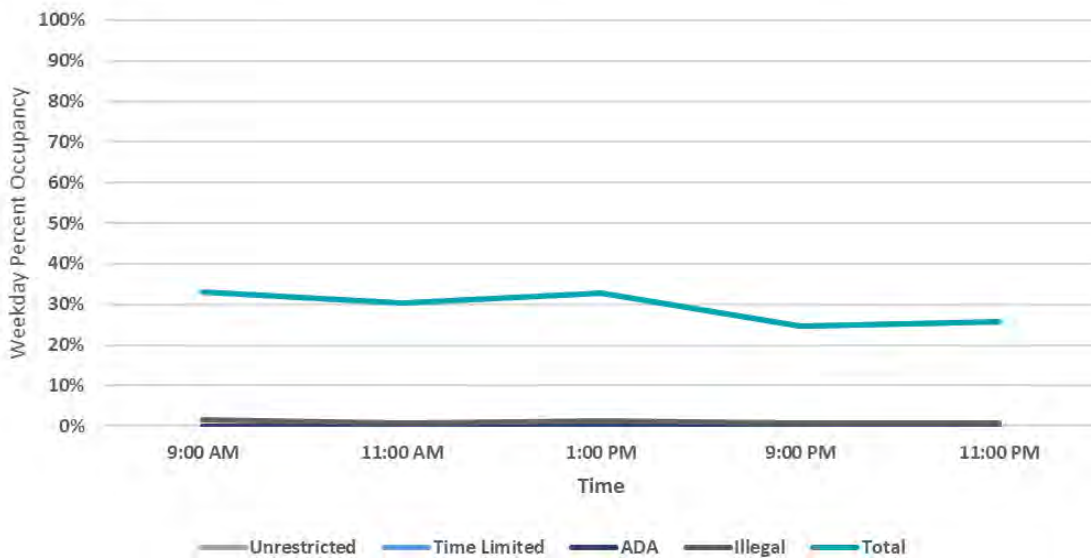
Table 20: Berkeley Observed Weekday Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	629	208	33%	0	0%	0	0%	9	1%	208	33%
11:00 AM	629	190	30%	0	0%	0	0%	4	1%	190	30%
1:00 PM	629	206	33%	0	0%	0	0%	8	1%	206	33%
9:00 PM	629	155	25%	0	0%	0	0%	5	1%	155	25%
11:00 PM	629	161	26%	0	0%	0	0%	4	1%	161	26%
Average	629	184	29%	0	0%	0	0%	6	1%	184	29%

Source: Walker Consultants

The figure below shows total weekday parking occupancy by time for the Berkeley neighborhood.

Figure 31: Berkeley Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 9 AM, demand by block face ranged from 0% occupied (multiple segments) to 100% occupied along all three south-side segments of 52nd Avenue studied (no parking is allowed on the north side of 52nd Avenue). Overall, there were 14 block faces with occupancies of 0% during the system-wide weekday peak and five block faces that were 85% or above. An additional four block faces were observed between 75% and 85% occupied. Block faces observed between 75% and 85% occupied were located along Meade Street between 52nd Avenue and 53rd Avenue, 53rd Avenue between Lowell Boulevard and King Street, 53rd Avenue between Julian Street and Irving Street, and the both sides of King Street between 53rd Avenue and the cul-de-sac.

Nine illegally parked vehicles were observed during the system-wide peak. These vehicles were observed at:

- Lowell Boulevard between 52nd Avenue and 53rd Avenue, Westside (1 vehicle)
- Meade Street between 53rd Avenue and 54th Avenue, Westside (1 vehicle)
- Newton Street between 52nd Avenue and 53rd Avenue, both sides (2 vehicles)
- Osceola Street between 52nd Avenue and 53rd Avenue, Westside (1 vehicle)
- 52nd Avenue between Lowell Boulevard and Meade Street, Southside (1 vehicle)
- 53rd Avenue between Julian Street and Irving Street, Northside (1 vehicle)
- 53rd Avenue between Lowell Boulevard and King Street, Southside (2 vehicles)

The following table summarizes parking occupancy by block face at 9 AM during the system-wide weekday peak.



Table 21: Berkeley Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					Weekday % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal	Total Weekday Demand (System-wide Peak)	
Grove St	Dead End	W 53rd Ave	W	18	4	0	0	0	4	22%
	W 53rd Ave	Dead End	E	0	0	0	0	0	0	0%
	W 53rd Ave	W 54th Ave	W	0	0	0	0	0	0	0%
Irving St	W 54th Ave	W 53rd Ave	E	0	0	0	0	0	0	0%
	W 53rd Ave	W 54th Ave	W	6	1	0	0	0	1	17%
Julian St	W 54th Ave	W 53rd Ave	E	13	0	0	0	0	0	0%
	Cul de sac	W 53rd Ave	E	15	3	0	0	0	3	20%
	W 53rd Ave	Cul de sac	W	12	10	0	0	0	10	83%
King Ct	W 53rd Ave	W 54th Ave	E	9	0	0	0	0	0	0%
	W 54th Ave	W 53rd Ave	W	12	3	0	0	0	3	25%
	Cul de sac	W 54th Ave	E	20	7	0	0	0	7	35%
King St	W 54th Ave	Cul de sac	W	17	3	0	0	0	3	18%
	Cul de sac	W 53rd Ave	E	7	6	0	0	0	6	86%
	W 53rd Ave	Cul de sac	W	3	1	0	0	0	1	33%
Lowell Blvd	W 53rd Ave	W 54th Ave	E	4	0	0	0	0	0	0%
	W 54th Ave	W 53rd Ave	W	12	0	0	0	0	0	0%
	W 53rd Ave	W 52nd Ave	W	17	13	0	0	1	13	76%
Meade St	W 53rd Ave	W 54th Ave	E	10	4	0	0	0	4	40%
	W 54th Ave	W 53rd Ave	W	0	0	0	0	0	0	0%
	W 52nd Ave	W 53rd Ave	E	17	16	0	0	0	16	94%
	W 53rd Ave	W 52nd Ave	W	16	0	0	0	0	0	0%
Newton St	W 53rd Ave	W 54th Ave	E	24	2	0	0	0	2	8%
	W 54th Ave	W 53rd Ave	W	23	2	0	0	1	2	9%
	W 52nd Ave	W 53rd Ave	E	17	9	0	0	1	9	53%
	W 53rd Ave	W 52nd Ave	W	15	4	0	0	1	4	27%
Osceola St	W 53rd Ave	W 54th Ave	E	18	6	0	0	0	6	33%
	W 54th Ave	W 53rd Ave	W	19	9	0	0	0	9	47%
	Dead End	W 53rd Ave	W	26	5	0	0	0	5	19%
W 52nd Ave	W 52nd Ave	W 53rd Ave	E	19	4	0	0	0	4	21%
	W 53rd Ave	Dead End	E	22	4	0	0	0	4	18%
	W 53rd Ave	W 52nd Ave	W	13	2	0	0	1	2	15%
W 53rd Ave	Meade St	Lowell Blvd	S	9	9	0	0	1	9	100%
	Newton St	Meade St	S	10	10	0	0	0	10	100%
	Osceola St	Newton St	S	8	8	0	0	0	8	100%
W 53rd Ave	Grove St	Irving St	S	13	0	0	0	0	0	0%
	Irving St	Grove St	N	0	0	0	0	0	0	0%
	Irving St	Julian St	N	5	2	0	0	1	2	40%
	Julian St	Irving St	S	5	3	0	0	0	3	60%
	Julian St	King St	N	8	5	0	0	0	5	63%
	King St	Julian St	S	6	1	0	0	0	1	17%
	King St	Lowell Blvd	N	14	8	0	0	0	8	57%
	Lowell Blvd	King St	S	6	4	0	0	2	4	67%
	Lowell Blvd	Meade St	N	9	7	0	0	0	7	78%
	Meade St	Lowell Blvd	S	9	7	0	0	0	7	78%
	Meade St	Newton St	N	8	4	0	0	0	4	50%
	Newton St	Meade St	S	10	4	0	0	0	4	40%
W 54th Ave	Newton St	Osceola St	N	10	2	0	0	0	2	20%
	Osceola St	Newton St	S	11	3	0	0	0	3	27%
	Julian St	Irving St	S	16	2	0	0	0	2	13%
	King Ct	King St	S	4	1	0	0	0	1	25%
	King St	Julian St	S	17	0	0	0	0	0	0%
	Lowell Blvd	King Ct	S	7	2	0	0	0	2	29%
	Lowell Blvd	Meade St	N	10	0	0	0	0	0	0%
	Meade St	Lowell Blvd	S	12	0	0	0	0	0	0%
Total (Weekday System-wide Peak)				629	208	0	0	9	208	33%

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (9 AM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

25 block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. For the weekday, the overall composite peak was approximately 8% higher than the system-wide peak.

Virtually all of the block faces with individual peaks greater than 9 AM levels of demand occurred at 9 PM. Because demand decreased for these areas between 9 and 11 PM, it is likely that at least some of these demand peaks, including Newton Street and Osceola Street, were due to overflow parking from Regis University.

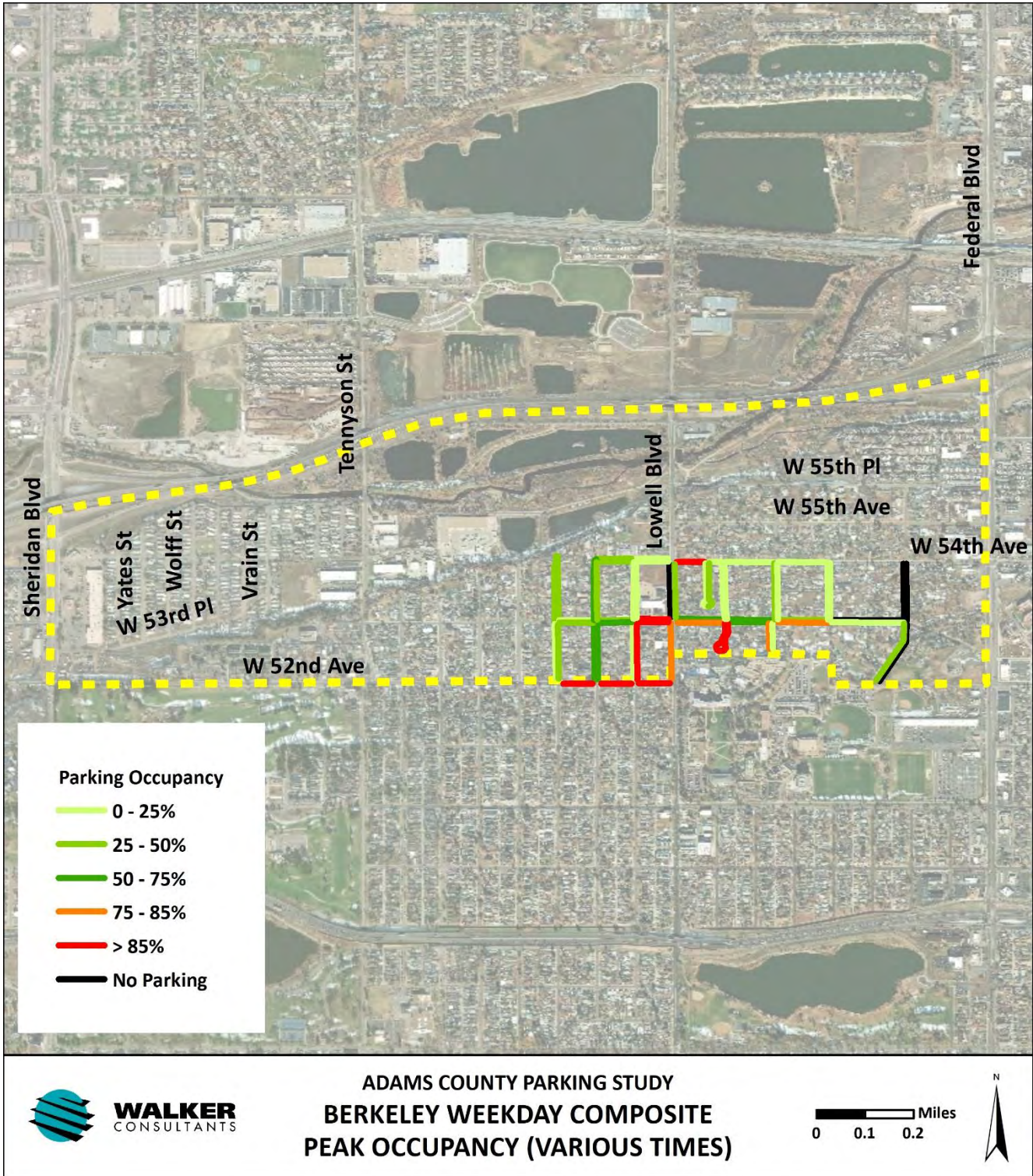
Table 22: Berkeley Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Weekday Peak % Occupancy (Across All Times)	Time that Peak Occurred
Grove St	Dead End	W 53rd Ave	W	18	6	0	0	0	6	33%	1:00:00 PM
	W 53rd Ave	Dead End	E	0	0	0	0	0	0	0%	
	W 53rd Ave	W 54th Ave	W	0	0	0	0	0	0	0%	
Irving St	W 54th Ave	W 53rd Ave	E	0	0	0	0	0	0	0%	
	W 53rd Ave	W 54th Ave	W	6	1	0	0	0	1	17%	9:00:00 AM
	W 54th Ave	W 53rd Ave	E	13	0	0	0	0	0	0%	
Julian St	Cul de sac	W 53rd Ave	E	15	3	0	0	0	3	20%	9:00:00 AM
	W 53rd Ave	Cul de sac	W	12	10	0	0	0	10	83%	9:00:00 AM
	W 53rd Ave	W 54th Ave	E	9	1	0	0	0	1	11%	11:00:00 PM
King Ct	W 54th Ave	W 53rd Ave	W	12	4	0	0	1	4	33%	11:00:00 PM
	Cul de sac	W 54th Ave	E	20	9	0	0	0	9	45%	9:00:00 PM
	W 54th Ave	Cul de sac	W	17	4	0	0	0	4	24%	9:00:00 PM
King St	Cul de sac	W 53rd Ave	E	7	6	0	0	0	6	86%	9:00:00 AM
	W 53rd Ave	Cul de sac	W	3	3	0	0	0	3	100%	11:00:00 AM
	W 53rd Ave	W 54th Ave	E	4	1	0	0	0	1	25%	9:00:00 PM
Lowell Blvd	W 54th Ave	W 53rd Ave	W	12	0	0	0	0	0	0%	
	W 53rd Ave	W 52nd Ave	W	17	13	0	0	1	13	76%	9:00:00 AM
	W 53rd Ave	W 54th Ave	E	10	4	0	0	0	4	40%	9:00:00 AM
Meade St	W 54th Ave	W 53rd Ave	W	0	0	0	0	0	0	0%	
	W 52nd Ave	W 53rd Ave	E	17	16	0	0	0	16	94%	9:00:00 AM
	W 53rd Ave	W 52nd Ave	W	16	0	0	0	0	0	0%	
Newton St	W 53rd Ave	W 54th Ave	E	24	2	0	0	0	2	8%	9:00:00 AM
	W 54th Ave	W 53rd Ave	W	23	4	0	0	0	4	17%	1:00:00 PM
	W 52nd Ave	W 53rd Ave	E	17	10	0	0	1	10	59%	1:00:00 PM
Osceola St	W 53rd Ave	W 52nd Ave	W	15	9	0	0	1	9	60%	9:00:00 PM
	W 53rd Ave	W 54th Ave	E	18	7	0	0	0	7	39%	9:00:00 PM
	W 54th Ave	W 53rd Ave	W	19	11	0	0	0	11	58%	9:00:00 PM
W 52nd Ave	Dead End	W 53rd Ave	W	26	9	0	0	0	9	35%	9:00:00 PM
	W 52nd Ave	W 53rd Ave	E	19	5	0	0	0	5	26%	9:00:00 PM
	W 53rd Ave	Dead End	E	22	7	0	0	0	7	32%	9:00:00 PM
W 53rd Ave	W 53rd Ave	W 52nd Ave	W	13	3	0	0	0	3	23%	11:00:00 PM
	Meade St	Lowell Blvd	S	9	9	0	0	1	9	100%	9:00:00 AM
	Newton St	Meade St	S	10	10	0	0	0	10	100%	9:00:00 AM
	Osceola St	Newton St	S	8	8	0	0	0	8	100%	9:00:00 AM
	Grove St	Irving St	S	13	1	0	0	0	1	8%	9:00:00 PM
	Irving St	Grove St	N	0	0	0	0	0	0	0%	
	Irving St	Julian St	N	5	2	0	0	1	2	40%	9:00:00 AM
	Julian St	Irving St	S	5	4	0	0	0	4	80%	11:00:00 AM
	Julian St	King St	N	8	5	0	0	0	5	63%	9:00:00 AM
	King St	Julian St	S	6	4	0	0	0	4	67%	11:00:00 AM
	King St	Lowell Blvd	N	14	8	0	0	0	8	57%	9:00:00 AM
	Lowell Blvd	King St	S	6	5	0	0	1	5	83%	11:00:00 AM
	Lowell Blvd	Meade St	N	9	8	0	0	0	8	89%	11:00:00 AM
	Meade St	Lowell Blvd	S	9	8	0	0	0	8	89%	11:00:00 AM
	Meade St	Newton St	N	8	4	0	0	0	4	50%	9:00:00 AM
	W 54th Ave	Newton St	Meade St	S	10	7	0	0	0	7	70%
Newton St		Osceola St	N	10	2	0	0	0	2	20%	9:00:00 AM
Osceola St		Newton St	S	11	3	0	0	0	3	27%	9:00:00 AM
Julian St		Irving St	S	16	2	0	0	0	2	13%	9:00:00 AM
King Ct		King St	S	4	1	0	0	0	1	25%	9:00:00 AM
King St		Julian St	S	17	2	0	0	0	2	12%	11:00:00 PM
W 54th Ave	Lowell Blvd	King Ct	S	7	6	0	0	0	6	86%	9:00:00 PM
	Lowell Blvd	Meade St	N	10	0	0	0	0	0	0%	
	Meade St	Lowell Blvd	S	12	1	0	0	0	1	8%	1:00:00 PM
	Meade St	Newton St	N	8	4	0	0	0	4	50%	9:00:00 AM
	Newton St	Meade St	S	10	4	0	0	0	4	40%	9:00:00 AM
Total (Weekday Composite)				629	256	0	0	7	256	41%	9:00 AM

Source: Walker Consultants

The following heat map is a composite of peak weekday parking demand per block face across all collection periods.

Figure 32: Berkeley Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the Berkeley neighborhood was performed on Sunday September 8, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 100 vehicles (16% occupied) at 7 PM to the system-wide peak of 137 vehicles (22% occupied) at 9 AM and 11 AM.

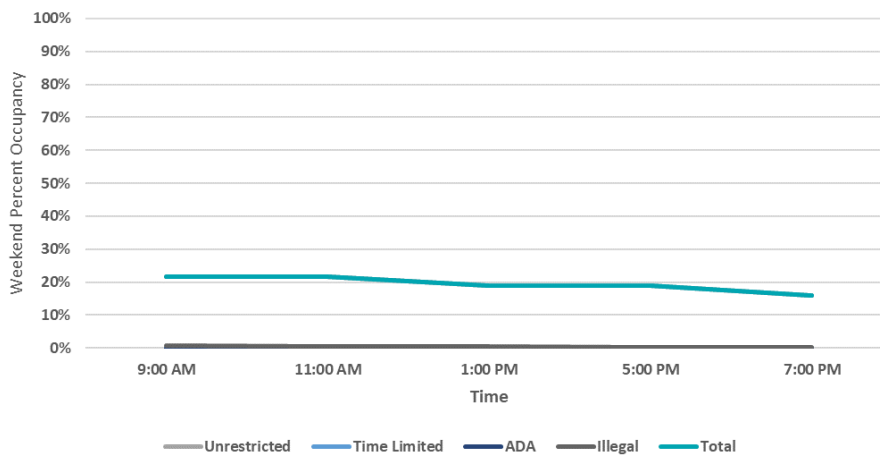
The table below shows parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

Table 23: Berkeley Observed Weekend Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	629	137	22%	0	0%	0	0%	4	1%	137	22%
11:00 AM	629	137	22%	0	0%	0	0%	3	0%	137	22%
1:00 PM	629	119	19%	0	0%	0	0%	3	0%	119	19%
5:00 PM	629	119	19%	0	0%	0	0%	2	0%	119	19%
7:00 PM	629	100	16%	0	0%	0	0%	2	0%	100	16%
Average	629	122	19%	0	0%	0	0%	3	0%	122	19%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the Berkeley neighborhood.

Figure 33: Berkeley Total Weekend Parking Occupancy by Percent


Source: Walker Consultants

During the system-wide weekend peak at 9 and 11 AM, demand by block face ranged from 0% occupied (eight segments) to 65% occupied along Meade Street between 52nd Avenue and 53rd Avenue. There were no block faces observed to be at or above 85% occupied. Parking demand during the system-wide weekday peak was

largely concentrated along the same streets as observed during the weekday, aside from the south side of 52nd Avenue which decreased dramatically. This is likely attributable to weekday commuter parking demands related to the University.

There were four illegally parked vehicles observed during weekend collections. These vehicles were parked on the following segments:

- Grove Street between 53rd Avenue and the cul-de-sac (2 vehicles)
- Newton Street between 52nd Avenue and 53rd Avenue, Westside (1 vehicle)
- 53rd Avenue between Lowell Boulevard and King Street, Southside (1 vehicle)

The following table summarizes parking occupancy by block face at 9 AM on Sunday.



Table 24: Berkeley Weekend System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					Weekend % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal	Total Weekend Demand (System-wide Peak)	
Grove St	Dead End	W 53rd Ave	W	18	3	0	0	0	3	17%
	W 53rd Ave	Dead End	E	0	0	0	0	2	0	0%
	W 53rd Ave	W 54th Ave	W	0	0	0	0	0	0	0%
Irving St	W 54th Ave	W 53rd Ave	E	0	0	0	0	0	0	0%
	W 53rd Ave	W 54th Ave	W	6	0	0	0	0	0	0%
Julian St	W 54th Ave	W 53rd Ave	E	13	1	0	0	0	1	8%
	Cul de sac	W 53rd Ave	E	15	2	0	0	0	2	13%
	W 53rd Ave	Cul de sac	W	12	6	0	0	0	6	50%
	W 53rd Ave	W 54th Ave	E	9	1	0	0	0	1	11%
King Ct	W 54th Ave	W 53rd Ave	W	12	5	0	0	0	5	42%
	Cul de sac	W 54th Ave	E	20	6	0	0	0	6	30%
King St	W 54th Ave	Cul de sac	W	17	5	0	0	0	5	29%
	Cul de sac	W 53rd Ave	E	7	2	0	0	0	2	29%
	W 53rd Ave	Cul de sac	W	3	1	0	0	0	1	33%
Lowell Blvd	W 53rd Ave	W 54th Ave	E	4	1	0	0	0	1	25%
	W 54th Ave	W 53rd Ave	W	12	1	0	0	0	1	8%
	W 53rd Ave	W 52nd Ave	W	17	4	0	0	0	4	24%
Meade St	W 53rd Ave	W 54th Ave	E	10	1	0	0	0	1	10%
	W 54th Ave	W 53rd Ave	W	0	0	0	0	0	0	0%
	W 52nd Ave	W 53rd Ave	E	17	11	0	0	0	11	65%
	W 53rd Ave	W 52nd Ave	W	16	7	0	0	0	7	44%
Newton St	W 53rd Ave	W 54th Ave	E	24	1	0	0	0	1	4%
	W 54th Ave	W 53rd Ave	W	23	2	0	0	0	2	9%
	W 52nd Ave	W 53rd Ave	E	17	2	0	0	0	2	12%
	W 53rd Ave	W 52nd Ave	W	15	5	0	0	1	5	33%
Osceola St	W 53rd Ave	W 54th Ave	E	18	8	0	0	0	8	44%
	W 54th Ave	W 53rd Ave	W	19	10	0	0	0	10	53%
	Dead End	W 53rd Ave	W	26	8	0	0	0	8	31%
	W 52nd Ave	W 53rd Ave	E	19	4	0	0	0	4	21%
W 52nd Ave	W 53rd Ave	Dead End	E	22	1	0	0	0	1	5%
	W 53rd Ave	W 52nd Ave	W	13	3	0	0	0	3	23%
	Meade St	Lowell Blvd	S	9	4	0	0	0	4	44%
W 53rd Ave	Newton St	Meade St	S	10	2	0	0	0	2	20%
	Osceola St	Newton St	S	8	1	0	0	0	1	13%
	Grove St	Irving St	S	13	0	0	0	0	0	0%
	Irving St	Grove St	N	0	0	0	0	0	0	0%
	Irving St	Julian St	N	5	1	0	0	0	1	20%
	Julian St	Irving St	S	5	1	0	0	0	1	20%
	Julian St	King St	N	8	1	0	0	0	1	13%
	King St	Julian St	S	6	1	0	0	0	1	17%
	King St	Lowell Blvd	N	14	1	0	0	0	1	7%
	Lowell Blvd	King St	S	6	0	0	0	1	0	0%
	Lowell Blvd	Meade St	N	9	1	0	0	0	1	11%
	Meade St	Lowell Blvd	S	9	4	0	0	0	4	44%
	Meade St	Newton St	N	8	3	0	0	0	3	38%
	Newton St	Meade St	S	10	1	0	0	0	1	10%
	W 54th Ave	Newton St	Osceola St	N	10	1	0	0	0	1
Osceola St		Newton St	S	11	2	0	0	0	2	18%
Julian St		Irving St	S	16	2	0	0	0	2	13%
King Ct		King St	S	4	1	0	0	0	1	25%
King St		Julian St	S	17	4	0	0	0	4	24%
Lowell Blvd		King Ct	S	7	1	0	0	0	1	14%
Lowell Blvd		Meade St	N	10	1	0	0	0	1	10%
Meade St		Lowell Blvd	S	12	1	0	0	0	1	8%
Total (Weekend System-wide Peak)				629	137	0	0	4	137	22%

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (9 AM) are highlighted in blue. Block faces with 0% occupancy across all times are highlighted in green.

14 block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. For the weekend, the overall composite peak was approximately 3% higher than the system peak.



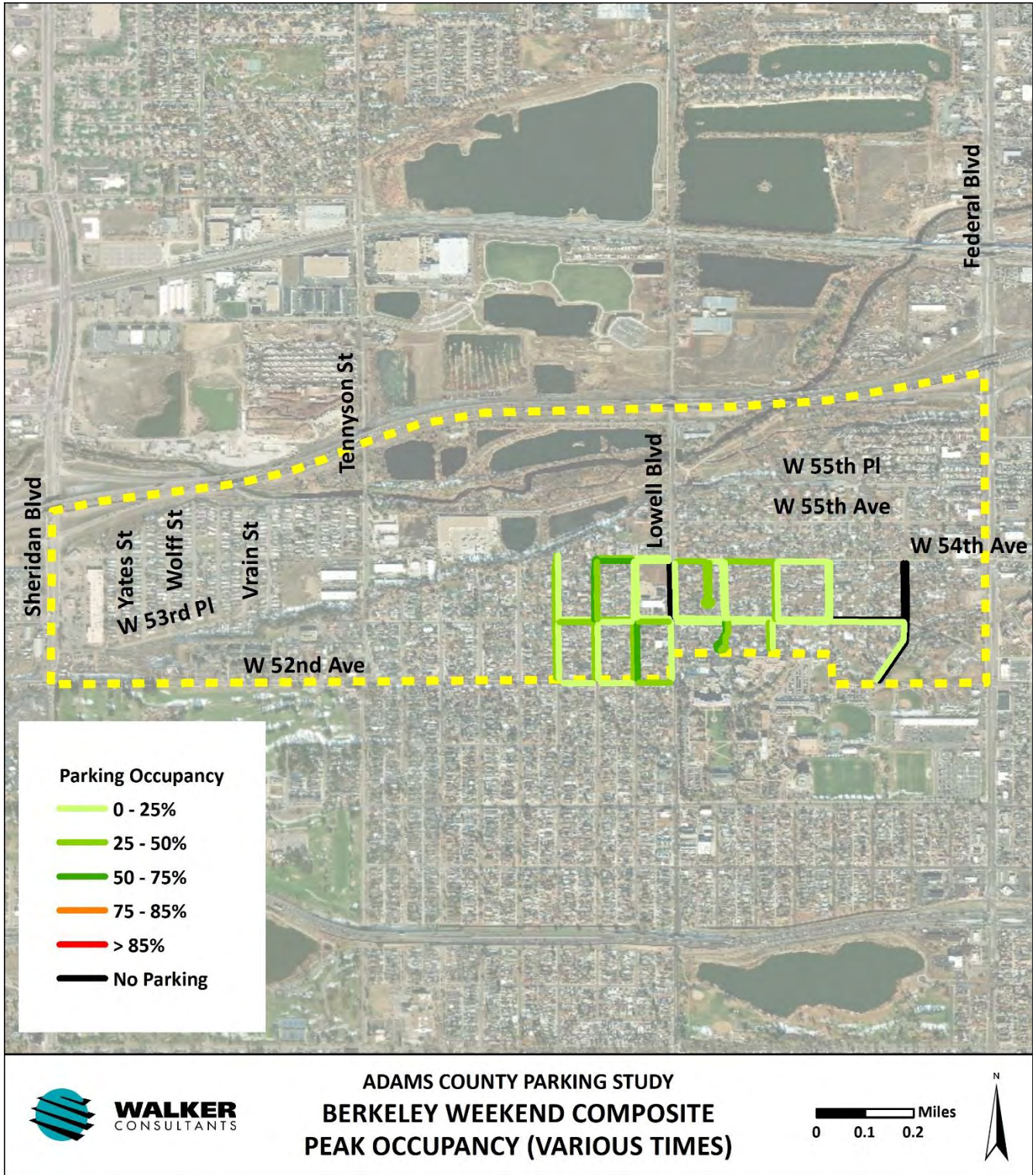
Table 25: Berkeley Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)	Time that Peak Occurred
Grove St	Dead End	W 53rd Ave	W	18	3	0	0	0	3	17%	9:00:00 AM
	W 53rd Ave	Dead End	E	0	0	0	0	2	0	0%	
	W 53rd Ave	W 54th Ave	W	0	0	0	0	0	0	0%	
Irving St	W 54th Ave	W 53rd Ave	E	0	0	0	0	0	0	0%	
	W 53rd Ave	W 54th Ave	W	6	0	0	0	0	0	0%	
	W 54th Ave	W 53rd Ave	E	13	1	0	0	0	1	8%	9:00:00 AM
Julian St	Cul de sac	W 53rd Ave	E	15	2	0	0	0	2	13%	9:00:00 AM
	W 53rd Ave	Cul de sac	W	12	6	0	0	0	6	50%	9:00:00 AM
	W 53rd Ave	W 54th Ave	E	9	2	0	0	0	2	22%	11:00:00 AM
King Ct	W 54th Ave	W 53rd Ave	W	12	5	0	0	0	5	42%	9:00:00 AM
	Cul de sac	W 54th Ave	E	20	6	0	0	0	6	30%	9:00:00 AM
	W 54th Ave	Cul de sac	W	17	5	0	0	0	5	29%	9:00:00 AM
King St	Cul de sac	W 53rd Ave	E	7	3	0	0	0	3	43%	11:00:00 AM
	W 53rd Ave	Cul de sac	W	3	2	0	0	0	2	67%	5:00:00 PM
	W 53rd Ave	W 54th Ave	E	4	1	0	0	0	1	25%	9:00:00 AM
Lowell Blvd	W 54th Ave	W 53rd Ave	W	12	1	0	0	0	1	8%	9:00:00 AM
	W 53rd Ave	W 52nd Ave	W	17	4	0	0	0	4	24%	9:00:00 AM
	W 53rd Ave	W 54th Ave	E	10	1	0	0	0	1	10%	9:00:00 AM
Meade St	W 54th Ave	W 53rd Ave	W	0	0	0	0	0	0	0%	
	W 52nd Ave	W 53rd Ave	E	17	11	0	0	0	11	65%	9:00:00 AM
	W 53rd Ave	W 52nd Ave	W	16	8	0	0	0	8	50%	11:00:00 AM
Newton St	W 53rd Ave	W 54th Ave	E	24	1	0	0	0	1	4%	9:00:00 AM
	W 54th Ave	W 53rd Ave	W	23	2	0	0	0	2	9%	9:00:00 AM
	W 52nd Ave	W 53rd Ave	E	17	4	0	0	0	4	24%	5:00:00 PM
Osceola St	W 53rd Ave	W 52nd Ave	W	15	5	0	0	1	5	33%	9:00:00 AM
	W 53rd Ave	W 54th Ave	E	18	8	0	0	0	8	44%	9:00:00 AM
	W 54th Ave	W 53rd Ave	W	19	11	0	0	0	11	58%	11:00:00 AM
W 52nd Ave	Dead End	W 53rd Ave	W	26	8	0	0	0	8	31%	9:00:00 AM
	W 52nd Ave	W 53rd Ave	E	19	4	0	0	0	4	21%	9:00:00 AM
	W 53rd Ave	Dead End	E	22	2	0	0	0	2	9%	11:00:00 AM
W 53rd Ave	W 53rd Ave	W 52nd Ave	W	13	4	0	0	0	4	31%	11:00:00 AM
	Meade St	Lowell Blvd	S	9	4	0	0	0	4	44%	9:00:00 AM
	Newton St	Meade St	S	10	2	0	0	0	2	20%	9:00:00 AM
W 53rd Ave	Osceola St	Newton St	S	8	1	0	0	0	1	13%	9:00:00 AM
	Grove St	Irving St	S	13	1	0	0	0	1	8%	11:00:00 AM
	Irving St	Grove St	N	0	0	0	0	0	0	0%	
	Irving St	Julian St	N	5	1	0	0	0	1	20%	9:00:00 AM
	Julian St	Irving St	S	5	1	0	0	0	1	20%	9:00:00 AM
	Julian St	King St	N	8	1	0	0	0	1	13%	9:00:00 AM
	King St	Julian St	S	6	1	0	0	0	1	17%	9:00:00 AM
	King St	Lowell Blvd	N	14	1	0	0	0	1	7%	9:00:00 AM
	Lowell Blvd	King St	S	6	1	0	0	0	1	17%	5:00:00 PM
	Lowell Blvd	Meade St	N	9	1	0	0	0	1	11%	9:00:00 AM
	Meade St	Lowell Blvd	S	9	4	0	0	0	4	44%	9:00:00 AM
	Meade St	Newton St	N	8	4	0	0	0	4	50%	5:00:00 PM
	Newton St	Meade St	S	10	1	0	0	0	1	10%	9:00:00 AM
	Newton St	Osceola St	N	10	2	0	0	0	2	20%	5:00:00 PM
	Osceola St	Newton St	S	11	3	0	0	0	3	27%	11:00:00 AM
W 54th Ave	Julian St	Irving St	S	16	2	0	0	0	2	13%	9:00:00 AM
	King Ct	King St	S	4	1	0	0	0	1	25%	9:00:00 AM
	King St	Julian St	S	17	5	0	0	0	5	29%	5:00:00 PM
	Lowell Blvd	King Ct	S	7	2	0	0	0	2	29%	5:00:00 PM
	Lowell Blvd	Meade St	N	10	1	0	0	0	1	10%	9:00:00 AM
	Meade St	Lowell Blvd	S	12	1	0	0	0	1	8%	9:00:00 AM
	Meade St	Newton St	N	8	3	0	0	0	3	38%	5:00:00 PM
Newton St	Meade St	S	10	6	0	0	0	6	60%	5:00:00 PM	
Total (Weekend Composite)				629	160	0	0	3	160	25%	9:00 AM

Source: Walker Consultants

The following heat map is a composite of peak weekend parking demand per block face across all collection periods.

Figure 34: Berkeley Heat Map of Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

FAIRVIEW

A total of 24 block faces were surveyed across five streets. One off-street surface lot was also surveyed.

INVENTORY

Walker staff observed a total of approximately 450 public on-street parking spaces along all corridors identified for study in Fairview, as well as 139 off-street parking spaces at the Waddell Street Lot, located at 2945 Cottonwood Drive. The tables below summarize parking supply by street and block face as well as the parking supply for the Waddell Street Lot.

Table 26: Fairview Observed On-street Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
Cottonwood Dr	Elmwood Ln	Fairview Ave	S	13	0	0	13
	Elmwood Ln	Valleyview Dr	N	6	0	0	6
	Fairview Ave	Elmwood Ln	N	17	0	0	17
	Fairview Ave	Greenleaf Ln	S	10	0	0	10
	Federal Blvd	Valleyview Dr	S	19	0	0	19
	Greenleaf Ln	Fairview Ave	N	9	0	0	9
	Valleyview Dr	Elmwood Ln	S	7	0	0	7
	Valleyview Dr	Federal Blvd	N	14	0	0	14
Elmwood Ln	Cottonwood Dr	Cottonwood Dr	E	14	0	0	14
	Cottonwood Dr	Cottonwood Dr	W	8	0	0	8
	Cottonwood Dr	Fairview Ave	E	14	11	0	25
	Cottonwood Dr	Meadowbrook Dr	W	10	0	0	10
	Fairview Ave	Cottonwood Dr	W	22	0	0	22
	Fairview Ave	Greenleaf Ln	S	10	0	0	10
	Greenleaf Ln	Fairview Ave	N	9	0	0	9
	Meadowbrook Dr	Cottonwood Dr	E	10	0	0	10
Meadowbrook Dr	Meadowbrook Dr	Westchester Dr	W	18	0	0	18
	Westchester Dr	Meadowbrook Dr	E	14	0	0	14
Fairview Ave	Cottonwood Dr	Elmwood Ln	E	30	0	0	30
	Elmwood Ln	Cottonwood Dr	W	35	0	0	35
Greenleaf Ln	Cottonwood Dr	Elmwood Ln	E	34	0	0	34
	Elmwood Ln	Cottonwood Dr	W	34	0	0	34
W 80th Ave	Clay St	Zuni St	S	30	0	0	30
	Zuni St	Clay St	N	52	0	0	52
Total On Street Parking				439	11	0	450

Source: Walker Consultants

Table 27: Fairview Observed Off-street Parking Supply by Facility

Off-street Lot	Un-restricted	Time Limited	ADA	Total
Waddell Street Lot	139	0	4	143

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the Fairview neighborhood was performed on Wednesday, September 4, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. On-street parking demand for the neighborhood ranged from 52 vehicles (12% occupied) at 9 AM, 11 AM, and 1 PM to the system-wide peak of 64 vehicles (14% occupied) at 9 and 11 PM. Off-street parking demand at the Waddell Surface Lot ranged from 0 spaces at 9 and 11 PM to 3 spaces (2% occupied) at 11 AM and 1 PM.

The tables below summarize parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

Table 28: Fairview Observed Weekday On-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	450	53	12%	1	0%	0	0%	2	0%	54	12%
11:00 AM	450	53	12%	1	0%	0	0%	2	0%	54	12%
1:00 PM	450	52	12%	0	0%	0	0%	2	0%	52	12%
9:00 PM	450	63	14%	0	0%	0	0%	1	0%	63	14%
11:00 PM	450	64	14%	0	0%	0	0%	1	0%	64	14%
Average	450	58	13%	0	0%	0	0%	2	0%	59	13%

Source: Walker Consultants

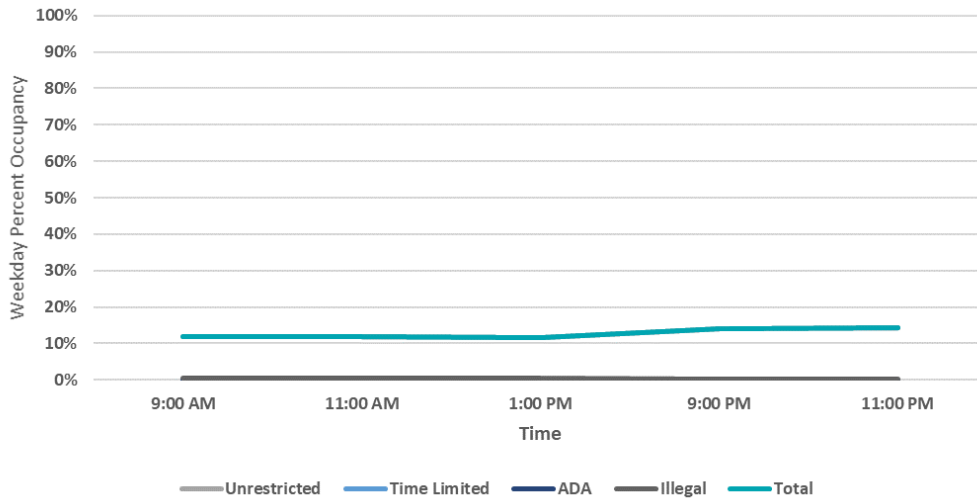
Table 29: Fairview Observed Weekday Off-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	143	1	1%	0	0%	0	0%	0	0%	1	1%
11:00 AM	143	3	2%	0	0%	0	0%	0	0%	3	2%
1:00 PM	143	2	1%	0	0%	1	1%	0	0%	3	2%
9:00 PM	143	0	0%	0	0%	0	0%	0	0%	0	0%
11:00 PM	143	0	0%	0	0%	0	0%	0	0%	0	0%
Average	143	1	1%	0	0%	1	1%	0	0%	1	1%

Source: Walker Consultants

The figure below shows total weekday parking occupancy by time for the Fairview neighborhood.

Figure 35: Fairview Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 9 and 11 PM, on-street parking demand by block face ranged from 0% occupied (10 segments) to 50% occupied along Cottonwood Drive between Fairview Avenue and Greenleaf Lane. Overall, there were no areas observed at or above 85% occupied. One block face was observed to experience occupancies at or above 50%.

One illegally parked vehicle was observed during the peak. This occurred on the northside of Cottonwood Drive between Valley View Drive and Federal Boulevard.

The following table summarizes parking occupancy by block face at 11 PM.

Table 30: Fairview Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					
					Un-restricted	Time Limited	ADA	Illegal	Total Weekday Demand (System-wide Peak)	Weekday % Occupancy (System-wide Peak)
Cottonwood Dr	Elmwood Ln	Fairview Ave	S	13	5	0	0	0	5	38%
	Elmwood Ln	Valleyview Dr	N	6	0	0	0	0	0	0%
	Fairview Ave	Elmwood Ln	N	17	0	0	0	0	0	0%
	Fairview Ave	Greenleaf Ln	S	10	5	0	0	0	5	50%
	Federal Blvd	Valleyview Dr	S	19	6	0	0	0	6	32%
	Greenleaf Ln	Fairview Ave	N	9	2	0	0	0	2	22%
	Valleyview Dr	Elmwood Ln	S	7	0	0	0	0	0	0%
Elmwood Ln	Valleyview Dr	Federal Blvd	N	14	3	0	0	1	3	21%
	Cottonwood Dr	Cottonwood Dr	E	14	0	0	0	0	0	0%
	Cottonwood Dr	Cottonwood Dr	W	8	2	0	0	0	2	25%
	Cottonwood Dr	Fairview Ave	E	25	0	0	0	0	0	0%
	Cottonwood Dr	Meadowbrook Dr	W	10	2	0	0	0	2	20%
	Fairview Ave	Cottonwood Dr	W	22	5	0	0	0	5	23%
	Fairview Ave	Greenleaf Ln	S	10	0	0	0	0	0	0%
	Greenleaf Ln	Fairview Ave	N	9	3	0	0	0	3	33%
	Meadowbrook Dr	Cottonwood Dr	E	10	0	0	0	0	0	0%
Fairview Ave	Meadowbrook Dr	Westchester Dr	W	18	3	0	0	0	3	17%
	Westchester Dr	Meadowbrook Dr	E	14	0	0	0	0	0	0%
	Cottonwood Dr	Elmwood Ln	E	30	9	0	0	0	9	30%
Greenleaf Ln	Elmwood Ln	Cottonwood Dr	W	35	0	0	0	0	0	0%
	Cottonwood Dr	Elmwood Ln	E	34	6	0	0	0	6	18%
W 80th Ave	Elmwood Ln	Cottonwood Dr	W	34	11	0	0	0	11	32%
	Clay St	Zuni St	S	30	2	0	0	0	2	7%
	Zuni St	Clay St	N	52	0	0	0	0	0	0%
Total (Weekday System-wide Peak)				450	64	0	0	1	64	14%

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak at 9 and 11 PM are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

Nine block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. For the weekday, the overall composite peak was approximately 4% higher than the system peak.

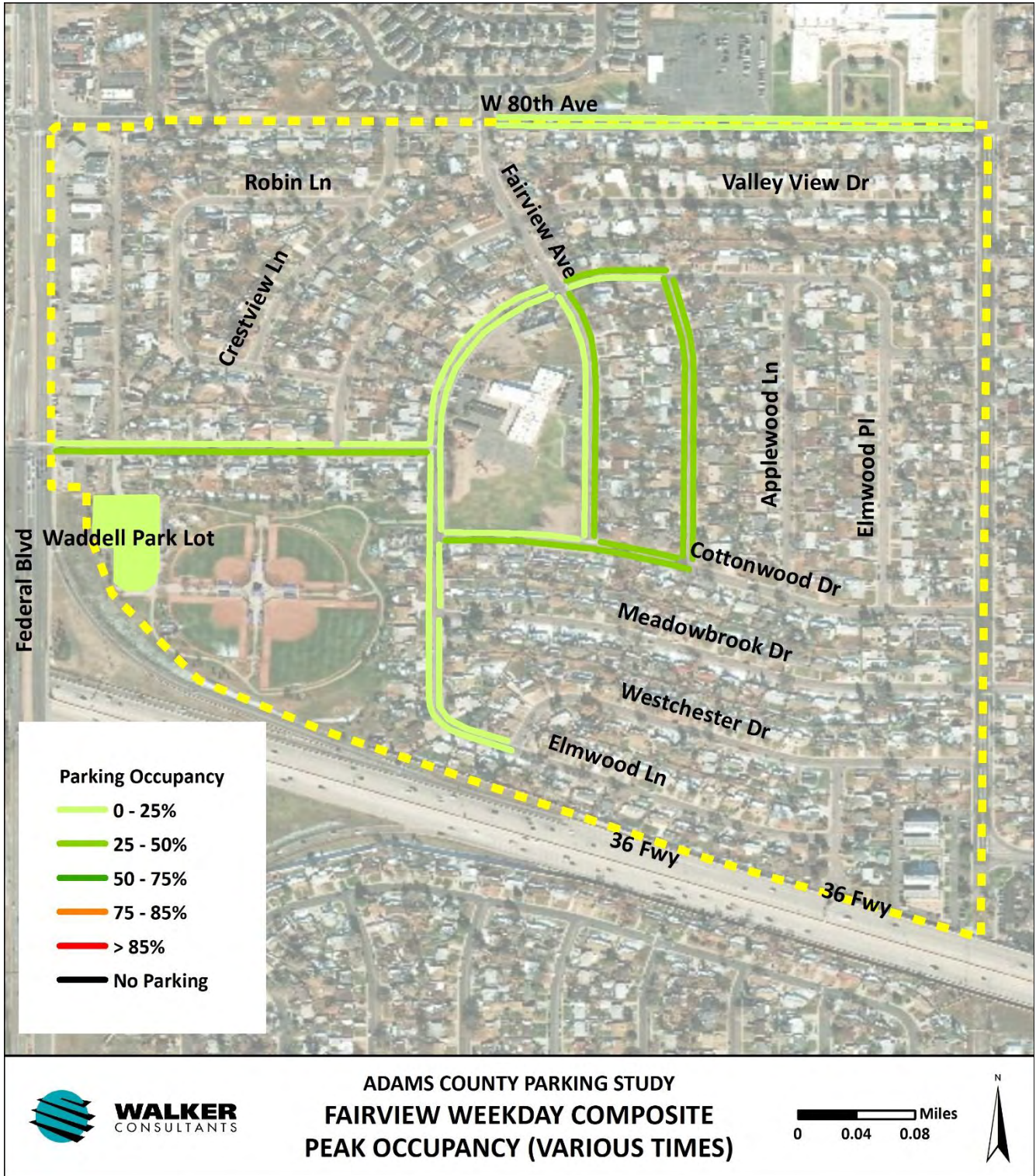
Table 31: Fairview Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Peak Weekday % Occupancy (Across All Times)	Time that Peak Occurred
Cottonwood Dr	Elmwood Ln	Fairview Ave	S	13	5	0	0	0	5	38%	11:00:00 PM
	Elmwood Ln	Valleyview Dr	N	6	1	0	0	0	1	17%	9:00:00 AM
	Fairview Ave	Elmwood Ln	N	17	0	0	0	0	0	0%	
	Fairview Ave	Greenleaf Ln	S	10	5	0	0	0	5	50%	11:00:00 PM
	Federal Blvd	Valleyview Dr	S	19	6	0	0	0	6	32%	11:00:00 PM
	Greenleaf Ln	Fairview Ave	N	9	3	0	0	0	3	33%	9:00:00 PM
	Valleyview Dr	Elmwood Ln	S	7	2	0	0	0	2	29%	9:00:00 AM
	Valleyview Dr	Federal Blvd	N	14	3	0	0	1	3	21%	11:00:00 PM
Elmwood Ln	Cottonwood Dr	Cottonwood Dr	E	14	0	0	0	0	0	0%	
	Cottonwood Dr	Cottonwood Dr	W	8	2	0	0	0	2	25%	11:00:00 PM
	Cottonwood Dr	Fairview Ave	E	25	0	1	0	0	1	4%	9:00:00 AM
	Cottonwood Dr	Meadowbrook Dr	W	10	2	0	0	0	2	20%	11:00:00 PM
	Fairview Ave	Cottonwood Dr	W	22	5	0	0	0	5	23%	11:00:00 PM
	Fairview Ave	Greenleaf Ln	S	10	0	0	0	0	0	0%	
	Greenleaf Ln	Fairview Ave	N	9	3	0	0	0	3	33%	11:00:00 PM
	Meadowbrook Dr	Cottonwood Dr	E	10	0	0	0	0	0	0%	
Fairview Ave	Meadowbrook Dr	Westchester Dr	W	18	3	0	0	0	3	17%	11:00:00 PM
	Westchester Dr	Meadowbrook Dr	E	14	1	0	0	0	1	7%	1:00:00 PM
	Cottonwood Dr	Elmwood Ln	E	30	9	0	0	0	9	30%	11:00:00 PM
Greenleaf Ln	Elmwood Ln	Cottonwood Dr	W	35	5	0	0	0	5	14%	11:00:00 AM
	Cottonwood Dr	Elmwood Ln	E	34	11	0	0	0	11	32%	9:00:00 PM
W 80th Ave	Elmwood Ln	Cottonwood Dr	W	34	11	0	0	0	11	32%	11:00:00 PM
	Clay St	Zuni St	S	30	3	0	0	0	3	10%	9:00:00 PM
	Zuni St	Clay St	N	52	1	0	0	0	1	2%	9:00:00 AM
Total (Weekday Composite)				450	81	1	0	1	82	18%	11:00:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekday parking demand per block face across all collection periods.

Figure 36: Fairview Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the Fairview neighborhood was performed on Sunday September 8, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 65 vehicles (14% occupied) at 5 PM to the system-wide peak of 78 vehicles (17% occupied) at 1 PM. Weekend off-street demand at the Waddell Surface Lot ranged from 1 vehicle at 5 PM and 7 PM to 6 vehicles at 1 PM.

The tables below summarize parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

Table 32: Fairview Observed Weekend On-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	450	66	15%	0	0%	0	0%	0	0%	66	15%
11:00 AM	450	66	15%	0	0%	0	0%	0	0%	66	15%
1:00 PM	450	78	17%	0	0%	0	0%	0	0%	78	17%
5:00 PM	450	65	14%	0	0%	0	0%	0	0%	65	14%
7:00 PM	450	68	15%	0	0%	0	0%	0	0%	68	15%
Average	450	69	15%	0	0%	0	0%	0	0%	69	15%

Source: Walker Consultants

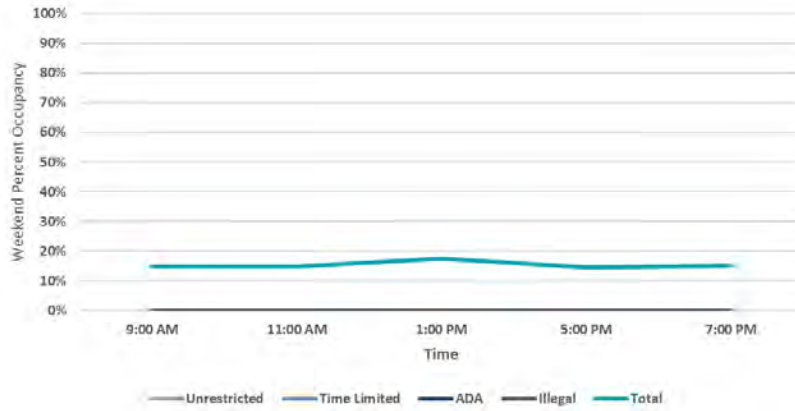
Table 33: Fairview Observed Weekend Off-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	143	3	100%	0	200%	0	0%	0	0%	3	2%
11:00 AM	143	4	100%	0	200%	0	0%	0	0%	4	3%
1:00 PM	143	6	0%	0	200%	0	0%	0	0%	6	4%
5:00 PM	143	1	0%	0	100%	0	0%	0	0%	1	1%
7:00 PM	143	1	0%	0	100%	0	0%	0	0%	1	1%
Average	143	3	2%	0	0%	0	1%	0	0%	3	2%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the Fairview neighborhood.

Figure 37: Fairview Total Weekend Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekend peak at 1 PM, demand by block face ranged from 0% occupied (nine segments) to 50% occupied along Cottonwood Drive between Fairview Avenue and Greenleaf Lane. There were no areas observed at or above 85% occupied. One block face was observed to experience occupancies of 50% or above. There were no illegally parked vehicles observed in the Fairview neighborhood during the weekend.

Table 34: Fairview Weekend System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					Total Weekend Demand (System-wide Peak)	Weekend % Occupancy (System-wide Peak)
					Unrestricted	Time Limited	ADA	Illegal			
Cottonwood Dr	Elmwood Ln	Fairview Ave	S	13	3	0	0	0	3	23%	
	Elmwood Ln	Valleyview Dr	N	6	0	0	0	0	0	0%	
	Fairview Ave	Elmwood Ln	N	17	0	0	0	0	0	0%	
	Fairview Ave	Greenleaf Ln	S	10	5	0	0	0	5	50%	
	Federal Blvd	Valleyview Dr	S	19	6	0	0	0	6	32%	
	Greenleaf Ln	Fairview Ave	N	9	3	0	0	0	3	33%	
	Valleyview Dr	Elmwood Ln	S	7	0	0	0	0	0	0%	
Elmwood Ln	Valleyview Dr	Federal Blvd	N	14	4	0	0	0	4	29%	
	Cottonwood Dr	Cottonwood Dr	E	14	0	0	0	0	0	0%	
	Cottonwood Dr	Cottonwood Dr	W	8	4	0	0	0	4	50%	
	Cottonwood Dr	Fairview Ave	E	25	0	0	0	0	0	0%	
	Cottonwood Dr	Meadowbrook Dr	W	10	1	0	0	0	1	10%	
	Fairview Ave	Cottonwood Dr	W	22	6	0	0	0	6	27%	
	Fairview Ave	Greenleaf Ln	S	10	0	0	0	0	0	0%	
	Greenleaf Ln	Fairview Ave	N	9	4	0	0	0	4	44%	
	Meadowbrook Dr	Cottonwood Dr	E	10	0	0	0	0	0	0%	
	Meadowbrook Dr	Westchester Dr	W	18	3	0	0	0	3	17%	
Fairview Ave	Westchester Dr	Meadowbrook Dr	E	14	0	0	0	0	0	0%	
	Cottonwood Dr	Elmwood Ln	E	30	7	0	0	0	7	23%	
Greenleaf Ln	Elmwood Ln	Cottonwood Dr	W	35	1	0	0	0	1	3%	
	Cottonwood Dr	Elmwood Ln	E	34	11	0	0	0	11	32%	
W 80th Ave	Elmwood Ln	Cottonwood Dr	W	34	9	0	0	0	9	26%	
	Clay St	Zuni St	S	30	11	0	0	0	11	37%	
	Zuni St	Clay St	N	52	0	0	0	0	0	0%	
Total (Weekend System-wide Peak)				450	78	0	0	0	78	17%	

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (1 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

Eight block faces were observed with individual peak occupancies greater than they exhibited during the system-wide peak. For the weekend, the overall composite peak was approximately 3% higher than the system peak.

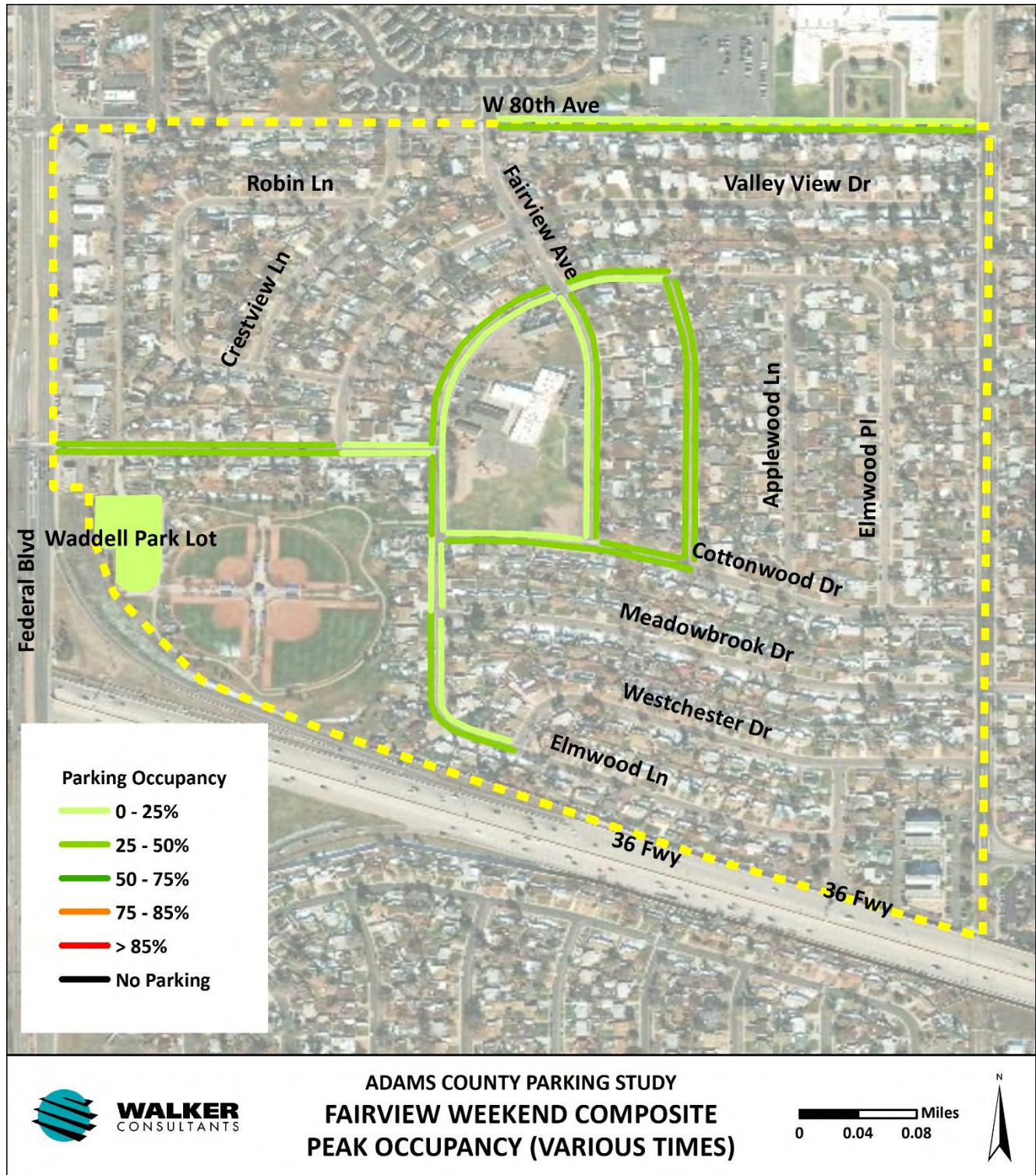
Table 35: Fairview Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)	Time that Peak Occurred
Cottonwood Dr	Elmwood Ln	Fairview Ave	S	13	5	0	0	0	5	38%	9:00:00 AM
	Elmwood Ln	Valleyview Dr	N	6	0	0	0	0	0	0%	
	Fairview Ave	Elmwood Ln	N	17	0	0	0	0	0	0%	
	Fairview Ave	Greenleaf Ln	S	10	5	0	0	0	5	50%	1:00:00 PM
	Federal Blvd	Valleyview Dr	S	19	6	0	0	0	6	32%	1:00:00 PM
	Greenleaf Ln	Fairview Ave	N	9	3	0	0	0	3	33%	1:00:00 PM
	Valleyview Dr	Elmwood Ln	S	7	1	0	0	0	1	14%	9:00:00 AM
	Valleyview Dr	Federal Blvd	N	14	4	0	0	0	4	29%	1:00:00 PM
Elmwood Ln	Cottonwood Dr	Cottonwood Dr	E	14	0	0	0	0	0	0%	
	Cottonwood Dr	Cottonwood Dr	W	8	4	0	0	0	4	50%	1:00:00 PM
	Cottonwood Dr	Fairview Ave	E	25	1	0	0	0	1	4%	7:00:00 PM
	Cottonwood Dr	Meadowbrook Dr	W	10	1	0	0	0	1	10%	1:00:00 PM
	Fairview Ave	Cottonwood Dr	W	22	6	0	0	0	6	27%	1:00:00 PM
	Fairview Ave	Greenleaf Ln	S	10	0	0	0	0	0	0%	
	Greenleaf Ln	Fairview Ave	N	9	4	0	0	0	4	44%	1:00:00 PM
	Meadowbrook Dr	Cottonwood Dr	E	10	0	0	0	0	0	0%	
	Meadowbrook Dr	Westchester Dr	W	18	5	0	0	0	5	28%	5:00:00 PM
	Westchester Dr	Meadowbrook Dr	E	14	1	0	0	0	1	7%	5:00:00 PM
Fairview Ave	Cottonwood Dr	Elmwood Ln	E	30	8	0	0	0	8	27%	11:00:00 AM
	Elmwood Ln	Cottonwood Dr	W	35	2	0	0	0	2	6%	5:00:00 PM
Greenleaf Ln	Cottonwood Dr	Elmwood Ln	E	34	11	0	0	0	11	32%	1:00:00 PM
	Elmwood Ln	Cottonwood Dr	W	34	10	0	0	0	10	29%	9:00:00 AM
W 80th Ave	Clay St	Zuni St	S	30	11	0	0	0	11	37%	1:00:00 PM
	Zuni St	Clay St	N	52	0	0	0	0	0	0%	
Total (Weekend Composite)				450	88	0	0	0	88	20%	1:00:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekend parking demand per block face across all collection periods.

Figure 38: Fairview Heat Map of Localized Weekend Peak Occupancy by Street Segment (Varying Times of Day)



Source: Walker Consultants

FLORADO

A total of 24 block faces were surveyed across five streets.

INVENTORY

Walker staff observed a total of approximately 237 public on-street parking spaces along all corridors identified for study in the Florado neighborhood. The table below summarizes parking supply by street and block face.

Table 36: Florado Observed Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
Dead End	Dead End	Dead End	N	3	0	1	4
Elmwood Ln	Florado St	Mid Block	N	13	0	0	13
	Mid Block	Vallejo St	N	7	0	0	7
	Umatilla St	Florado St	S	10	0	0	10
	Vallejo St	Umatilla St	S	8	0	0	8
Florado St	Dead End	Elmwood Ln	W	8	0	0	8
	Elmwood Ln	Dead End	E	10	0	0	10
Meadowbrook Dr	Meadowbrook Dr	Zuni St	N	8	0	0	8
	Wynadot St	Zane St	N	9	0	0	9
	Zane St	Wynadot St	S	7	0	0	7
	Zuni St	Mid Block	S	8	0	0	8
Vallejo St	Elmwood Ln	Valleyview Dr	E	17	0	0	17
	Valleyview Dr	Elmwood Ln	W	16	0	0	16
	Valleyview Dr	W 80th Ave	E	8	0	0	8
	W 80th Ave	Valleyview Dr	W	8	0	0	8
Valleyview Dr	Vallejo St	W 80th Ave	S	15	0	1	16
	W 80th Ave	Vallejo St	N	14	0	2	16
W 80th Ave	Dead End	Valleyview Dr	N	0	0	0	0
	Vallejo St	Valleyview Dr	S	10	0	0	10
	Valleyview Dr	Dead End	S	0	0	0	0
	Valleyview Dr	Vallejo St	N	12	0	0	12
Zane St	Mid Block	W 76th Ave	W	14	0	0	14
	W 76th Ave	Meadowbrook Dr	E	16	0	0	16
Zuni St	W 76th Ave	Meadowbrook Dr	E	10	0	2	12
Total On Street Parking				231	0	6	237

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the Florado neighborhood was performed on Tuesday, September 3, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. On-street parking demand for the neighborhood ranged from 50 vehicles (21% occupied) at 11 AM to the system-wide peak of 90 vehicles (38% occupied) at 11 PM.

The table below shows parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

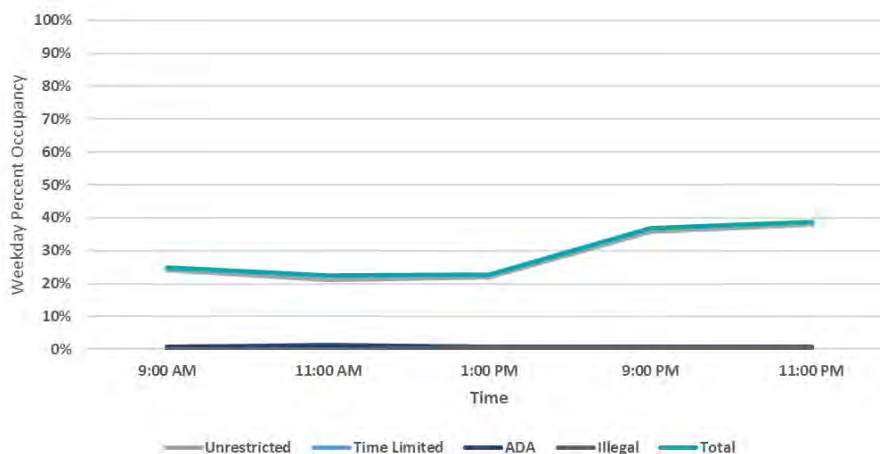
Table 37: Florado Observed Weekday Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	237	57	24%	0	0%	2	1%	0	0%	59	25%
11:00 AM	237	50	21%	0	0%	3	1%	0	0%	53	22%
1:00 PM	237	52	22%	0	0%	2	1%	1	0%	54	23%
9:00 PM	237	85	36%	0	0%	2	1%	1	0%	87	37%
11:00 PM	237	90	38%	0	0%	2	1%	1	0%	92	39%
Average	237	67	28%	0	0%	2	1%	1	0%	69	29%

Source: Walker Consultants

The figure below shows total weekday parking occupancy by time for the Florado neighborhood.

Figure 39: Florado Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 11 PM, on-street parking demand by block face ranged from 0% occupied (6 segments) to 88% occupied along Vallejo Street between 80th Avenue and Valley View Drive. One

block face was observed to experience an occupancy at or above 85% . Eight additional block faces were observed between 50% and 85% occupied.

One illegally parked vehicle was observed during the peak, on 80th Avenue between Valley View Drive and the dead end at the neighborhood’s eastern boundary.

The following table summarizes parking occupancy by block face at 11 PM.

Table 38: Florado Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					Total Weekday Demand (System-wide Peak)	Weekday % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
Dead End	Dead End	Dead End	N	13	0	0	0	0	0	0%	
Elmwood Ln	Florado St	Mid Block	N	6	0	0	0	0	0	0%	
	Mid Block	Vallejo St	N	17	0	0	0	0	0	0%	
	Umatilla St	Florado St	S	10	4	0	0	0	4	40%	
	Vallejo St	Umatilla St	S	19	1	0	0	0	1	13%	
Florado St	Dead End	Elmwood Ln	W	9	2	0	0	0	2	25%	
	Elmwood Ln	Dead End	E	7	3	0	0	0	3	30%	
Meadowbrook Dr	Meadowbrook Dr	Zuni St	N	14	0	0	0	0	0	0%	
	Wynadot St	Zane St	N	14	3	0	0	0	3	33%	
	Zane St	Wynadot St	S	8	5	0	0	0	5	71%	
	Zuni St	Mid Block	S	25	4	0	0	0	4	50%	
Vallejo St	Elmwood Ln	Valleyview Dr	E	10	9	0	0	0	9	53%	
	Valleyview Dr	Elmwood Ln	W	22	7	0	0	0	7	44%	
	Valleyview Dr	W 80th Ave	E	10	4	0	0	0	4	50%	
	W 80th Ave	Valleyview Dr	W	9	7	0	0	0	7	88%	
Valleyview Dr	Vallejo St	W 80th Ave	S	10	8	0	0	0	8	50%	
	W 80th Ave	Vallejo St	N	18	4	0	2	0	6	38%	
W 80th Ave	Dead End	Valleyview Dr	N	14	0	0	0	0	0	0%	
	Vallejo St	Valleyview Dr	S	30	3	0	0	0	3	30%	
	Valleyview Dr	Dead End	S	35	0	0	0	1	0	0%	
Zane St	Valleyview Dr	Vallejo St	N	34	8	0	0	0	8	67%	
	Mid Block	W 76th Ave	W	34	7	0	0	0	7	50%	
Zuni St	W 76th Ave	Meadowbrook Dr	E	30	9	0	0	0	9	56%	
	W 76th Ave	Meadowbrook Dr	E	52	2	0	0	0	2	17%	
Total (Weekday System-wide Peak)				450	90	0	2	1	92	20%	

Source: Walker Consultants

Table 36 summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (9 PM and 11 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

One block face was observed with an individual demand peak occupancy greater than it exhibited during the system-wide. For the weekday, the overall composite peak was approximately 1% higher than the system peak.

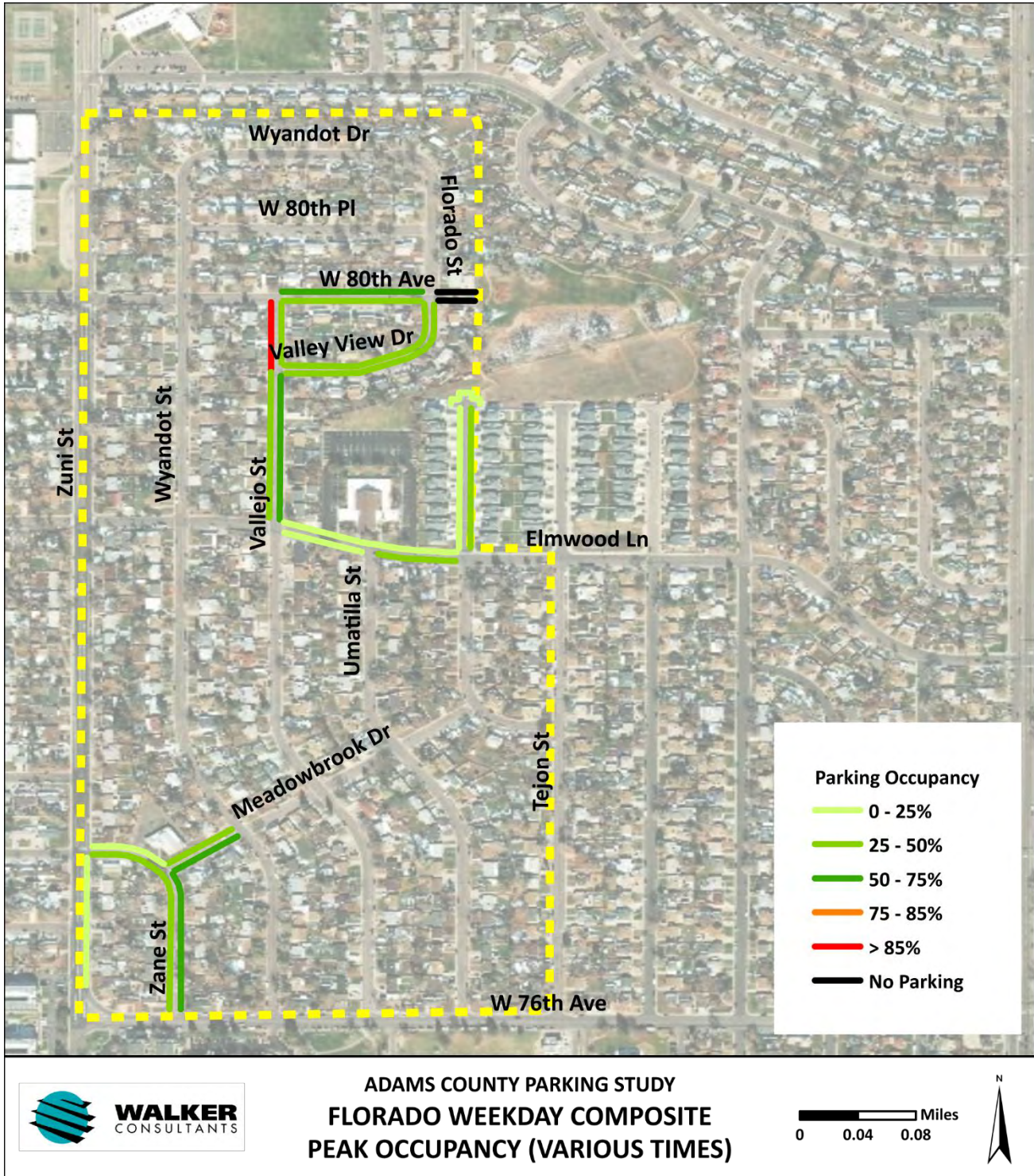
Table 39: Florado Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Peak Weekday % Occupancy (Across All Times)	Time that Peak Occurred
Dead End	Dead End	Dead End	N	13	0	0	0	0	0	0%	
Elmwood Ln	Florado St	Mid Block	N	6	0	0	0	0	0	0%	
	Mid Block	Vallejo St	N	17	0	0	0	0	0	0%	
	Umatilla St	Florado St	S	10	4	0	0	0	4	40%	11:00:00 PM
	Vallejo St	Umatilla St	S	19	1	0	0	0	1	13%	11:00:00 PM
Florado St	Dead End	Elmwood Ln	W	9	2	0	0	0	2	25%	11:00:00 PM
	Elmwood Ln	Dead End	E	7	3	0	0	0	3	30%	11:00:00 PM
Meadowbrook Dr	Meadowbrook Dr	Zuni St	N	14	0	0	0	0	0	0%	
	Wynadot St	Zane St	N	14	3	0	0	0	3	33%	11:00:00 PM
	Zane St	Wynadot St	S	8	5	0	0	0	5	71%	11:00:00 PM
	Zuni St	Mid Block	S	25	4	0	0	0	4	50%	11:00:00 PM
Vallejo St	Elmwood Ln	Valleyview Dr	E	10	9	0	0	0	9	53%	11:00:00 PM
	Valleyview Dr	Elmwood Ln	W	22	7	0	0	0	7	44%	11:00:00 PM
	Valleyview Dr	W 80th Ave	E	10	4	0	0	0	4	50%	11:00:00 PM
	W 80th Ave	Valleyview Dr	W	9	7	0	0	1	7	88%	11:00:00 PM
Valleyview Dr	Vallejo St	W 80th Ave	S	10	8	0	0	0	8	50%	11:00:00 PM
	W 80th Ave	Vallejo St	N	18	5	0	2	1	7	44%	1:00:00 PM
W 80th Ave	Dead End	Valleyview Dr	N	14	0	0	0	0	0	0%	
	Vallejo St	Valleyview Dr	S	30	3	0	0	0	3	30%	11:00:00 PM
	Valleyview Dr	Dead End	S	35	0	0	0	0	0	0%	
	Valleyview Dr	Vallejo St	N	34	8	0	0	0	8	67%	11:00:00 PM
Zane St	Mid Block	W 76th Ave	W	34	7	0	0	0	7	50%	11:00:00 PM
	W 76th Ave	Meadowbrook Dr	E	30	9	0	0	0	9	56%	11:00:00 PM
Zuni St	W 76th Ave	Meadowbrook Dr	E	52	1	0	1	0	2	17%	11:00:00 PM
Total (Weekday Composite)				450	90	0	3	2	93	21%	11:00:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekday parking demand per block face across all collection periods.

Figure 40: Florado Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the Florado neighborhood was performed on Sunday, September 1, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 77 vehicles (32% occupied) at 5 PM to the system-wide peak of 94 vehicles (40% occupied) at 9 AM.

The table below shows parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

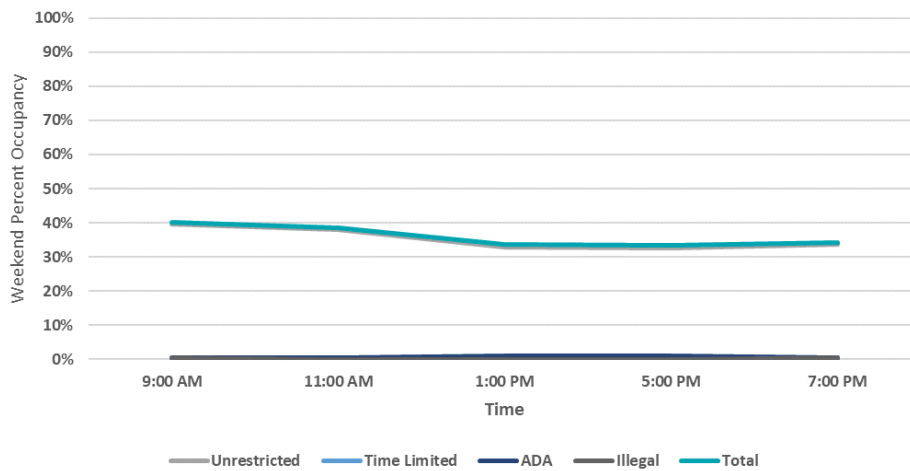
Table 40: Florado Observed Weekend Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	237	94	40%	0	0%	1	0%	1	0%	95	40%
11:00 AM	237	90	38%	0	0%	1	0%	0	0%	91	38%
1:00 PM	237	78	33%	0	0%	2	1%	0	0%	80	34%
5:00 PM	237	77	32%	0	0%	2	1%	0	0%	79	33%
7:00 PM	237	80	34%	0	0%	1	0%	1	0%	81	34%
Average	237	84	35%	0	0%	1	1%	0	0%	85	36%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the Florado neighborhood.

Figure 41: Florado Total Weekend Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekend peak at 9 AM, demand by block face ranged from 0% occupied (six segments) to 86% occupied along Meadowbrook Drive between Wyandot Street and Zane Street. There was one block face that was observed at or above 85% occupied and an additional nine block faces that were observed between 50% and 85% occupied. There was one illegally parked vehicle observed along Meadowbrook Drive east of Zuni.

Table 41: Florado Weekend System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					Weekend % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal	Total Weekend Demand (System-wide Peak)	
Dead End	Dead End	Dead End	N	4	0	0	0	0	0	0%
Elmwood Ln	Florado St	Mid Block	N	13	2	0	0	0	2	15%
	Mid Block	Vallejo St	N	7	4	0	0	0	4	57%
	Umatilla St	Florado St	S	10	5	0	0	0	5	50%
	Vallejo St	Umatilla St	S	8	5	0	0	0	5	63%
Florado St	Dead End	Elmwood Ln	W	8	0	0	0	0	0	0%
	Elmwood Ln	Dead End	E	10	0	0	0	0	0	0%
Meadowbrook Dr	Meadowbrook Dr	Zuni St	N	8	0	0	0	0	0	0%
	Wynadot St	Zane St	N	9	4	0	0	0	4	44%
	Zane St	Wynadot St	S	7	6	0	0	0	6	86%
	Zuni St	Mid Block	S	8	2	0	0	1	2	25%
Vallejo St	Elmwood Ln	Valleyview Dr	E	17	8	0	0	0	8	47%
	Valleyview Dr	Elmwood Ln	W	16	9	0	0	0	9	56%
	Valleyview Dr	W 80th Ave	E	8	4	0	0	0	4	50%
	W 80th Ave	Valleyview Dr	W	8	6	0	0	0	6	75%
Valleyview Dr	Vallejo St	W 80th Ave	S	16	11	0	0	0	11	69%
	W 80th Ave	Vallejo St	N	16	5	0	0	0	5	31%
W 80th Ave	Dead End	Valleyview Dr	N	0	0	0	0	0	0	0%
	Vallejo St	Valleyview Dr	S	10	2	0	0	0	2	20%
	Valleyview Dr	Dead End	S	0	0	0	0	0	0	0%
	Valleyview Dr	Vallejo St	N	12	7	0	0	0	7	58%
Zane St	Mid Block	W 76th Ave	W	14	6	0	0	0	6	43%
	W 76th Ave	Meadowbrook Dr	E	16	8	0	0	0	8	50%
Zuni St	W 76th Ave	Meadowbrook Dr	E	12	0	0	1	0	1	8%
Total (Weekend System-wide Peak)				237	94	0	1	1	95	40%

Source: Walker Consultants

Table 39 summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (9 AM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

Nine block faces were observed with individual peak occupancies greater than they exhibited during the system-wide peak. Localized peaks were significantly higher for Zane Street, Zuni Street, and Elmwood Lane between Umatilla Street and Florado Street. The segments of Meadowbrook Drive and Elmwood Lane that peaked at more than 85% occupied are both immediately adjacent churches. This suggests an overflow parking issue for those areas on Sunday mornings. For the weekend, the overall composite peak was approximately 8% higher than the system peak.



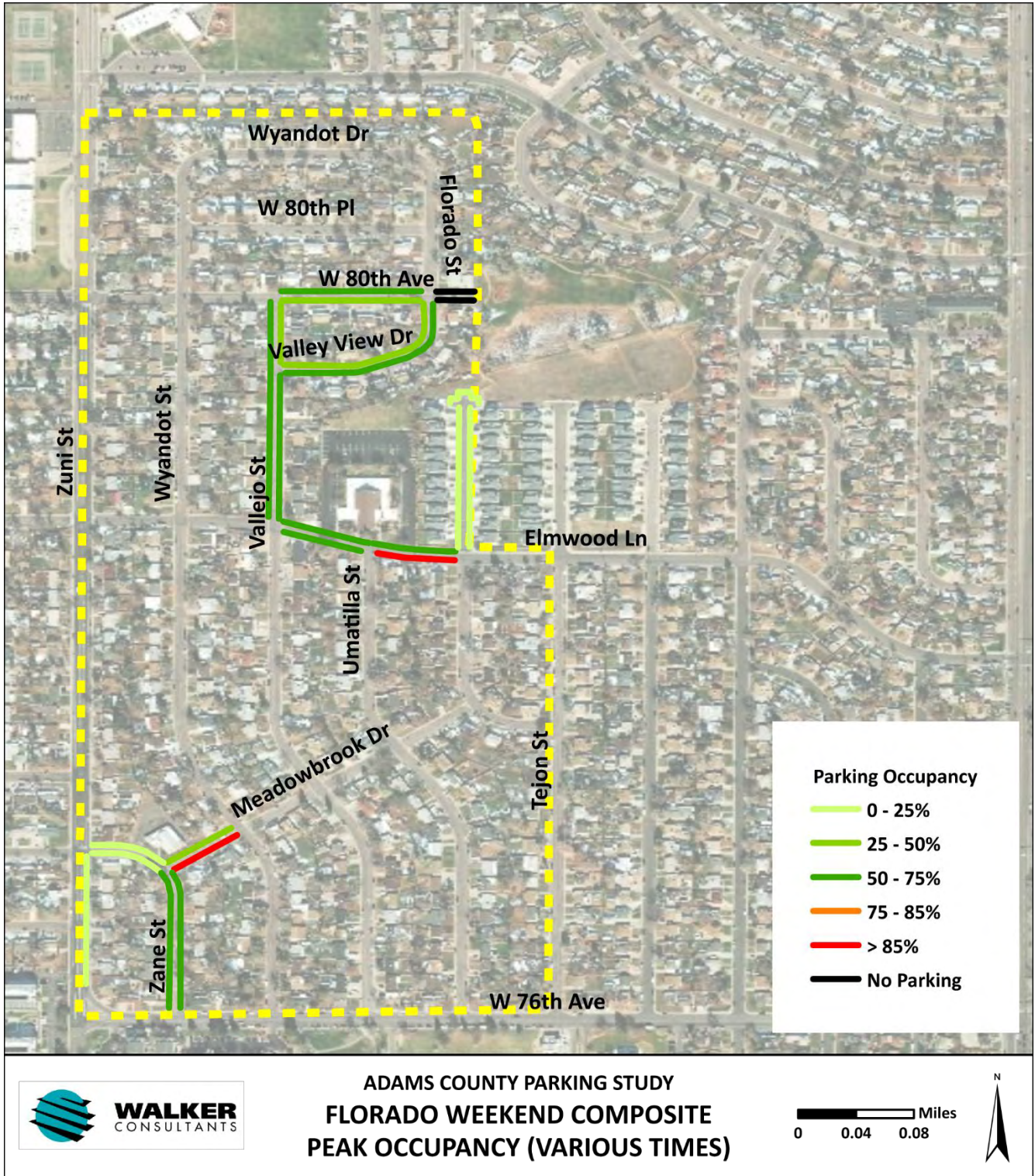
Table 42: Florado Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)	Time that Peak Occurred
Dead End	Dead End	Dead End	N	4	0	0	0	0	0	0%	
Elmwood Ln	Florado St	Mid Block	N	13	7	0	0	0	7	54%	11:00:00 AM
	Mid Block	Vallejo St	N	7	4	0	0	0	4	57%	9:00:00 AM
	Umatilla St	Florado St	S	10	9	0	0	0	9	90%	11:00:00 AM
	Vallejo St	Umatilla St	S	8	5	0	0	0	5	63%	9:00:00 AM
Florado St	Dead End	Elmwood Ln	W	8	0	0	0	0	0	0%	
	Elmwood Ln	Dead End	E	10	0	0	0	0	0	0%	
Meadowbrook Dr	Meadowbrook Dr	Zuni St	N	8	1	0	0	0	1	13%	11:00:00 AM
	Wynadot St	Zane St	N	9	4	0	0	0	4	44%	9:00:00 AM
	Zane St	Wynadot St	S	7	6	0	0	0	6	86%	9:00:00 AM
	Zuni St	Mid Block	S	8	2	0	0	1	2	25%	9:00:00 AM
Vallejo St	Elmwood Ln	Valleyview Dr	E	17	9	0	0	0	9	53%	1:00:00 PM
	Valleyview Dr	Elmwood Ln	W	16	9	0	0	0	9	56%	9:00:00 AM
	Valleyview Dr	W 80th Ave	E	8	4	0	0	0	4	50%	9:00:00 AM
	W 80th Ave	Valleyview Dr	W	8	6	0	0	0	6	75%	9:00:00 AM
Valleyview Dr	Vallejo St	W 80th Ave	S	16	11	0	0	0	11	69%	9:00:00 AM
	W 80th Ave	Vallejo St	N	16	5	0	1	0	6	38%	5:00:00 PM
W 80th Ave	Dead End	Valleyview Dr	N	0	0	0	0	0	0	0%	
	Vallejo St	Valleyview Dr	S	10	4	0	0	0	4	40%	7:00:00 PM
	Valleyview Dr	Dead End	S	0	0	0	0	0	0	0%	
	Valleyview Dr	Vallejo St	N	12	7	0	0	0	7	58%	9:00:00 AM
Zane St	Mid Block	W 76th Ave	W	14	8	0	0	0	8	57%	11:00:00 AM
	W 76th Ave	Meadowbrook Dr	E	16	9	0	0	0	9	56%	1:00:00 PM
Zuni St	W 76th Ave	Meadowbrook Dr	E	12	2	0	1	0	3	25%	5:00:00 PM
Total (Weekend Composite)				237	112	0	2	1	114	48%	9:00:00 AM

Source: Walker Consultants

The following heat map is a composite of peak weekend parking demand per block face across all collection periods.

Figure 42: Florado Heat Map of Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

GOAT HILL

A total of 40 block faces were surveyed across 10 streets. One off-street surface lot was also surveyed.

INVENTORY

Walker staff observed a total of approximately 389 public on-street parking spaces along all corridors identified for study in Goat Hill, as well as 21 off-street parking spaces at the Baker Community Center surface parking lot, located at 6755 Irving Street. The tables below summarize parking supply by street and block face as well as the parking supply for the Baker Community Center facility.

Table 43: Goat Hill Observed On-street Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
Clay St	W 63rd Ave	W 64th Ave	E	0	0	0	0
	W 64th Ave	W 63rd Ave	W	3	0	0	3
	W 65th Ave	W 64th Ave	W	3	0	0	3
	W 65th Ave	W 65th Pl	E	10	0	0	10
	W 65th Pl	W 65th Ave	W	7	0	0	7
Irving St	W 67th Ave	W 68th Ave	E	21	0	0	21
	W 68th Ave	W 67th Ave	W	20	0	0	20
King St	W 67th Ave	W 68th Ave	E	16	0	0	16
	W 68th Ave	W 67th Ave	W	9	0	0	9
W 63rd Ave	Clay St	Decatur St	N	12	0	0	12
	Decatur St	Clay St	S	0	0	0	0
	Decatur St	Federal Blvd	N	4	0	0	4
	Federal Blvd	Decatur St	S	0	0	0	0
W 64th Ave	Clay St	Federal Blvd	N	0	0	0	0
W 65th Pl	Clay St	Dead End	S	16	0	0	16
	Clay St	Federal Blvd	N	8	0	0	8
	Dead End	Clay St	N	16	0	0	16
	Federal Blvd	Clay St	S	14	0	0	14
W 66th Pl	Decatur St	Federal Blvd	N	16	0	0	16
	Federal Blvd	Decatur St	S	24	0	0	24
W 67th Ave	Irving St	Julian St	N	18	0	0	18
	Julian St	Irving St	S	8	0	0	8
	Julian St	Knox Ct	N	9	0	0	9
	King St	Knox Ct	S	10	0	0	10
	King St	Lowell Blvd	N	10	0	0	10
	Knox Ct	Julian St	S	11	0	0	11
	Knox Ct	King St	N	7	0	0	7
	Lowell Blvd	King St	S	8	0	0	8
W 67th Pl	Decatur St	Federal Blvd	N	25	0	0	25
	Federal Blvd	Decatur St	S	3	0	0	3
W 68th Ave	Irving St	Julian St	N	7	0	0	7
	Julian St	Irving St	S	21	0	0	21
	Julian St	Knox Ct	N	9	0	0	9
	King St	Knox St	S	11	0	0	11
	King St	Lowell Blvd	N	0	0	0	0
	Knox Ct	Julian St	S	14	0	0	14
	Knox Ct	Knox St	N	3	0	0	3
	Knox St	King St	N	10	0	0	10
	Knox St	Knox Ct	S	6	0	0	6
	Lowell Blvd	King St	S	0	0	0	0
Total On Street Parking				389	0	0	389

Source: Walker Consultants

Table 44: Goat Hill Observed Off-street Parking Supply by Facility

Off-street Lot	Un-restricted	Time Limited	ADA	Total
Baker Community Center	21	0	2	23

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the Goat Hill neighborhood was performed on Wednesday, August 28, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. On-street parking demand for the neighborhood ranged from 25 vehicles (6% occupied) at 1 PM to the system-wide peak of 38 vehicles (10% occupied) at 9 AM. Off-street parking demand at the Baker Community Center Lot peaked at 9 AM, with one vehicle parked. The lot was unoccupied during all other observation times.

The tables below summarize parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

Table 45: Goat Hill Observed Weekday On-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	389	38	10%	0	0%	0	0%	6	2%	38	10%
11:00 AM	389	34	8%	0	0%	0	0%	4	1%	34	8%
1:00 PM	389	25	6%	0	0%	0	0%	4	1%	25	6%
9:00 PM	389	31	8%	0	0%	0	0%	3	1%	31	8%
11:00 PM	389	33	8%	0	0%	0	0%	5	1%	33	8%
Average	389	32	8%	0	0%	0	0%	4	1%	32	8%

Source: Walker Consultants

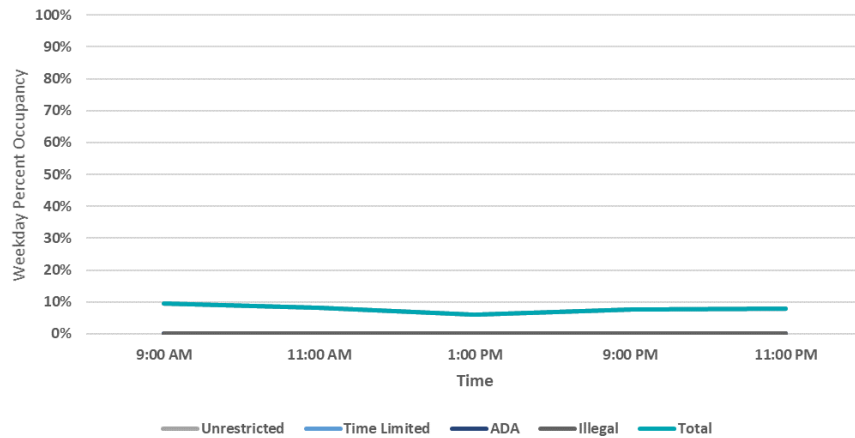
Table 46: Goat Hill Observed Weekday Off-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	23	1	4%	0	0%	0	0%	0	0%	1	4%
11:00 AM	23	0	0%	0	0%	0	0%	0	0%	0	0%
1:00 PM	23	0	0%	0	0%	0	0%	0	0%	0	0%
9:00 PM	23	0	0%	0	0%	0	0%	0	0%	0	0%
11:00 PM	23	0	0%	0	0%	0	0%	0	0%	0	0%
Average	23	1	4%	0	0%	0	0%	0	0%	1	4%

Source: Walker Consultants

The figure below shows total weekday parking occupancy by time for the Goat Hill neighborhood.

Figure 43: Goat Hill Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 9 and 11 PM, on-street parking demand by block face ranged from 0% occupied (21 segments) to 58% occupied along 63rd Avenue between Clay Street and Decatur Street. Overall, there were no block faces observed at or above 85% occupied. Two block faces were observed to experience occupancies of 50% or above.

Six illegally parked vehicles were observed during the system-wide peak along three streets. These were:

- King Street between 67th Avenue and 68th Avenue, Westside (1 vehicle)
- 63rd Avenue between Federal Boulevard and Decatur Street, Southside (3 vehicles)
- 65th Place between Federal Boulevard and Clay Street, North- side (2 vehicles)

The following table summarizes parking occupancy by block face at 9 AM.

Table 47: Goat Hill Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					Weekday % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal	Total Weekday Demand (System-wide Peak)	
Clay St	W 63rd Ave	W 64th Ave	E	0	0	0	0	0	0	0%
	W 64th Ave	W 63rd Ave	W	3	0	0	0	0	0	0%
	W 65th Ave	W 64th Ave	W	3	0	0	0	0	0	0%
	W 65th Ave	W 65th Pl	E	10	1	0	0	0	1	10%
	W 65th Pl	W 65th Ave	W	7	0	0	0	0	0	0%
Irving St	W 67th Ave	W 68th Ave	E	21	3	0	0	0	3	14%
	W 68th Ave	W 67th Ave	W	20	0	0	0	0	0	0%
King St	W 67th Ave	W 68th Ave	E	16	1	0	0	0	1	6%
	W 68th Ave	W 67th Ave	W	9	4	0	0	1	4	44%
W 63rd Ave	Clay St	Decatur St	N	12	7	0	0	0	7	58%
	Decatur St	Clay St	S	0	0	0	0	0	0	0%
	Decatur St	Federal Blvd	N	4	2	0	0	0	2	50%
	Federal Blvd	Decatur St	S	0	0	0	0	3	0	0%
W 64th Ave	Clay St	Federal Blvd	N	0	0	0	0	0	0	0%
W 65th Pl	Clay St	Dead End	S	16	0	0	0	0	0	0%
	Clay St	Federal Blvd	N	8	0	0	0	2	0	0%
	Dead End	Clay St	N	16	0	0	0	0	0	0%
	Federal Blvd	Clay St	S	14	1	0	0	0	1	7%
W 66th Pl	Decatur St	Federal Blvd	N	16	0	0	0	0	0	0%
	Federal Blvd	Decatur St	S	24	2	0	0	0	2	8%
W 67th Ave	Irving St	Julian St	N	18	3	0	0	0	3	17%
	Julian St	Irving St	S	8	0	0	0	0	0	0%
	Julian St	Knox Ct	N	9	2	0	0	0	2	22%
	King St	Knox Ct	S	10	2	0	0	0	2	20%
	King St	Lowell Blvd	N	10	0	0	0	0	0	0%
	Knox Ct	Julian St	S	11	1	0	0	0	1	9%
	Knox Ct	King St	N	7	0	0	0	0	0	0%
	Lowell Blvd	King St	S	8	1	0	0	0	1	13%
W 67th Pl	Decatur St	Federal Blvd	N	25	2	0	0	0	2	8%
	Federal Blvd	Decatur St	S	3	1	0	0	0	1	33%
W 68th Ave	Irving St	Julian St	N	7	0	0	0	0	0	0%
	Julian St	Irving St	S	21	0	0	0	0	0	0%
	Julian St	Knox Ct	N	9	0	0	0	0	0	0%
	King St	Knox St	S	11	0	0	0	0	0	0%
	King St	Lowell Blvd	N	0	0	0	0	0	0	0%
	Knox Ct	Julian St	S	14	0	0	0	0	0	0%
	Knox Ct	Knox St	N	3	1	0	0	0	1	33%
	Knox St	King St	N	10	4	0	0	0	4	40%
	Knox St	Knox Ct	S	6	0	0	0	0	0	0%
	Lowell Blvd	King St	S	0	0	0	0	0	0	0%
Total (Weekday System-wide Peak)				389	38	0	0	6	38	10%

Source: Walker Consultants

Table 45 summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (9 PM and 11 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

Seven block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. For the weekday, the overall composite peak was approximately 4% higher than the system peak.



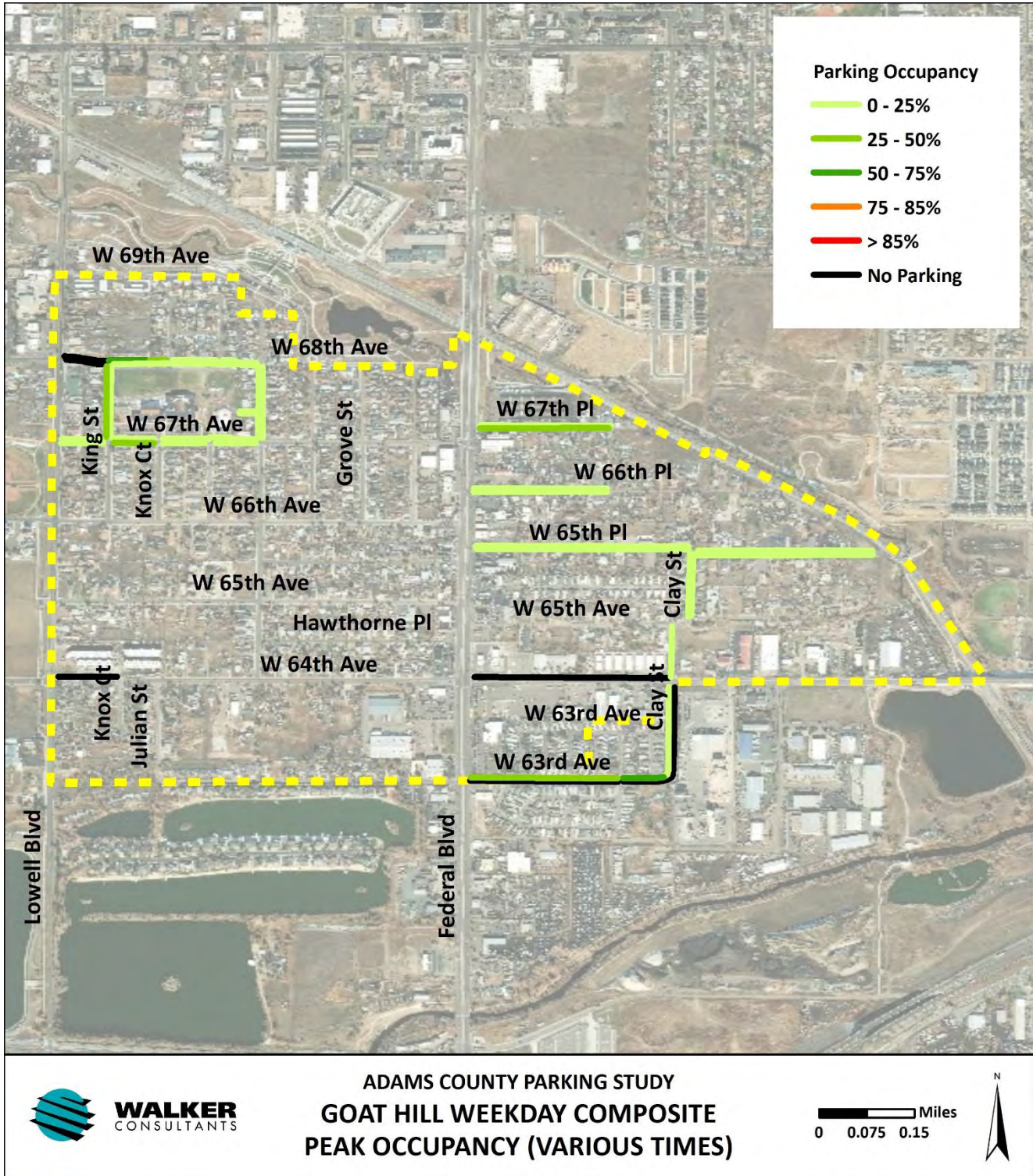
Table 48: Goat Hill Localized Weekday Peak Occupancy by Street Segment (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Peak Weekday % Occupancy (Across All Times)	Time that Peak Occurred
Clay St	W 63rd Ave	W 64th Ave	E	0	0	0	0	0	0	0%	
	W 64th Ave	W 63rd Ave	W	3	0	0	0	0	0	0%	
	W 65th Ave	W 64th Ave	W	3	0	0	0	0	0	0%	
	W 65th Ave	W 65th Pl	E	10	1	0	0	0	1	10%	9:00:00 AM
Irving St	W 65th Pl	W 65th Ave	W	7	1	0	0	0	1	14%	11:00:00 PM
	W 67th Ave	W 68th Ave	E	21	5	0	0	0	5	24%	9:00:00 PM
King St	W 68th Ave	W 67th Ave	W	20	3	0	0	0	3	15%	11:00:00 PM
	W 67th Ave	W 68th Ave	E	16	1	0	0	0	1	6%	9:00:00 AM
W 63rd Ave	W 68th Ave	W 67th Ave	W	9	4	0	0	1	4	44%	9:00:00 AM
	Clay St	Decatur St	N	12	7	0	0	0	7	58%	9:00:00 AM
	Decatur St	Clay St	S	0	0	0	0	0	0	0%	
	Decatur St	Federal Blvd	N	4	2	0	0	0	2	50%	9:00:00 AM
W 64th Ave	Federal Blvd	Decatur St	S	0	0	0	0	3	0	0%	
	Clay St	Federal Blvd	N	0	0	0	0	0	0	0%	
W 65th Pl	Clay St	Dead End	S	16	0	0	0	0	0	0%	
	Clay St	Federal Blvd	N	8	0	0	0	2	0	0%	
	Dead End	Clay St	N	16	0	0	0	0	0	0%	
W 66th Pl	Federal Blvd	Clay St	S	14	1	0	0	0	1	7%	9:00:00 AM
	Decatur St	Federal Blvd	N	16	0	0	0	0	0	0%	
W 67th Ave	Federal Blvd	Decatur St	S	24	3	0	0	0	3	13%	9:00:00 PM
	Irving St	Julian St	N	18	3	0	0	0	3	17%	9:00:00 AM
	Julian St	Irving St	S	8	0	0	0	0	0	0%	
	Julian St	Knox Ct	N	9	2	0	0	0	2	22%	9:00:00 AM
	King St	Knox Ct	S	10	4	0	0	0	4	40%	9:00:00 PM
	King St	Lowell Blvd	N	10	0	0	0	0	0	0%	
	Knox Ct	Julian St	S	11	1	0	0	0	1	9%	9:00:00 AM
W 67th Pl	Knox Ct	King St	N	7	1	0	0	0	1	14%	
	Lowell Blvd	King St	S	8	2	0	0	0	2	25%	
W 68th Ave	Decatur St	Federal Blvd	N	25	2	0	0	0	2	8%	9:00:00 AM
	Federal Blvd	Decatur St	S	3	1	0	0	0	1	33%	9:00:00 AM
W 68th Ave	Irving St	Julian St	N	7	0	0	0	0	0	0%	
	Julian St	Irving St	S	21	0	0	0	0	0	0%	
	Julian St	Knox Ct	N	9	0	0	0	0	0	0%	
	King St	Knox St	S	11	2	0	0	0	2	18%	11:00:00 PM
	King St	Lowell Blvd	N	0	0	0	0	0	0	0%	
	Knox Ct	Julian St	S	14	0	0	0	0	0	0%	
	Knox Ct	Knox St	N	3	1	0	0	0	1	33%	9:00:00 AM
	Knox St	King St	N	10	7	0	0	0	7	70%	11:00:00 PM
Total (Weekday Composite)	Knox St	Knox Ct	S	6	0	0	0	0	0	0%	
	Lowell Blvd	King St	S	0	0	0	0	0	0	0%	
				389	54	0	0	6	54	14%	9:00:00 AM

Source: Walker Consultants

The following heat map is a composite of peak weekday parking demand per block face across all collection periods.

Figure 44: Goat Hill Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the Goat Hill neighborhood was performed on Saturday, August 31 and Sunday, September 1, 2019. Corridors in and around churches were counted on Sunday whereas all other corridors were counted on Saturday in order to capture maximum potential weekend parking demand for this neighborhood. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 46 vehicles (12% occupied) at 1 PM to the system-wide peak of 50 vehicles (13% occupied) at 9 AM and 7 PM. No vehicles were observed parked at the Baker Community Center during the weekend.

The tables below summarize parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

Table 49: Goat Hill Observed Weekend On-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	389	50	13%	0	0%	0	0%	9	2%	50	13%
11:00 AM	389	48	12%	0	0%	0	0%	9	2%	48	12%
1:00 PM	389	46	12%	0	0%	0	0%	7	2%	46	12%
5:00 PM	389	49	13%	0	0%	0	0%	4	1%	49	13%
7:00 PM	389	50	13%	0	0%	0	0%	4	1%	50	13%
Average	389	49	12%	0	0%	0	0%	7	2%	49	12%

Source: Walker Consultants

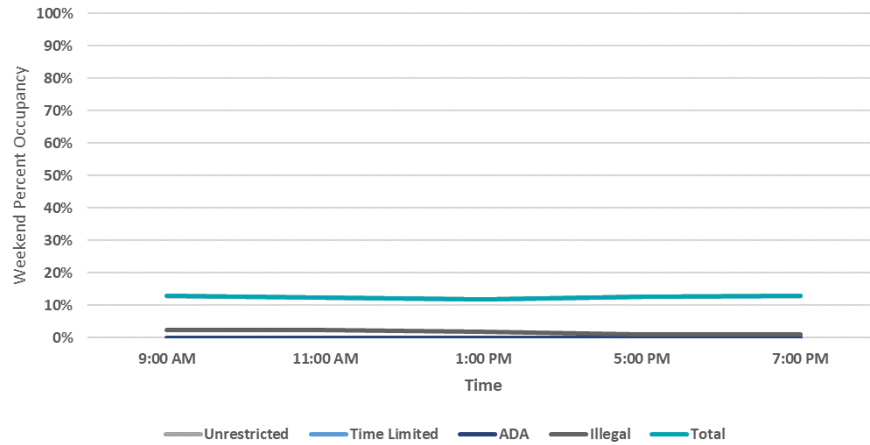
Table 50: Goat Hill Observed Weekend Off-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	23	0	0%	0	0%	0	0%	0	0%	0	0%
11:00 AM	23	0	0%	0	0%	0	0%	0	0%	0	0%
1:00 PM	23	0	0%	0	0%	0	0%	0	0%	0	0%
5:00 PM	23	0	0%	0	0%	0	0%	0	0%	0	0%
7:00 PM	23	0	0%	0	0%	0	0%	0	0%	0	0%
Average	23	0	0%	0	0%	0	0%	0	0%	0	0%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the Goat Hill neighborhood.

Figure 45: Goat Hill Total Weekend Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekend peak at 9 AM, demand by block face ranged from 0% occupied (22 segments) to 75% occupied along 63rd Avenue between Federal Boulevard and Decatur Street. There were no block faces observed at or above 85% occupied. One block face was observed to be at or above 50% occupied.

There were nine illegally parked vehicles observed during the weekend system-wide peak at the following locations:

- King Street between 67th Avenue and 68th Avenue, Westside (1 vehicle)
- 63rd Avenue between Federal Boulevard and Decatur Street, both sides (8 vehicles)

Table 51: Goat Hill Weekend System-wide Peak Parking Occupancy by Street Segment

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					Total Weekend Demand (System-wide Peak)	Weekend % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
Clay St	W 63rd Ave	W 64th Ave	E	0	0	0	0	0	0	0%	
	W 64th Ave	W 63rd Ave	W	3	0	0	0	0	0	0%	
	W 65th Ave	W 64th Ave	W	3	0	0	0	0	0	0%	
	W 65th Ave	W 65th Pl	E	10	4	0	0	0	4	40%	
Irving St	W 65th Pl	W 65th Ave	W	7	1	0	0	0	1	14%	
	W 67th Ave	W 68th Ave	E	21	5	0	0	0	5	24%	
King St	W 68th Ave	W 67th Ave	W	20	1	0	0	0	1	5%	
	W 67th Ave	W 68th Ave	E	16	0	0	0	0	0	0%	
W 63rd Ave	W 68th Ave	W 67th Ave	W	9	4	0	0	1	4	44%	
	Clay St	Decatur St	N	12	4	0	0	0	4	33%	
	Decatur St	Clay St	S	0	0	0	0	0	0	0%	
	Decatur St	Federal Blvd	N	4	3	0	0	1	3	75%	
W 64th Ave	Federal Blvd	Decatur St	S	0	0	0	0	7	0	0%	
	Clay St	Federal Blvd	N	0	0	0	0	0	0	0%	
W 65th Pl	Clay St	Dead End	S	16	2	0	0	0	2	13%	
	Clay St	Federal Blvd	N	8	0	0	0	0	0	0%	
	Dead End	Clay St	N	16	7	0	0	0	7	44%	
W 66th Pl	Federal Blvd	Clay St	S	14	0	0	0	0	0	0%	
	Decatur St	Federal Blvd	N	16	0	0	0	0	0	0%	
W 67th Ave	Federal Blvd	Decatur St	S	24	2	0	0	0	2	8%	
	Irving St	Julian St	N	18	0	0	0	0	0	0%	
	Julian St	Irving St	S	8	0	0	0	0	0	0%	
	Julian St	Knox Ct	N	9	0	0	0	0	0	0%	
	King St	Knox Ct	S	10	4	0	0	0	4	40%	
	King St	Lowell Blvd	N	10	1	0	0	0	1	10%	
	Knox Ct	Julian St	S	11	0	0	0	0	0	0%	
W 67th Pl	Knox Ct	King St	N	7	3	0	0	0	3	43%	
	Lowell Blvd	King St	S	8	1	0	0	0	1	13%	
W 68th Ave	Decatur St	Federal Blvd	N	25	1	0	0	0	1	4%	
	Federal Blvd	Decatur St	S	3	0	0	0	0	0	0%	
W 68th Ave	Irving St	Julian St	N	7	0	0	0	0	0	0%	
	Julian St	Irving St	S	21	0	0	0	0	0	0%	
	Julian St	Knox Ct	N	9	0	0	0	0	0	0%	
	King St	Knox St	S	11	2	0	0	0	2	18%	
	King St	Lowell Blvd	N	0	0	0	0	0	0	0%	
	Knox Ct	Julian St	S	14	0	0	0	0	0	0%	
	Knox Ct	Knox St	N	3	1	0	0	0	1	33%	
	Knox St	King St	N	10	4	0	0	0	4	40%	
	Knox St	Knox Ct	S	6	0	0	0	0	0	0%	
	Lowell Blvd	King St	S	0	0	0	0	0	0	0%	
Total (Weekend System-wide Peak)				389	50	0	0	9	50	13%	

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (9 AM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

Nine block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. For the weekend, the overall composite peak was approximately 4% higher than the system peak.



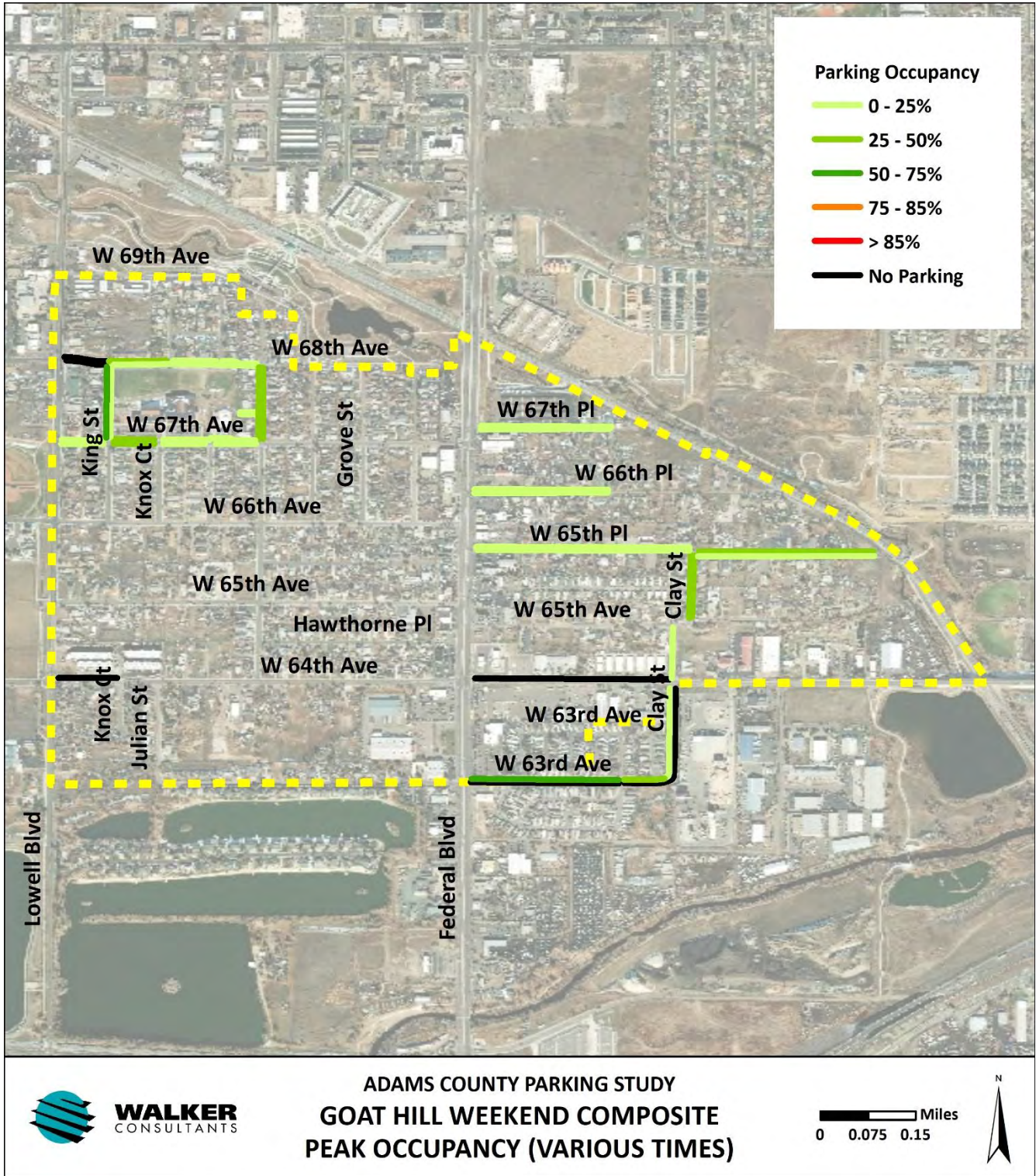
Table 52: Goat Hill Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)	Time that Peak Occurred
Clay St	W 63rd Ave	W 64th Ave	E	0	0	0	0	0	0	0%	
	W 64th Ave	W 63rd Ave	W	3	0	0	0	0	0	0%	
	W 65th Ave	W 64th Ave	W	3	0	0	0	0	0	0%	
	W 65th Ave	W 65th Pl	E	10	4	0	0	0	4	40%	9:00:00 AM
Irving St	W 65th Pl	W 65th Ave	W	7	2	0	0	0	2	29%	5:00:00 PM
	W 67th Ave	W 68th Ave	E	21	8	0	0	0	8	38%	7:00:00 PM
King St	W 68th Ave	W 67th Ave	W	20	7	0	0	0	7	35%	7:00:00 PM
	W 67th Ave	W 68th Ave	E	16	0	0	0	0	0	0%	
W 63rd Ave	W 68th Ave	W 67th Ave	W	9	5	0	0	1	5	56%	11:00:00 AM
	Clay St	Decatur St	N	12	5	0	0	0	5	42%	1:00:00 PM
	Decatur St	Clay St	S	0	0	0	0	0	0	0%	
	Decatur St	Federal Blvd	N	4	3	0	0	1	3	75%	9:00:00 AM
W 64th Ave	Federal Blvd	Decatur St	S	0	0	0	0	7	0	0%	
	Clay St	Federal Blvd	N	0	0	0	0	0	0	0%	
W 65th Pl	Clay St	Dead End	S	16	2	0	0	0	2	13%	9:00:00 AM
	Clay St	Federal Blvd	N	8	0	0	0	0	0	0%	
	Dead End	Clay St	N	16	8	0	0	0	8	50%	11:00:00 AM
	Federal Blvd	Clay St	S	14	0	0	0	0	0	0%	
W 66th Pl	Decatur St	Federal Blvd	N	16	1	0	0	0	1	6%	5:00:00 PM
	Federal Blvd	Decatur St	S	24	2	0	0	0	2	8%	9:00:00 AM
W 67th Ave	Irving St	Julian St	N	18	0	0	0	0	0	0%	
	Julian St	Irving St	S	8	1	0	0	0	1	13%	5:00:00 PM
	Julian St	Knox Ct	N	9	0	0	0	0	0	0%	
	King St	Knox Ct	S	10	4	0	0	0	4	40%	9:00:00 AM
	King St	Lowell Blvd	N	10	1	0	0	0	1	10%	9:00:00 AM
	Knox Ct	Julian St	S	11	0	0	0	0	0	0%	
	Knox Ct	King St	N	7	3	0	0	0	3	43%	9:00:00 AM
W 67th Pl	Lowell Blvd	King St	S	8	1	0	0	0	1	13%	9:00:00 AM
	Decatur St	Federal Blvd	N	25	2	0	0	0	2	8%	1:00:00 PM
W 68th Ave	Federal Blvd	Decatur St	S	3	0	0	0	0	0	0%	
	Irving St	Julian St	N	7	0	0	0	0	0	0%	
	Julian St	Irving St	S	21	0	0	0	0	0	0%	
	Julian St	Knox Ct	N	9	0	0	0	0	0	0%	
	King St	Knox St	S	11	2	0	0	0	2	18%	9:00:00 AM
	King St	Lowell Blvd	N	0	0	0	0	0	0	0%	
	Knox Ct	Julian St	S	14	0	0	0	0	0	0%	
	Knox Ct	Knox St	N	3	1	0	0	0	1	33%	9:00:00 AM
	Knox St	King St	N	10	5	0	0	0	5	50%	5:00:00 PM
	Knox St	Knox Ct	S	6	0	0	0	0	0	0%	
Lowell Blvd	King St	S	0	0	0	0	0	0	0%		
Total (Weekend Composite)				389	67	0	0	9	67	17%	9:00:00 AM

Source: Walker Consultants

The following heat map is a composite of peak weekend parking demand per block face across all collection periods.

Figure 46: Goat Hill Heat Map of Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

LAKESHORE ESTATES

A total of 34 block faces were surveyed across eight streets. One off-street surface lot was also surveyed.

INVENTORY

Walker staff observed a total of approximately 301 public on-street parking spaces along all corridors identified for study in the Lakeshore Estates neighborhood. An additional 121 off-street parking spaces were inventoried at the Hidden Lake Park Lot located at 3735 W. 66th Avenue. The tables below summarize parking supply by street and block face as well as the parking supply for the Baker Community Center.

Table 53: Lakeshore Estates Observed On-street Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
Newton Ct	Cul de sac	W 66th Ave	W	5	0	0	5
	W 66th Ave	Cul de sac	E	5	0	0	5
Osceola Ct	Cul de sac	W 66th Ave	W	2	0	0	2
	W 66th Ave	Cul de sac	E	3	0	0	3
Perry Ct	Cul de sac	W 66th Ave	W	5	0	0	5
	W 66th Ave	Cul de sac	E	6	0	0	6
Quitman Ct	Cul de sac	W 66th Ave	W	9	0	0	9
	W 66th Ave	Cul de sac	E	10	0	0	10
Raleigh Ct	Cul de sac	W 66th Ave	W	10	0	0	10
	W 66th Ave	Cul de sac	E	7	0	0	7
Raleigh St	Cul de sac	W 65th Ave	E	15	0	0	15
	W 64th Ave	W 65th Ave	W	16	0	0	16
	W 65th Ave	Cul de sac	W	15	0	0	15
	W 65th Ave	W 64th Ave	E	19	0	0	19
Stuart Ct	Cul de sac	W 66th Ave	W	5	0	0	5
	W 66th Ave	Cul de sac	E	5	0	0	5
W 66th Ave	Lowell Blvd	Meade Ct	N	7	0	0	7
	Meade Ct	Lowell Blvd	S	5	0	0	5
	Meade Ct	Newton St	N	11	0	0	11
	Newton Ct	Newton St	S	15	0	0	15
	Newton Ct	Osceola Ct	N	10	0	0	10
	Newton St	Meade Ct	S	4	0	0	4
	Newton St	Newton Ct	N	20	0	0	20
	Osceola Ct	Newton Ct	S	11	0	0	11
	Osceola Ct	Perry Ct	N	12	0	0	12
	Perry Ct	Osceola Ct	S	10	0	0	10
	Perry Ct	Quitman Ct	N	11	0	0	11
	Quitman Ct	Perry Ct	S	10	0	0	10
	Quitman Ct	Raleigh Ct	N	9	0	0	9
	Raleigh Ct	Quitman Ct	S	8	0	0	8
	Raleigh Ct	Stuart Ct	N	7	0	0	7
	Stuart Ct	Raleigh Ct	S	8	0	0	8
Stuart Ct	Tennyson St	N	3	0	0	3	
Tennyson St	Stuart Ct	S	3	0	0	3	
Total On Street Parking				301	0	0	301

Source: Walker Consultants

Table 54: Lakeshore Estates Observed Off-street Parking Supply by Facility

Off-street Lot	Un-restricted	Time Limited	ADA	Total
Hidden Lake Park Lot	116	0	5	121

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the Lakeshore Estates neighborhood was performed on Wednesday, August 28, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. On-street parking demand for the neighborhood ranged from 11 vehicles (4% occupied) at 11 AM to the system-wide peak of 22 vehicles (7% occupied) at 11 PM. Off-street parking demand at the Hidden Lake Park Lot ranged from 0 vehicles at 9 PM to 5 vehicles (4% occupied) at 1 PM.

The tables below summarize parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

Table 55: Lakeshore Estates Observed Weekday On-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	301	14	5%	0	0%	0	0%	0	0%	14	5%
11:00 AM	301	11	4%	0	0%	0	0%	0	0%	11	4%
1:00 PM	301	16	5%	0	0%	0	0%	0	0%	16	5%
9:00 PM	301	21	7%	0	0%	0	0%	0	0%	21	7%
11:00 PM	301	22	7%	0	0%	0	0%	0	0%	22	7%
Average	301	17	6%	0	0%	0	0%	0	0%	17	6%

Source: Walker Consultants

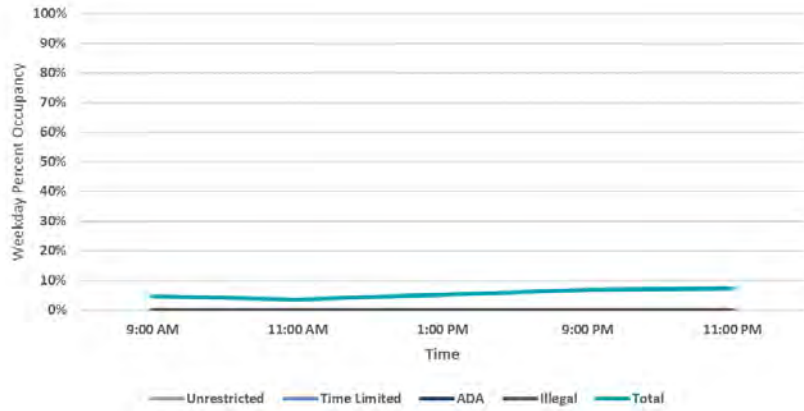
Table 56: Lakeshore Estates Observed Weekday Off-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	121	1	1%	0	0%	0	0%	0	0%	1	1%
11:00 AM	121	4	3%	0	0%	0	0%	0	0%	4	3%
1:00 PM	121	5	4%	0	0%	0	0%	0	0%	5	4%
9:00 PM	121	0	0%	0	0%	0	0%	0	0%	0	0%
11:00 PM	121	1	1%	0	0%	0	0%	0	0%	1	1%
Average	121	2	2%	0	0%	1	1%	0	0%	2	2%

Source: Walker Consultants

The figure below shows total weekday parking occupancy by time for the Lakeshore Estates neighborhood.

Figure 47: Lakeshore Estates Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 11 PM, on-street parking demand by block face ranged from 0% occupied (22 segments) to 31% occupied along Raleigh Street between 64th Avenue and 65th Avenue. Overall, there were zero block faces observed at or above 50% occupied.

No illegally parked vehicles were observed on the weekday in the Lakeshore Estates neighborhood.



The following table summarizes parking occupancy by block face during the system-wide peak of 11 PM.

Table 57: Lakeshore Estates Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					Total Weekday Demand (System-wide Peak)	Weekday % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
Newton Ct	Cul de sac	W 66th Ave	W	5	0	0	0	0	0	0%	
	W 66th Ave	Cul de sac	E	5	0	0	0	0	0	0%	
Osceola Ct	Cul de sac	W 66th Ave	W	2	0	0	0	0	0	0%	
	W 66th Ave	Cul de sac	E	3	0	0	0	0	0	0%	
Perry Ct	Cul de sac	W 66th Ave	W	5	0	0	0	0	0	0%	
	W 66th Ave	Cul de sac	E	6	0	0	0	0	0	0%	
Quitman Ct	Cul de sac	W 66th Ave	W	9	2	0	0	0	2	22%	
	W 66th Ave	Cul de sac	E	10	0	0	0	0	0	0%	
Raleigh Ct	Cul de sac	W 66th Ave	W	10	1	0	0	0	1	10%	
	W 66th Ave	Cul de sac	E	7	1	0	0	0	1	14%	
Raleigh St	Cul de sac	W 65th Ave	E	15	2	0	0	0	2	13%	
	W 64th Ave	W 65th Ave	W	16	5	0	0	0	5	31%	
	W 65th Ave	Cul de sac	W	15	0	0	0	0	0	0%	
	W 65th Ave	W 64th Ave	E	19	1	0	0	0	1	5%	
Stuart Ct	Cul de sac	W 66th Ave	W	5	1	0	0	0	1	20%	
	W 66th Ave	Cul de sac	E	5	0	0	0	0	0	0%	
W 66th Ave	Lowell Blvd	Meade Ct	N	7	0	0	0	0	0	0%	
	Meade Ct	Lowell Blvd	S	5	0	0	0	0	0	0%	
	Meade Ct	Newton St	N	11	0	0	0	0	0	0%	
	Newton Ct	Newton St	S	15	0	0	0	0	0	0%	
	Newton Ct	Osceola Ct	N	10	3	0	0	0	3	30%	
	Newton St	Meade Ct	S	4	0	0	0	0	0	0%	
	Newton St	Newton Ct	N	20	0	0	0	0	0	0%	
	Osceola Ct	Newton Ct	S	11	2	0	0	0	2	18%	
	Osceola Ct	Perry Ct	N	12	0	0	0	0	0	0%	
	Perry Ct	Osceola Ct	S	10	0	0	0	0	0	0%	
	Perry Ct	Quitman Ct	N	11	0	0	0	0	0	0%	
	Quitman Ct	Perry Ct	S	10	2	0	0	0	2	20%	
	Quitman Ct	Raleigh Ct	N	9	1	0	0	0	1	11%	
	Raleigh Ct	Quitman Ct	S	8	1	0	0	0	1	13%	
	Raleigh Ct	Stuart Ct	N	7	0	0	0	0	0	0%	
	Stuart Ct	Raleigh Ct	S	8	0	0	0	0	0	0%	
	Stuart Ct	Tennyson St	N	3	0	0	0	0	0	0%	
Tennyson St	Stuart Ct	S	3	0	0	0	0	0	0%		
Total (Weekday System-wide Peak)				301	22	0	0	0	22	7%	

Source: Walker Consultants

Table 55 summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (11 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

Seven block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak., Most of these instances occurred in the morning. For the weekday, the overall composite peak was approximately 4% higher than the system peak.

Table 58: Lakeshore Estates Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Peak % Occupancy (Across All Times)	Time that Peak Occurred
Newton Ct	Cul de sac	W 66th Ave	W	5	0	0	0	0	0	0%	
	W 66th Ave	Cul de sac	E	5	0	0	0	0	0	0%	
Osceola Ct	Cul de sac	W 66th Ave	W	2	0	0	0	0	0	0%	
	W 66th Ave	Cul de sac	E	3	0	0	0	0	0	0%	
Perry Ct	Cul de sac	W 66th Ave	W	5	0	0	0	0	0	0%	
	W 66th Ave	Cul de sac	E	6	1	0	0	0	1	17%	11:00:00 AM
Quitman Ct	Cul de sac	W 66th Ave	W	9	3	0	0	0	3	33%	1:00:00 PM
	W 66th Ave	Cul de sac	E	10	1	0	0	0	1	10%	9:00:00 AM
Raleigh Ct	Cul de sac	W 66th Ave	W	10	2	0	0	0	2	20%	9:00:00 AM
	W 66th Ave	Cul de sac	E	7	1	0	0	0	1	14%	11:00:00 PM
Raleigh St	Cul de sac	W 65th Ave	E	15	2	0	0	0	2	13%	11:00:00 PM
	W 64th Ave	W 65th Ave	W	16	5	0	0	0	5	31%	11:00:00 PM
	W 65th Ave	Cul de sac	W	15	2	0	0	0	2	13%	9:00:00 PM
	W 65th Ave	W 64th Ave	E	19	1	0	0	0	1	5%	11:00:00 PM
Stuart Ct	Cul de sac	W 66th Ave	W	5	1	0	0	0	1	20%	11:00:00 PM
	W 66th Ave	Cul de sac	E	5	0	0	0	0	0	0%	
W 66th Ave	Lowell Blvd	Meade Ct	N	7	0	0	0	0	0	0%	
	Meade Ct	Lowell Blvd	S	5	0	0	0	0	0	0%	
	Meade Ct	Newton St	N	11	0	0	0	0	0	0%	
	Newton Ct	Newton St	S	15	3	0	0	0	3	20%	1:00:00 PM
	Newton Ct	Osceola Ct	N	10	3	0	0	0	3	30%	11:00:00 PM
	Newton St	Meade Ct	S	4	1	0	0	0	1	25%	9:00:00 AM
	Newton St	Newton Ct	N	20	0	0	0	0	0	0%	
	Osceola Ct	Newton Ct	S	11	2	0	0	0	2	18%	11:00:00 PM
	Osceola Ct	Perry Ct	N	12	0	0	0	0	0	0%	
	Perry Ct	Osceola Ct	S	10	0	0	0	0	0	0%	
	Perry Ct	Quitman Ct	N	11	0	0	0	0	0	0%	
	Quitman Ct	Perry Ct	S	10	2	0	0	0	2	20%	11:00:00 PM
	Quitman Ct	Raleigh Ct	N	9	1	0	0	0	1	11%	11:00:00 PM
	Raleigh Ct	Quitman Ct	S	8	1	0	0	0	1	13%	11:00:00 PM
	Raleigh Ct	Stuart Ct	N	7	0	0	0	0	0	0%	
	Stuart Ct	Raleigh Ct	S	8	0	0	0	0	0	0%	
	Stuart Ct	Tennyson St	N	3	0	0	0	0	0	0%	
Tennyson St	Stuart Ct	S	3	0	0	0	0	0	0%		
Total (Weekday Composite)				301	32	0	0	0	32	11%	11:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekday parking demand per block face across all collection periods.

Figure 48: Lakeshore Estates Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the Lakeshore Estates neighborhood was performed on Saturday, August 31, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 14 vehicles (5% occupied) at 9 AM and 5 PM to a system-wide peak of 18 vehicles (6% occupied) at 11 AM, 1 PM, and 7 PM. Off-street parking demand at the Hidden Lake Lot ranged from 0 vehicles at 9 AM and 11 AM to 4 vehicles at 7 PM.

The tables below summarize parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

Table 59: Lakeshore Estates Observed Weekend On-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	301	15	5%	0	0%	0	0%	0	0%	15	5%
11:00 AM	301	17	6%	0	0%	0	0%	0	0%	17	6%
1:00 PM	301	18	6%	0	0%	0	0%	0	0%	18	6%
5:00 PM	301	14	5%	0	0%	0	0%	1	0%	14	5%
7:00 PM	301	18	6%	0	0%	0	0%	1	0%	18	6%
Average	301	16	5%	0	0%	0	0%	0	0%	16	5%

Source: Walker Consultants

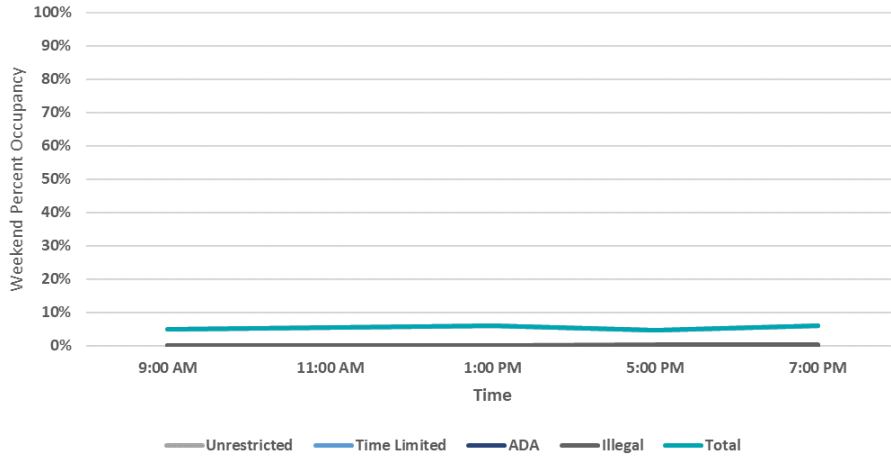
Table 60: Lakeshore Estates Observed Weekend Off-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	121	0	0%	0	0%	0	0%	0	0%	0	0%
11:00 AM	121	0	0%	0	0%	0	0%	0	0%	0	0%
1:00 PM	121	1	1%	0	0%	0	0%	0	0%	1	1%
5:00 PM	121	3	2%	0	0%	0	0%	0	0%	3	2%
7:00 PM	121	4	3%	0	0%	0	0%	0	0%	4	3%
Average	121	2	1%	0	0%	1	1%	0	0%	2	1%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the Lakeshore Estates neighborhood.

Figure 49: Lakeshore Estates Total Weekend Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekend peak at 7 PM, demand by block face ranged from 0% occupied (24 segments) to 50% occupied along Osceola Court between 66th Avenue and the cul-de-sac. There were zero block faces observed at or above 85% occupied and one block face at or above 50% occupied.

One illegally parked vehicle was observed along Raleigh Street between 64th Avenue and 65th Avenue.

The following table summarizes parking occupancy by block face during the weekend system-wide peak of 7 PM.

Table 61: Lakeshore Estates Weekend System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					Weekend % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal	Total Weekend Demand (System-wide Peak)	
Newton Ct	Cul de sac	W 66th Ave	W	5	0	0	0	0	0	0%
	W 66th Ave	Cul de sac	E	5	0	0	0	0	0	0%
Osceola Ct	Cul de sac	W 66th Ave	W	2	1	0	0	0	1	50%
	W 66th Ave	Cul de sac	E	3	0	0	0	0	0	0%
Perry Ct	Cul de sac	W 66th Ave	W	5	0	0	0	0	0	0%
	W 66th Ave	Cul de sac	E	6	0	0	0	0	0	0%
Quitman Ct	Cul de sac	W 66th Ave	W	9	3	0	0	0	3	33%
	W 66th Ave	Cul de sac	E	10	1	0	0	0	1	10%
Raleigh Ct	Cul de sac	W 66th Ave	W	10	0	0	0	0	0	0%
	W 66th Ave	Cul de sac	E	7	1	0	0	0	1	14%
Raleigh St	Cul de sac	W 65th Ave	E	15	0	0	0	0	0	0%
	W 64th Ave	W 65th Ave	W	16	3	0	0	1	3	19%
	W 65th Ave	Cul de sac	W	15	3	0	0	0	3	20%
Stuart Ct	W 65th Ave	W 64th Ave	E	19	1	0	0	0	1	5%
	Cul de sac	W 66th Ave	W	5	1	0	0	0	1	20%
Stuart Ct	W 66th Ave	Cul de sac	E	5	0	0	0	0	0	0%
	Lowell Blvd	Meade Ct	N	7	0	0	0	0	0	0%
W 66th Ave	Meade Ct	Lowell Blvd	S	5	0	0	0	0	0	0%
	Meade Ct	Newton St	N	11	0	0	0	0	0	0%
	Newton Ct	Newton St	S	15	0	0	0	0	0	0%
	Newton Ct	Osceola Ct	N	10	0	0	0	0	0	0%
	Newton St	Meade Ct	S	4	0	0	0	0	0	0%
	Newton St	Newton Ct	N	20	0	0	0	0	0	0%
	Osceola Ct	Newton Ct	S	11	1	0	0	0	1	9%
	Osceola Ct	Perry Ct	N	12	0	0	0	0	0	0%
	Perry Ct	Osceola Ct	S	10	0	0	0	0	0	0%
	Perry Ct	Quitman Ct	N	11	0	0	0	0	0	0%
	Quitman Ct	Perry Ct	S	10	0	0	0	0	0	0%
	Quitman Ct	Raleigh Ct	N	9	0	0	0	0	0	0%
	Raleigh Ct	Quitman Ct	S	8	2	0	0	0	2	25%
	Raleigh Ct	Stuart Ct	N	7	0	0	0	0	0	0%
	Stuart Ct	Raleigh Ct	S	8	1	0	0	0	1	13%
	Stuart Ct	Tennyson St	N	3	0	0	0	0	0	0%
Tennyson St	Stuart Ct	S	3	0	0	0	0	0	0%	
Total (Weekend System-wide Peak)				301	18	0	0	1	18	6%

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (7 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

Six block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. This predominantly occurred along Raleigh Street. For the weekend, the overall composite peak was approximately 3% higher than the system peak.



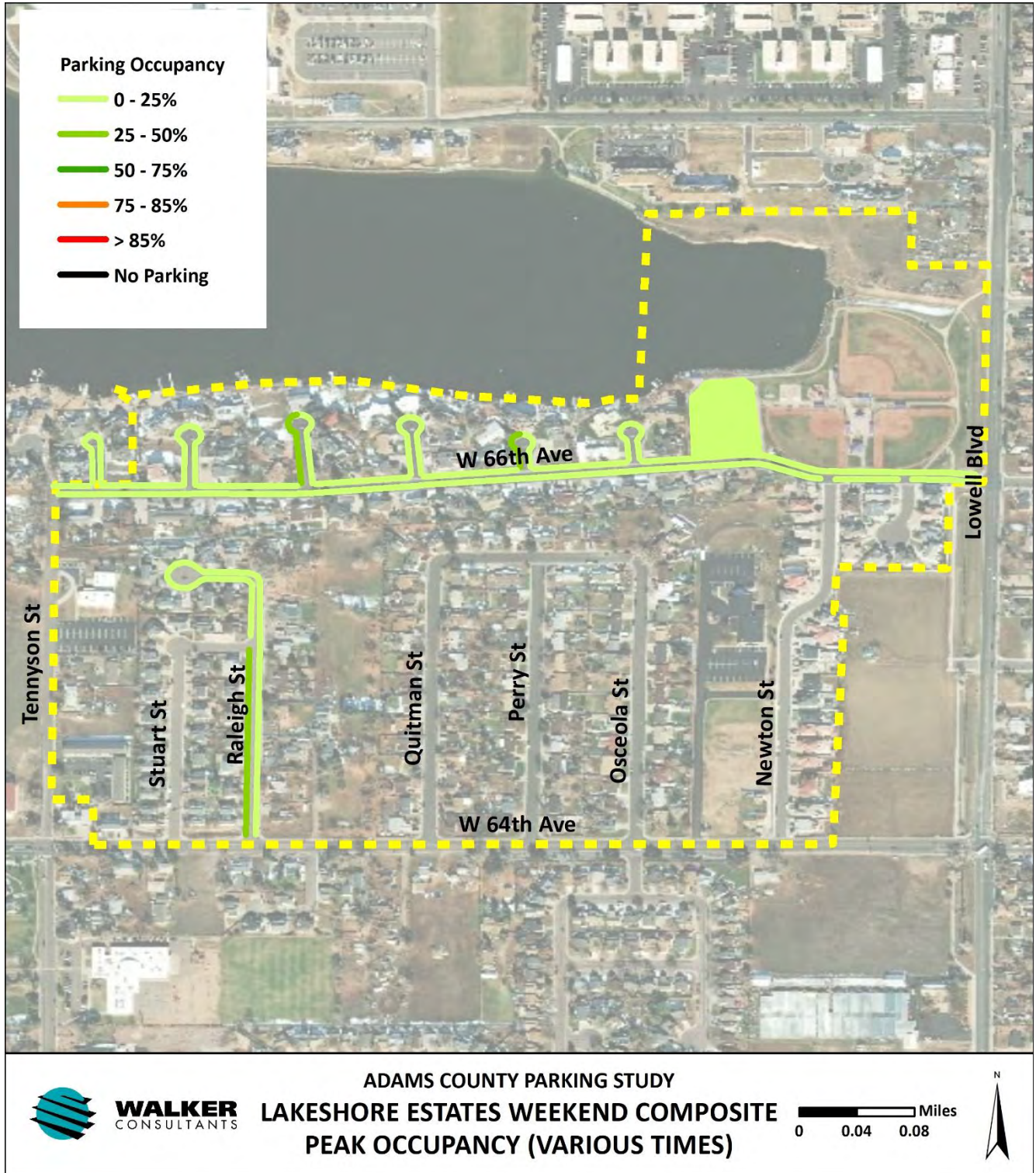
Table 62: Lakeshore Estates Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)	Time that Peak Occurred
Newton Ct	Cul de sac	W 66th Ave	W	5	0	0	0	0	0	0%	
	W 66th Ave	Cul de sac	E	5	0	0	0	0	0	0%	
Osceola Ct	Cul de sac	W 66th Ave	W	2	1	0	0	0	1	50%	7:00:00 PM
	W 66th Ave	Cul de sac	E	3	0	0	0	0	0	0%	
Perry Ct	Cul de sac	W 66th Ave	W	5	0	0	0	0	0	0%	
	W 66th Ave	Cul de sac	E	6	0	0	0	0	0	0%	
Quitman Ct	Cul de sac	W 66th Ave	W	9	3	0	0	0	3	33%	7:00:00 PM
	W 66th Ave	Cul de sac	E	10	1	0	0	0	1	10%	7:00:00 PM
Raleigh Ct	Cul de sac	W 66th Ave	W	10	1	0	0	0	1	10%	9:00:00 AM
	W 66th Ave	Cul de sac	E	7	1	0	0	0	1	14%	7:00:00 PM
Raleigh St	Cul de sac	W 65th Ave	E	15	1	0	0	0	1	7%	9:00:00 AM
	W 64th Ave	W 65th Ave	W	16	5	0	0	0	5	31%	9:00:00 AM
	W 65th Ave	Cul de sac	W	15	3	0	0	0	3	20%	7:00:00 PM
	W 65th Ave	W 64th Ave	E	19	3	0	0	0	3	16%	1:00:00 PM
Stuart Ct	Cul de sac	W 66th Ave	W	5	1	0	0	0	1	20%	7:00:00 PM
	W 66th Ave	Cul de sac	E	5	0	0	0	0	0	0%	
W 66th Ave	Lowell Blvd	Meade Ct	N	7	0	0	0	0	0	0%	
	Meade Ct	Lowell Blvd	S	5	0	0	0	0	0	0%	
	Meade Ct	Newton St	N	11	0	0	0	0	0	0%	
	Newton Ct	Newton St	S	15	1	0	0	0	1	7%	11:00:00 AM
	Newton Ct	Osceola Ct	N	10	0	0	0	0	0	0%	
	Newton St	Meade Ct	S	4	0	0	0	0	0	0%	
	Newton St	Newton Ct	N	20	0	0	0	0	0	0%	
	Osceola Ct	Newton Ct	S	11	1	0	0	0	1	9%	7:00:00 PM
	Osceola Ct	Perry Ct	N	12	0	0	0	0	0	0%	
	Perry Ct	Osceola Ct	S	10	0	0	0	0	0	0%	
	Perry Ct	Quitman Ct	N	11	0	0	0	0	0	0%	
	Quitman Ct	Perry Ct	S	10	2	0	0	0	2	20%	9:00:00 AM
	Quitman Ct	Raleigh Ct	N	9	0	0	0	0	0	0%	
	Raleigh Ct	Quitman Ct	S	8	2	0	0	0	2	25%	7:00:00 PM
	Raleigh Ct	Stuart Ct	N	7	0	0	0	0	0	0%	
	Stuart Ct	Raleigh Ct	S	8	1	0	0	0	1	13%	7:00:00 PM
	Stuart Ct	Tennyson St	N	3	0	0	0	0	0	0%	
Tennyson St	Stuart Ct	S	3	0	0	0	0	0	0%		
Total (Weekend Composite)				301	27	0	0	0	27	9%	11:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekend parking demand per block face across all collection periods.

Figure 50: Lakeshore Estates Heat Map of Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

MAPLETON

A total of 23 block faces were surveyed across eight streets.

INVENTORY

Walker staff observed a total of approximately 306 public on-street parking spaces along all corridors identified for study in Mapleton. The table below summarizes parking supply by street and block face.

Table 63: Mapleton Observed Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
Clarkson St	E 57th Ave	E 58th Ave	W	22	0	0	22
	E 58th Ave	E 57th Ave	E	20	0	0	20
E 55th Ave	Driveway	Driveway	S	27	0	0	27
	Lincoln St	Driveway	S	32	0	0	32
	Logan Ct	Pennsylvania St	S	15	0	0	15
	Pearl St	Pennsylvania St	N	4	0	0	4
	Pearl St	Washington St	S	3	0	0	3
	Pennsylvania St	Logan Ct	N	7	0	0	7
	Pennsylvania St	Pearl St	S	12	0	0	12
	Washington St	Pearl St	N	6	0	0	6
E 56th Ave	Logan Ct	Logan St	S	9	0	0	9
	Logan Ct	Pennsylvania St	S	9	0	0	9
	Pennsylvania St	Pearl St	S	7	0	0	7
E 57th Ave	Clarkson St	Emerson St	N	6	0	0	6
	Emerson St	Clarkson St	S	9	0	0	9
Emerson St	E 57th Ave	E 58th Ave	W	22	0	0	22
	E 58th Ave	E 57th Ave	E	19	0	0	19
Logan Ct	E 55th Ave	E 56th Ave	E	8	0	0	8
	E 56th Ave	E 55th Ave	W	14	0	0	14
Pearl St	E 55th Ave	E 56th Ave	E	9	0	0	9
	E 56th Ave	E 55th Ave	W	0	0	0	0
Pennsylvania St	E 55th Ave	E 56th Ave	E	20	0	0	20
	E 56th Ave	E 55th Ave	W	26	0	0	26
Total On Street Parking				306	0	0	306

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the Mapleton neighborhood was performed on Wednesday, September 4, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. On-street parking demand for the neighborhood ranged from 39 vehicles (13% occupied) at 9 PM and 11 PM to a system-wide peak of 71 vehicles (23% occupied) at 9 AM and 11 AM.

The table below summarizes parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

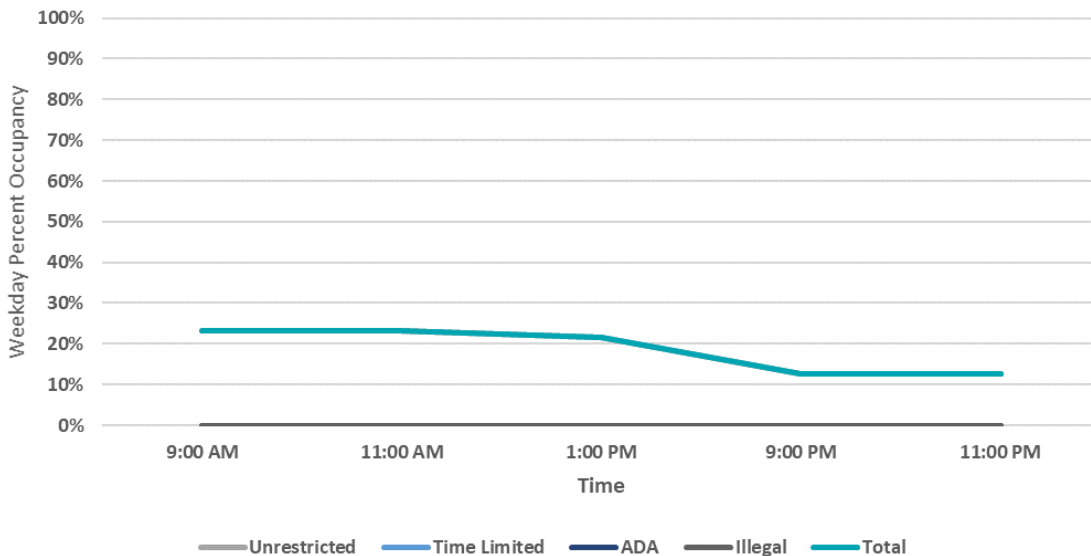
Table 64: Mapleton Observed Weekday Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	306	71	23%	0	0%	0	0%	0	0%	71	23%
11:00 AM	306	71	23%	0	0%	0	0%	0	0%	71	23%
1:00 PM	306	66	22%	0	0%	0	0%	0	0%	66	22%
9:00 PM	306	39	13%	0	0%	0	0%	0	0%	39	13%
11:00 PM	306	39	13%	0	0%	0	0%	0	0%	39	13%
Average	306	57	19%	0	0%	0	0%	0	0%	57	19%

Source: Walker Consultants

The figure below shows total weekday parking occupancy by time for the Mapleton neighborhood.

Figure 51: Mapleton Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 9 AM, on-street parking demand by block face ranged from 0% occupied (12 segments) to 100% occupied along Logan Court between 55th Avenue and 56th Avenue. During the observed peak period, there was one block face observed at or above 85% occupied. Three additional block faces were observed between 50% and 85% occupied, occurring along Logan Court, Pearl Street, and 56th Avenue.

No illegally parked vehicles were observed in Mapleton during data collections.

The following table summarizes parking occupancy by block face at 9 AM.

Table 65: Mapleton Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					Weekday % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal	Total Weekday Demand (System-wide Peak)	
Clarkson St	E 57th Ave	E 58th Ave	W	22	1	0	0	0	1	5%
	E 58th Ave	E 57th Ave	E	20	4	0	0	0	4	20%
E 55th Ave	Driveway	Driveway	S	27	9	0	0	0	9	33%
	Lincoln St	Driveway	S	32	0	0	0	0	0	0%
	Logan Ct	Pennsylvania St	S	15	0	0	0	0	0	0%
	Pearl St	Pennsylvania St	N	4	0	0	0	0	0	0%
	Pearl St	Washington St	S	3	0	0	0	0	0	0%
	Pennsylvania St	Logan Ct	N	7	0	0	0	0	0	0%
	Pennsylvania St	Pearl St	S	12	0	0	0	0	0	0%
E 56th Ave	Washington St	Pearl St	N	6	0	0	0	0	0	0%
	Logan Ct	Logan St	S	9	5	0	0	0	5	56%
	Logan Ct	Pennsylvania St	S	9	0	0	0	0	0	0%
E 57th Ave	Pennsylvania St	Pearl St	S	7	0	0	0	0	0	0%
	Clarkson St	Emerson St	N	6	0	0	0	0	0	0%
Emerson St	Emerson St	Clarkson St	S	9	0	0	0	0	0	0%
	E 57th Ave	E 58th Ave	W	22	5	0	0	0	5	23%
Logan Ct	E 58th Ave	E 57th Ave	E	19	8	0	0	0	8	42%
	E 55th Ave	E 56th Ave	E	8	8	0	0	0	8	100%
Pearl St	E 56th Ave	E 55th Ave	W	14	9	0	0	0	9	64%
	E 55th Ave	E 56th Ave	E	9	7	0	0	0	7	78%
Pennsylvania St	E 56th Ave	E 55th Ave	W	0	0	0	0	0	0	0%
	E 55th Ave	E 56th Ave	E	20	7	0	0	0	7	35%
	E 56th Ave	E 55th Ave	W	26	8	0	0	0	8	31%
Total (Weekday System-wide Peak)				306	71	0	0	0	71	23%

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (9 AM) are highlighted in blue. Block faces with 0% occupancy across all times are highlighted in green.

Seven block faces were observed with individual parking demand peak occupancies greater than they exhibited during the system-wide peak. This was predominantly observed to occur during the 11 AM collection period. For the weekday, the overall composite peak was approximately 2% higher than the system peak.

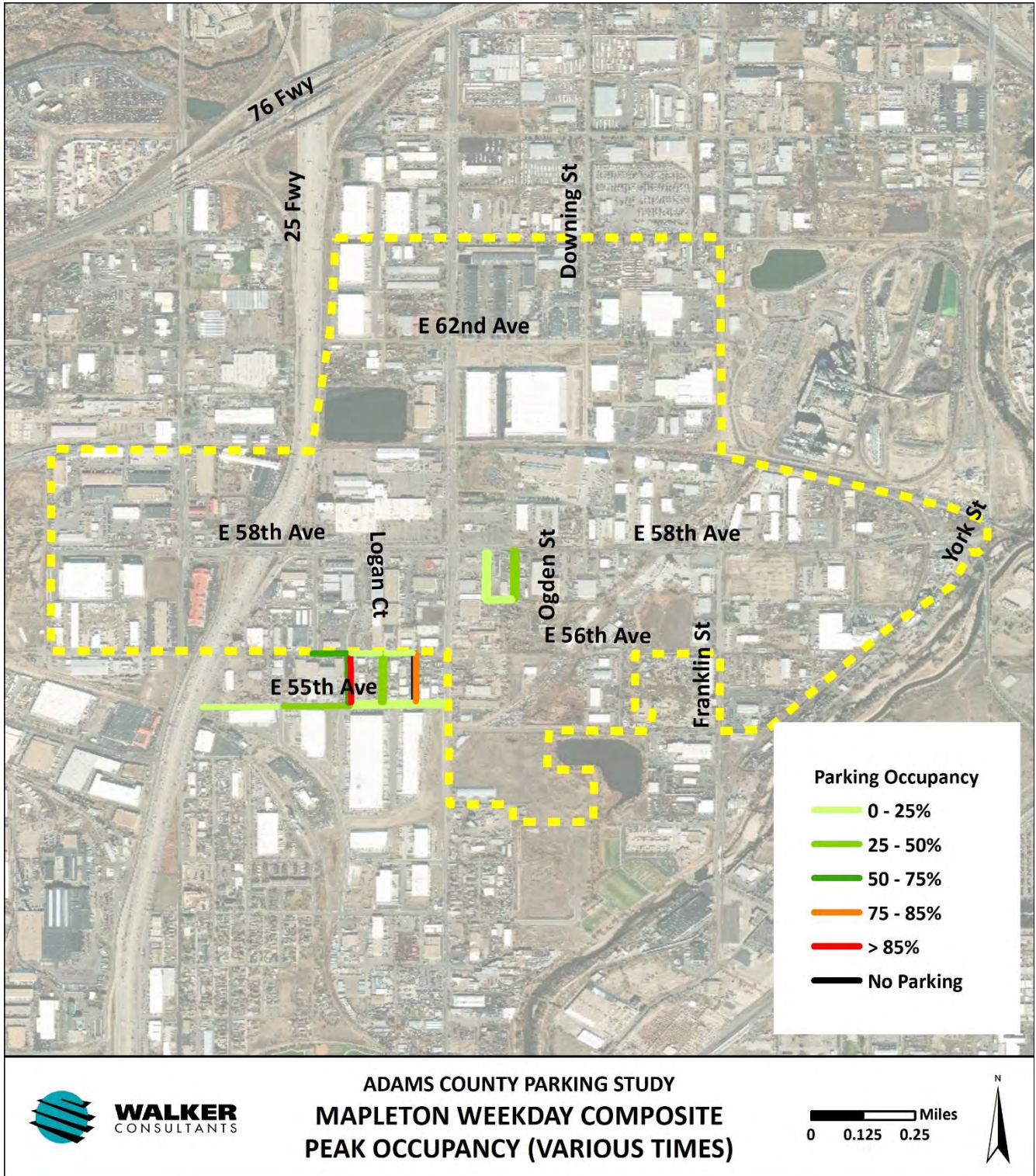
Table 66: Mapleton Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Peak % Occupancy (Across All Times)	Time that Peak Occurred
Clarkson St	E 57th Ave	E 58th Ave	W	22	1	0	0	0	1	5%	9:00:00 AM
	E 58th Ave	E 57th Ave	E	20	5	0	0	0	5	25%	11:00:00 AM
E 55th Ave	Driveway	Driveway	S	27	9	0	0	0	9	33%	9:00:00 AM
	Lincoln St	Driveway	S	32	0	0	0	0	0	0%	
	Logan Ct	Pennsylvania St	S	15	1	0	0	0	1	7%	11:00:00 AM
	Pearl St	Pennsylvania St	N	4	0	0	0	0	0	0%	
	Pearl St	Washington St	S	3	0	0	0	0	0	0%	
	Pennsylvania St	Logan Ct	N	7	0	0	0	0	0	0%	
	Pennsylvania St	Pearl St	S	12	0	0	0	0	0	0%	
	Washington St	Pearl St	N	6	0	0	0	0	0	0%	
E 56th Ave	Logan Ct	Logan St	S	9	5	0	0	0	5	56%	9:00:00 AM
	Logan Ct	Pennsylvania St	S	9	0	0	0	0	0	0%	
E 57th Ave	Pennsylvania St	Pearl St	S	7	1	0	0	0	1	14%	9:00:00 PM
	Clarkson St	Emerson St	N	6	0	0	0	0	0	0%	
Emerson St	Emerson St	Clarkson St	S	9	1	0	0	0	1	11%	11:00:00 AM
	E 57th Ave	E 58th Ave	W	22	6	0	0	0	6	27%	9:00:00 PM
Logan Ct	E 58th Ave	E 57th Ave	E	19	8	0	0	0	8	42%	9:00:00 AM
	E 55th Ave	E 56th Ave	E	8	8	0	0	0	8	100%	9:00:00 AM
Pearl St	E 56th Ave	E 55th Ave	W	14	10	0	0	0	10	71%	11:00:00 AM
	E 55th Ave	E 56th Ave	E	9	7	0	0	0	7	78%	9:00:00 AM
Pennsylvania St	E 56th Ave	E 55th Ave	W	0	0	0	0	0	0	0%	
	E 55th Ave	E 56th Ave	E	20	7	0	0	0	7	35%	9:00:00 AM
	E 56th Ave	E 55th Ave	W	26	9	0	0	0	9	35%	9:00:00 PM
Total (Weekday Composite)				306	78	0	0	0	78	25%	9:00 AM

Source: Walker Consultants

The following heat map is a composite of peak weekday parking demand per block face across all collection periods.

Figure 52: Mapleton Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the Mapleton neighborhood was performed on Saturday, September 7, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 22 vehicles (7% occupied) at 7 PM to the system-wide peak of 41 vehicles (13% occupied) at 1 PM.

The table below summarizes parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

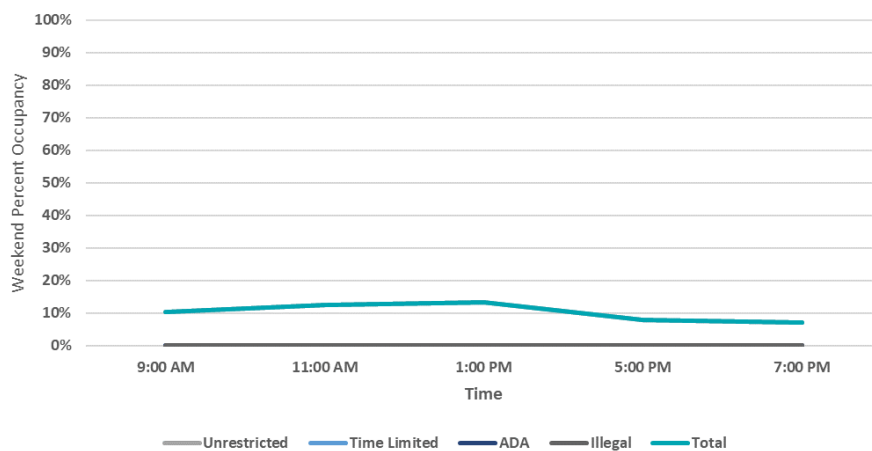
Table 67: Mapleton Observed Weekend Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	306	32	10%	0	0%	0	0%	0	0%	32	10%
11:00 AM	306	38	12%	0	0%	0	0%	0	0%	38	12%
1:00 PM	306	41	13%	0	0%	0	0%	0	0%	41	13%
5:00 PM	306	24	8%	0	0%	0	0%	0	0%	24	8%
7:00 PM	306	22	7%	0	0%	0	0%	0	0%	22	7%
Average	306	31	10%	0	0%	0	0%	0	0%	31	10%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the Mapleton neighborhood.

Figure 53: Mapleton Total Weekend Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekend peak at 1 PM, parking demand by block face ranged from 0% occupied (ten segments) to 43% occupied along Logan Court between 55th Avenue and 56th Avenue. There were no areas at or above 50% occupancy. There were no illegally parked vehicles observed in the Mapleton neighborhood on the weekend.

Table 68: Mapleton Weekend System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					Total Weekend Demand (System-wide Peak)	Weekend % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
Clarkson St	E 57th Ave	E 58th Ave	W	22	1	0	0	0	1	5%	
	E 58th Ave	E 57th Ave	E	20	2	0	0	0	2	10%	
E 55th Ave	Driveway	Driveway	S	27	5	0	0	0	5	19%	
	Lincoln St	Driveway	S	32	0	0	0	0	0	0%	
	Logan Ct	Pennsylvania St	S	15	3	0	0	0	3	20%	
	Pearl St	Pennsylvania St	N	4	0	0	0	0	0	0%	
	Pearl St	Washington St	S	3	0	0	0	0	0	0%	
	Pennsylvania St	Logan Ct	N	7	0	0	0	0	0	0%	
	Pennsylvania St	Pearl St	S	12	0	0	0	0	0	0%	
	Washington St	Pearl St	N	6	0	0	0	0	0	0%	
E 56th Ave	Logan Ct	Logan St	S	9	3	0	0	0	3	33%	
	Logan Ct	Pennsylvania St	S	9	1	0	0	0	1	11%	
	Pennsylvania St	Pearl St	S	7	0	0	0	0	0	0%	
E 57th Ave	Clarkson St	Emerson St	N	6	0	0	0	0	0	0%	
	Emerson St	Clarkson St	S	9	0	0	0	0	0	0%	
Emerson St	E 57th Ave	E 58th Ave	W	22	4	0	0	0	4	18%	
	E 58th Ave	E 57th Ave	E	19	7	0	0	0	7	37%	
Logan Ct	E 55th Ave	E 56th Ave	E	8	1	0	0	0	1	13%	
	E 56th Ave	E 55th Ave	W	14	6	0	0	0	6	43%	
Pearl St	E 55th Ave	E 56th Ave	E	9	2	0	0	0	2	22%	
	E 56th Ave	E 55th Ave	W	0	0	0	0	0	0	0%	
Pennsylvania St	E 55th Ave	E 56th Ave	E	20	2	0	0	0	2	10%	
	E 56th Ave	E 55th Ave	W	26	4	0	0	0	4	15%	
Total (Weekend System-wide Peak)				306	41	0	0	0	41	13%	

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (1 PM) are highlighted in blue. Block faces with 0% occupancy across all times are highlighted in green.

Seven block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. For the weekend, the overall composite peak was approximately 3% higher than the system peak.

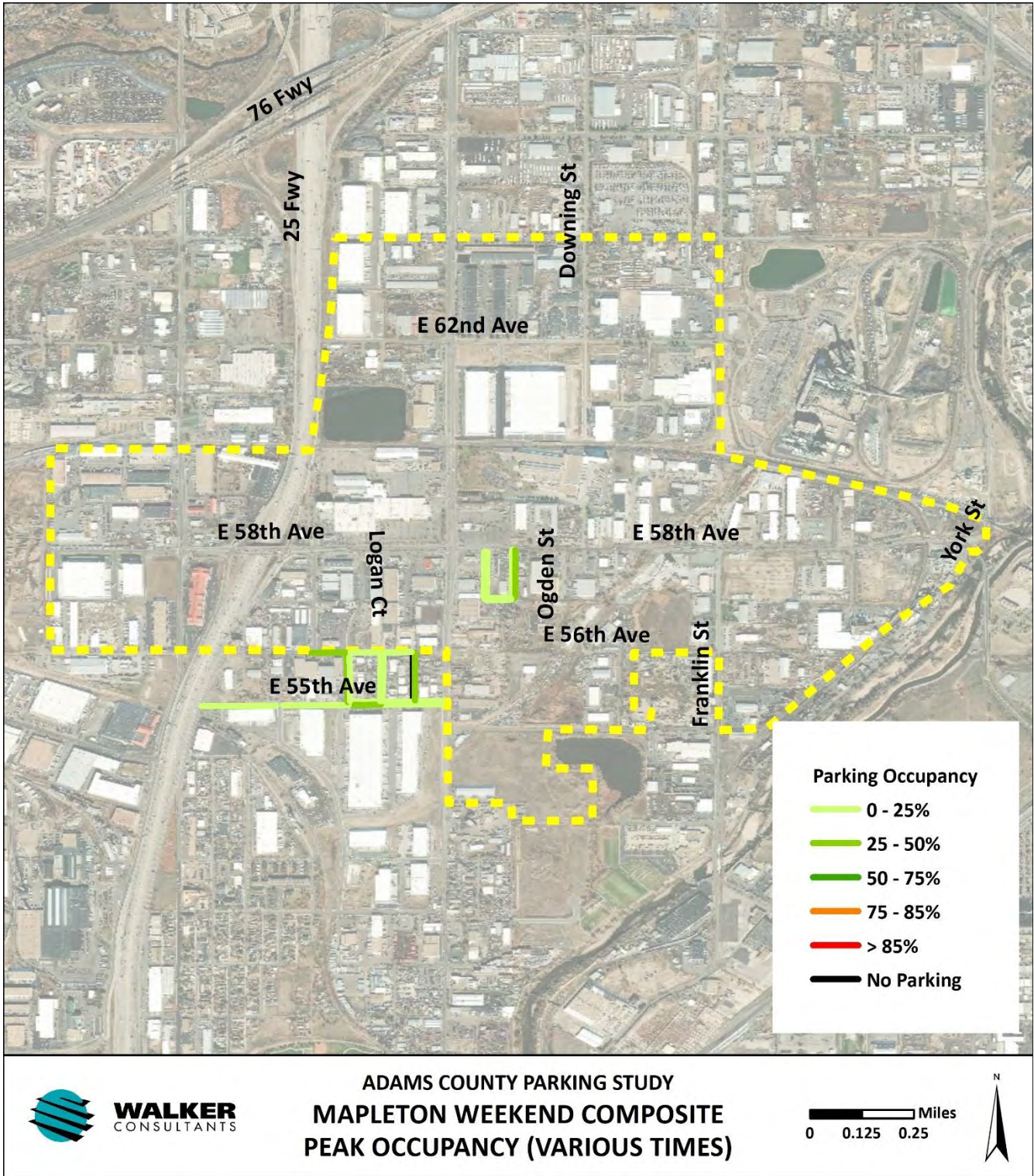
Table 69: Mapleton Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)	Time that Peak Occurred
Clarkson St	E 57th Ave	E 58th Ave	W	22	1	0	0	0	1	5%	1:00:00 PM
	E 58th Ave	E 57th Ave	E	20	3	0	0	0	3	15%	11:00:00 AM
E 55th Ave	Driveway	Driveway	S	27	5	0	0	0	5	19%	1:00:00 PM
	Lincoln St	Driveway	S	32	0	0	0	0	0	0%	
	Logan Ct	Pennsylvania St	S	15	4	0	0	0	4	27%	9:00:00 AM
	Pearl St	Pennsylvania St	N	4	0	0	0	0	0	0%	
	Pearl St	Washington St	S	3	0	0	0	0	0	0%	
	Pennsylvania St	Logan Ct	N	7	0	0	0	0	0	0%	
	Pennsylvania St	Pearl St	S	12	0	0	0	0	0	0%	
	Washington St	Pearl St	N	6	0	0	0	0	0	0%	
E 56th Ave	Logan Ct	Logan St	S	9	3	0	0	0	3	33%	1:00:00 PM
	Logan Ct	Pennsylvania St	S	9	1	0	0	0	1	11%	1:00:00 PM
	Pennsylvania St	Pearl St	S	7	0	0	0	0	0	0%	
E 57th Ave	Clarkson St	Emerson St	N	6	0	0	0	0	0	0%	
	Emerson St	Clarkson St	S	9	0	0	0	0	0	0%	
Emerson St	E 57th Ave	E 58th Ave	W	22	4	0	0	0	4	18%	1:00:00 PM
	E 58th Ave	E 57th Ave	E	19	9	0	0	0	9	47%	5:00:00 PM
Logan Ct	E 55th Ave	E 56th Ave	E	8	2	0	0	0	2	25%	9:00:00 AM
	E 56th Ave	E 55th Ave	W	14	7	0	0	0	7	50%	11:00:00 AM
Pearl St	E 55th Ave	E 56th Ave	E	9	3	0	0	0	3	33%	9:00:00 AM
	E 56th Ave	E 55th Ave	W	0	0	0	0	0	0	0%	9:00:00 AM
Pennsylvania St	E 55th Ave	E 56th Ave	E	20	2	0	0	0	2	10%	1:00:00 PM
	E 56th Ave	E 55th Ave	W	26	5	0	0	0	5	19%	7:00:00 PM
Total (Weekend Composite)				306	49	0	0	0	49	16%	1:00 PM

Source: Walker Consultants

The following heat map is a composite of peak parking demand for per block face across all collection periods.

Figure 54: Mapleton Heat Map of Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

MIDTOWN AT CLEAR CREEK

A total of 18 block faces were surveyed across three streets.

INVENTORY

Walker staff observed a total of approximately 155 public on-street parking spaces along all corridors identified for study in the Midtown at Clear Creek (“Midtown”) neighborhood. The table below shows parking supply by street and block face.

Table 70: Midtown Observed Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
Larsh Dr	Mid Block	W 66th Pl	W	10	0	0	10
	W 66th Pl	W 67th Ave	E	12	0	0	12
	W 67th Ave	W 67th Pl	E	10	0	0	10
	W 67th Pl	Mid Block	W	8	0	0	8
W 67th Ave	Alan Dr	Avrum Dr	N	12	0	0	12
	Alan Dr	Raritan Dr	S	12	0	0	12
	Avrum Dr	Alan Dr	S	8	0	2	10
	Avrum Dr	Larsh Dr	N	10	0	0	10
	Larsh Dr	Avrum Dr	S	10	0	0	10
	N Pecos St	Raritan Dr	N	10	0	0	10
	Raritan Dr	N Pecos St	S	3	0	0	3
W 68th Ave	Raritan St	Alan Dr	N	4	0	0	4
	Alan Dr	Raritan Dr	S	4	0	0	4
	Avrum Dr	Alan Dr	S	4	0	0	4
	Larsh Dr	Avrum Dr	S	10	0	0	10
	Morrison Dr	Warren Dr	S	10	0	0	10
	Ruth Way	Larsh Dr	S	10	0	0	10
Total On Street Parking				155	0	2	157

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the Midtown neighborhood was performed on Thursday, August 29, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. On-street parking demand for the neighborhood ranged from 19 vehicles (12% occupancy) at 9 AM to the system-wide peak of 38 vehicles (24% occupancy) at 9 PM.

The table below summarizes parking occupancy by user group across time of day across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

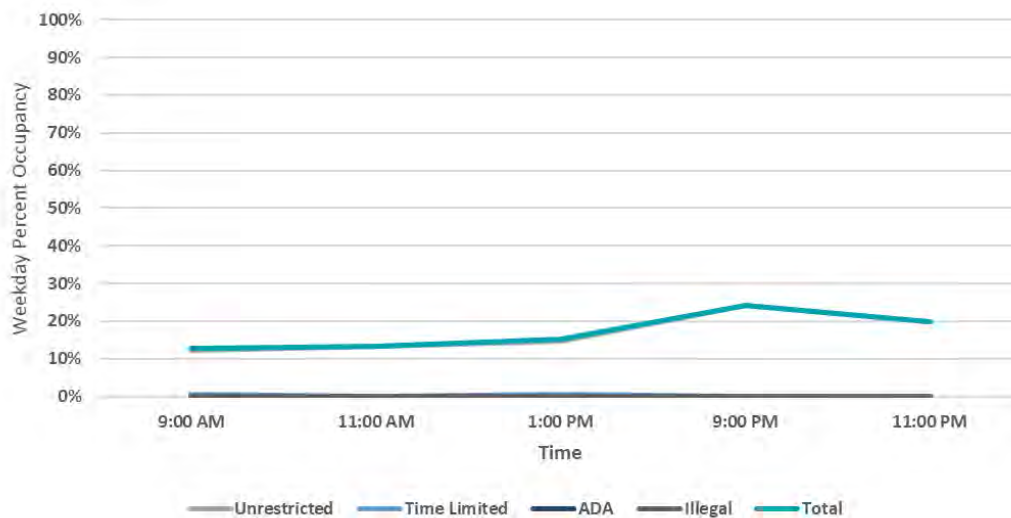
Table 71: Midtown Observed Weekday Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	157	19	12%	1	1%	0	0%	0	0%	20	13%
11:00 AM	157	21	13%	0	0%	0	0%	0	0%	21	13%
1:00 PM	157	23	15%	1	1%	0	0%	0	0%	24	15%
9:00 PM	157	38	24%	0	0%	0	0%	0	0%	38	24%
11:00 PM	157	31	20%	0	0%	0	0%	0	0%	31	20%
Average	157	26	17%	0	0%	0	0%	0	0%	27	17%

Source: Walker Consultants

The figure below shows total weekday parking occupancy by time for the Midtown neighborhood.

Figure 55: Midtown Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 9 PM, on-street parking demand by block face ranged from 0% occupied (5 segments) to 60% occupied along 67th Avenue between Pecos Street and Raritan Drive. There were no block faces observed to experience occupancies above 85% and three that were at or above 50% occupancy, occurring along 67th Avenue and 68th Avenue.

No illegally parked vehicles were observed in Midtown during the weekday.

The following table summarizes parking occupancy by block face at 9 PM.

Table 72: Midtown Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					Total Weekday Demand (System-wide Peak)	Weekday % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
Larsh Dr	Mid Block	W 66th Pl	W	10	0	0	0	0	0	0%	
	W 66th Pl	W 67th Ave	E	12	0	0	0	0	0	0%	
	W 67th Ave	W 67th Pl	E	10	1	0	0	0	1	33%	
	W 67th Pl	Mid Block	W	8	0	0	0	0	0	0%	
W 67th Ave	Alan Dr	Avrum Dr	N	12	0	0	0	0	0	0%	
	Alan Dr	Raritan Dr	S	12	2	0	0	0	2	20%	
	Avrum Dr	Alan Dr	S	10	0	0	0	0	0	0%	
	Avrum Dr	Larsh Dr	N	10	1	0	0	0	1	10%	
	Larsh Dr	Avrum Dr	S	10	1	0	0	0	1	10%	
	N Pecos St	Raritan Dr	N	10	6	0	0	0	6	60%	
	Raritan Dr	N Pecos St	S	3	1	0	0	0	1	13%	
Raritan St	Alan Dr	N	4	5	0	0	0	5	50%		
W 68th Ave	Alan Dr	Raritan Dr	S	4	6	0	0	0	6	50%	
	Avrum Dr	Alan Dr	S	4	5	0	0	0	5	42%	
	Larsh Dr	Avrum Dr	S	10	3	0	0	0	3	38%	
	Morrison Dr	Warren Dr	S	10	3	0	0	0	3	30%	
	Ruth Way	Larsh Dr	S	10	3	0	0	0	3	30%	
	Warren Dr	Ruth Way	S	8	1	0	0	0	1	8%	
Total (Weekday System-wide Peak)				157	38	0	0	0	38	24%	

Source: Walker Consultants

The next table a composite of the individual parking demand peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (9 PM) are highlighted in blue. Block faces with 0% occupancy across all times are highlighted in green.

Five block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. For the weekday, the overall composite peak was approximately 4% higher than the system peak.

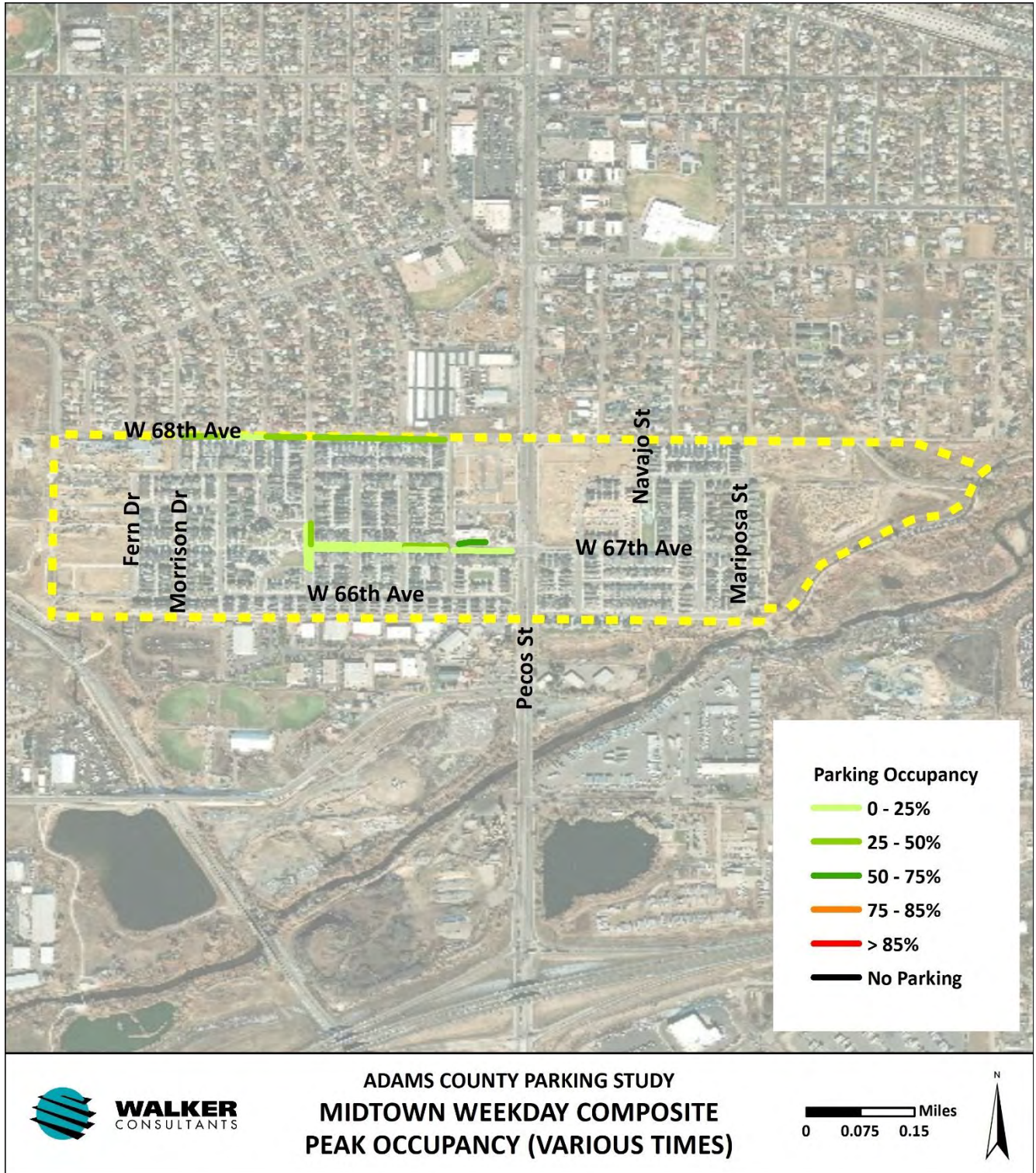
Table 73: Midtown Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Peak Weekday % Occupancy (Across All Times)	Time that Peak Occurred
Larsh Dr	Mid Block	W 66th Pl	W	10	0	0	0	0	0	0%	
	W 66th Pl	W 67th Ave	E	12	0	0	0	0	0	0%	
	W 67th Ave	W 67th Pl	E	10	1	0	0	0	1	33%	9:00:00 PM
	W 67th Pl	Mid Block	W	8	1	0	0	0	1	25%	11:00:00 AM
W 67th Ave	Alan Dr	Avrum Dr	N	12	0	0	0	0	0	0%	
	Alan Dr	Raritan Dr	S	12	2	0	0	0	2	20%	9:00:00 PM
	Avrum Dr	Alan Dr	S	10	2	0	0	0	2	20%	11:00:00 AM
	Avrum Dr	Larsh Dr	N	10	2	0	0	0	2	20%	1:00:00 PM
	Larsh Dr	Avrum Dr	S	10	1	0	0	0	1	10%	9:00:00 PM
	N Pecos St	Raritan Dr	N	10	6	0	0	0	6	60%	9:00:00 PM
	Raritan Dr	N Pecos St	S	3	2	0	0	0	2	25%	9:00:00 AM
	Raritan St	Alan Dr	N	4	5	0	0	0	5	50%	9:00:00 PM
W 68th Ave	Alan Dr	Raritan Dr	S	4	6	0	0	0	6	50%	9:00:00 PM
	Avrum Dr	Alan Dr	S	4	5	0	0	0	5	42%	9:00:00 PM
	Larsh Dr	Avrum Dr	S	10	3	0	0	0	3	38%	9:00:00 PM
	Morrison Dr	Warren Dr	S	10	3	0	0	0	3	30%	9:00:00 PM
	Ruth Way	Larsh Dr	S	10	3	0	0	0	3	30%	9:00:00 PM
	Warren Dr	Ruth Way	S	8	2	0	0	0	2	17%	1:00:00 PM
Total (Weekday Composite)				157	44	0	0	0	44	28%	9:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekday parking demand per block face across all collection periods.

Figure 56: Midtown Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the Midtown neighborhood was performed on Saturday, August 31, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 35 vehicles (22% occupied) at 9 AM to the system-wide peak of 43 vehicles (27% occupied) at 5 PM.

The table below summarizes parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

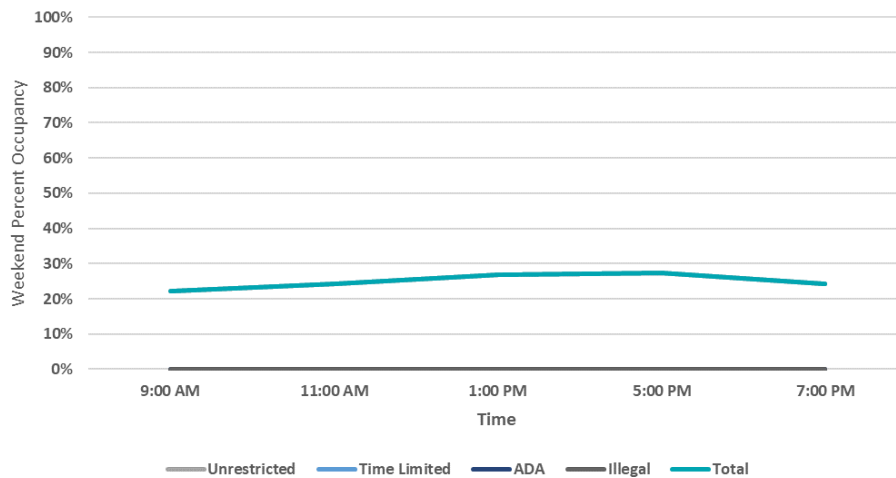
Table 74: Midtown Observed Weekend Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	157	35	22%	0	0%	0	0%	0	0%	35	22%
11:00 AM	157	38	24%	0	0%	0	0%	0	0%	38	24%
1:00 PM	157	42	27%	0	0%	0	0%	0	0%	42	27%
5:00 PM	157	43	27%	0	0%	0	0%	0	0%	43	27%
7:00 PM	157	38	24%	0	0%	0	0%	0	0%	38	24%
Average	157	39	25%	0	0%	0	0%	0	0%	39	25%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the Midtown neighborhood.

Figure 57: Midtown Total Weekend Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekend peak at 5 PM, parking demand by block face ranged from 0% occupied (five segments) to 75% occupied along 67th Avenue between Raritan Drive and Pecos Street. There were no areas at or above 85% occupancy, and four block faces that were observed between 50% and 85% occupied during the system-wide peak. There were no illegally parked vehicles observed in Midtown on the weekend.

Table 75: Midtown Weekend System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					Total Weekend Demand (System-wide Peak)	Weekend % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
Larsh Dr	Mid Block	W 66th Pl	W	4	1	0	0	0	1	25%	
	W 66th Pl	W 67th Ave	E	4	0	0	0	0	0	0%	
	W 67th Ave	W 67th Pl	E	3	0	0	0	0	0	0%	
	W 67th Pl	Mid Block	W	4	0	0	0	0	0	0%	
W 67th Ave	Alan Dr	Avrum Dr	N	10	1	0	0	0	1	10%	
	Alan Dr	Raritan Dr	S	10	1	0	0	0	1	10%	
	Avrum Dr	Alan Dr	S	10	0	0	0	0	0	0%	
	Avrum Dr	Larsh Dr	N	10	5	0	0	0	5	50%	
	Larsh Dr	Avrum Dr	S	10	1	0	0	0	1	10%	
	N Pecos St	Raritan Dr	N	10	6	0	0	0	6	60%	
	Raritan Dr	N Pecos St	S	8	6	0	0	0	6	75%	
	Raritan St	Alan Dr	N	10	3	0	0	0	3	30%	
W 68th Ave	Alan Dr	Raritan Dr	S	12	2	0	0	0	2	17%	
	Avrum Dr	Alan Dr	S	12	4	0	0	0	4	33%	
	Larsh Dr	Avrum Dr	S	8	0	0	0	0	0	0%	
	Morrison Dr	Warren Dr	S	10	5	0	0	0	5	50%	
	Ruth Way	Larsh Dr	S	10	4	0	0	0	4	40%	
	Warren Dr	Ruth Way	S	12	4	0	0	0	4	33%	
Total (Weekend System-wide Peak)				157	43	0	0	0	43	27%	

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (5 PM) are highlighted in blue. Block faces with 0% occupancy across all times are highlighted in green.

Ten block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. Most of these instances occur in the morning. For the weekend, the overall composite peak was approximately 9% higher than the system peak.



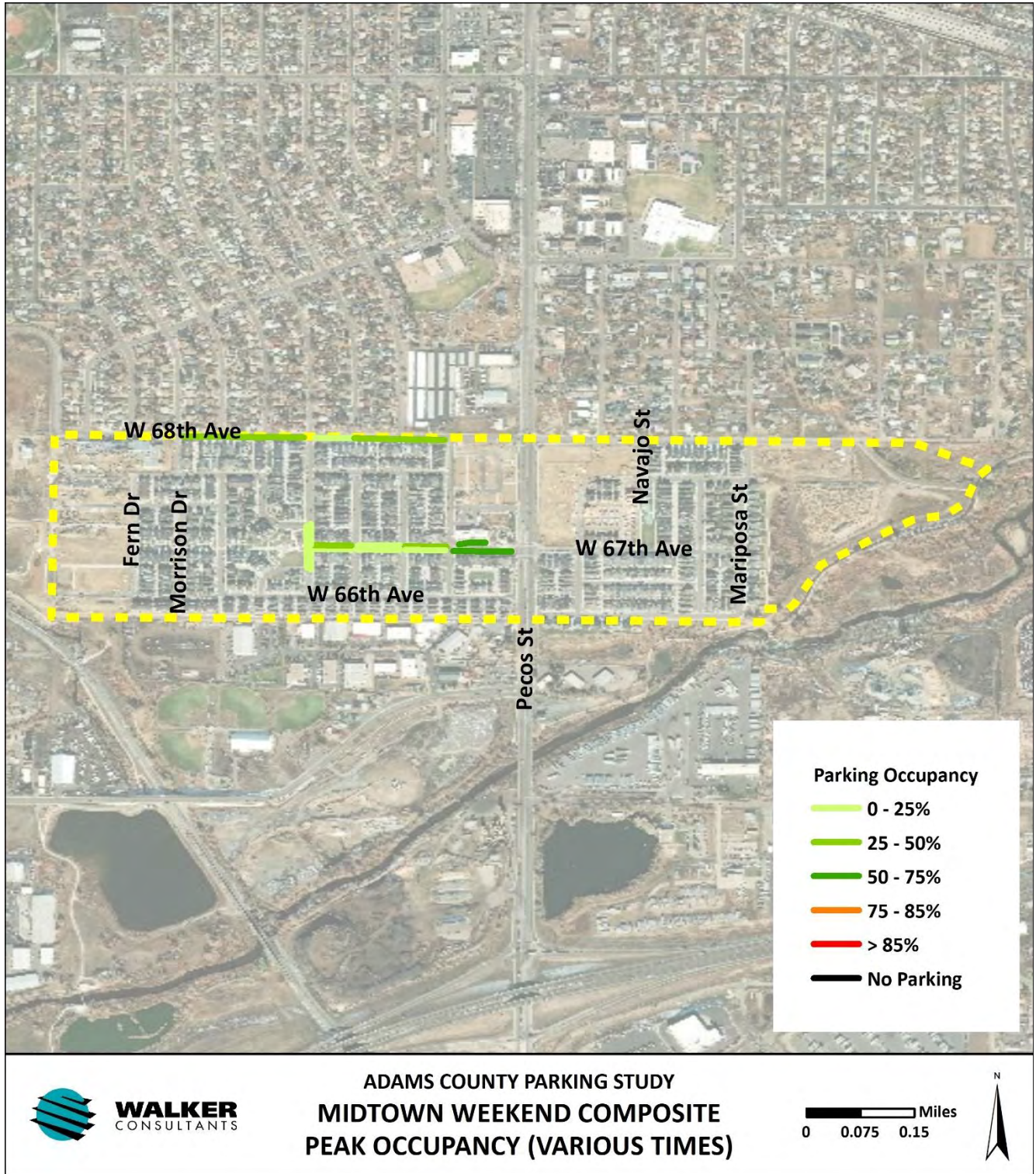
Table 76: Midtown Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)	Time that Peak Occurred
Larsh Dr	Mid Block	W 66th Pl	W	4	1	0	0	0	1	25%	5:00:00 PM
	W 66th Pl	W 67th Ave	E	4	1	0	0	0	1	25%	1:00:00 PM
	W 67th Ave	W 67th Pl	E	3	0	0	0	0	0	0%	
	W 67th Pl	Mid Block	W	4	1	0	0	0	1	25%	7:00:00 PM
W 67th Ave	Alan Dr	Avrum Dr	N	10	1	0	0	0	1	10%	5:00:00 PM
	Alan Dr	Raritan Dr	S	10	2	0	0	0	2	20%	9:00:00 AM
	Avrum Dr	Alan Dr	S	10	1	0	0	0	1	10%	9:00:00 AM
	Avrum Dr	Larsh Dr	N	10	5	0	0	0	5	50%	5:00:00 PM
	Larsh Dr	Avrum Dr	S	10	2	0	0	0	2	20%	7:00:00 PM
	N Pecos St	Raritan Dr	N	10	7	0	0	0	7	70%	1:00:00 PM
	Raritan Dr	N Pecos St	S	8	6	0	0	0	6	75%	5:00:00 PM
	Raritan St	Alan Dr	N	10	4	0	0	0	4	40%	1:00:00 PM
W 68th Ave	Alan Dr	Raritan Dr	S	12	6	0	0	0	6	50%	1:00:00 PM
	Avrum Dr	Alan Dr	S	12	5	0	0	0	5	42%	9:00:00 AM
	Larsh Dr	Avrum Dr	S	8	2	0	0	0	2	25%	9:00:00 AM
	Morrison Dr	Warren Dr	S	10	5	0	0	0	5	50%	5:00:00 PM
	Ruth Way	Larsh Dr	S	10	4	0	0	0	4	40%	5:00:00 PM
	Warren Dr	Ruth Way	S	12	4	0	0	0	4	33%	5:00:00 PM
Total (Weekend Composite)				157	57	0	0	0	57	36%	5:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekend parking demand per block face across all collection periods.

Figure 58: Midtown Heat Map of Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

NORTH PECOS

A total of four block faces were surveyed along 52nd Avenue, the only public street with on-street parking within the North Pecos neighborhood.

INVENTORY

Walker staff observed a total of approximately 20 public on-street parking spaces along all corridors identified for study in North Pecos. The table below summarizes parking supply by street and block face. It should be noted that the parking along the south side of 52nd Avenue lies within the City and County of Denver.

Table 77: North Pecos Observed Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
W 52nd Ave	Cul de sac	Kalamath St	N	10	0	0	10
	Kalamath St	Cul de sac	S	0	0	0	0
	N Pecos St	Osage St	S	10	0	0	10
	Osage St	Kalamath St	S	0	0	0	0
Total On Street Parking				20	0	0	20

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the North Pecos neighborhood was performed on Wednesday, August 28, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. On-street parking demand for the neighborhood ranged from 5 vehicles (25% occupied) at 9 AM to the system-wide peak of 10 vehicles (50% occupied) at 9 PM and 11 PM.

The table below summarizes parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

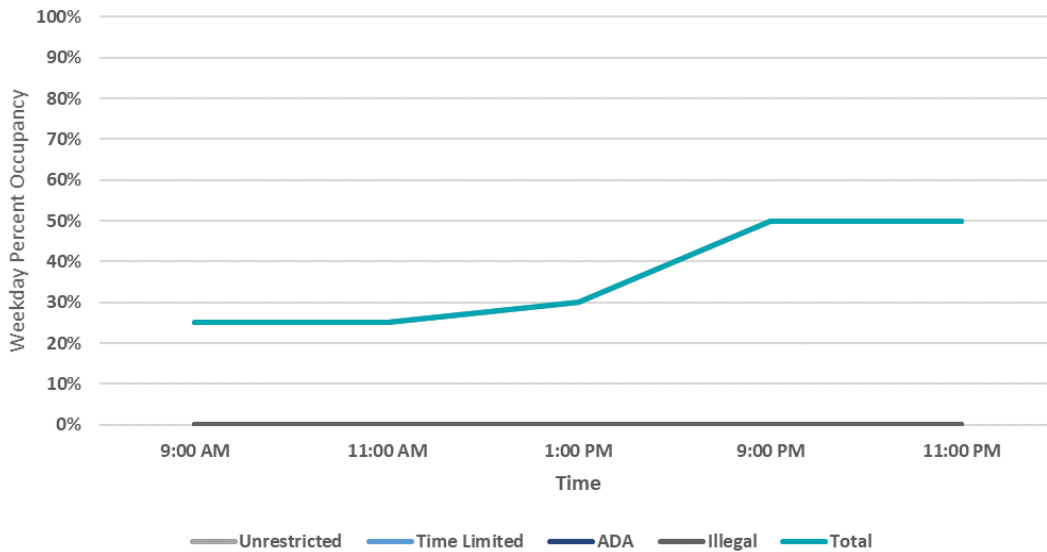
Table 78: North Pecos Observed Weekday Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	20	5	25%	0	0%	0	0%	0	0%	5	25%
11:00 AM	20	5	25%	0	0%	0	0%	0	0%	5	25%
1:00 PM	20	6	30%	0	0%	0	0%	0	0%	6	30%
9:00 PM	20	10	50%	0	0%	0	0%	0	0%	10	50%
11:00 PM	20	10	50%	0	0%	0	0%	0	0%	10	50%
Average	20	7	36%	0	0%	0	0%	0	0%	7	36%

Source: Walker Consultants

The figure below shows total weekday parking occupancy by time for the North Pecos neighborhood.

Figure 59: North Pecos Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 9 PM and 11 PM, on-street parking demand by block face ranged from 0% occupied (2 segments) to 80% occupied along 52nd Avenue between Kalamath Street and the cul-de-sac toward the railroad tracks and eastern edge of the neighborhood boundary. There were no areas observed to experience occupancies above 85% occupancy. One block face was observed at or above 50% occupancy.

No illegally parked vehicles were observed in North Pecos during the weekday.

The following table summarizes parking occupancy by block face at 9 PM and 11 PM.

Table 79: North Pecos Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					Total Weekday Demand (System-wide Peak)	Weekday % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
W 52nd Ave	Cul de sac	Kalamath St	N	10	8	0	0	0	8	80%	
	Kalamath St	Cul de sac	S	0	0	0	0	0	0	0%	
	N Pecos St	Osage St	S	10	2	0	0	0	2	20%	
	Osage St	Kalamath St	S	0	0	0	0	0	0	0%	
Total (Weekday System-wide Peak)				20	10	0	0	0	10	50%	

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (9 PM and 11 PM) are highlighted in blue. Block faces with 0% occupancy across all times are highlighted in green.

No block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak of 9 PM and 11 PM.

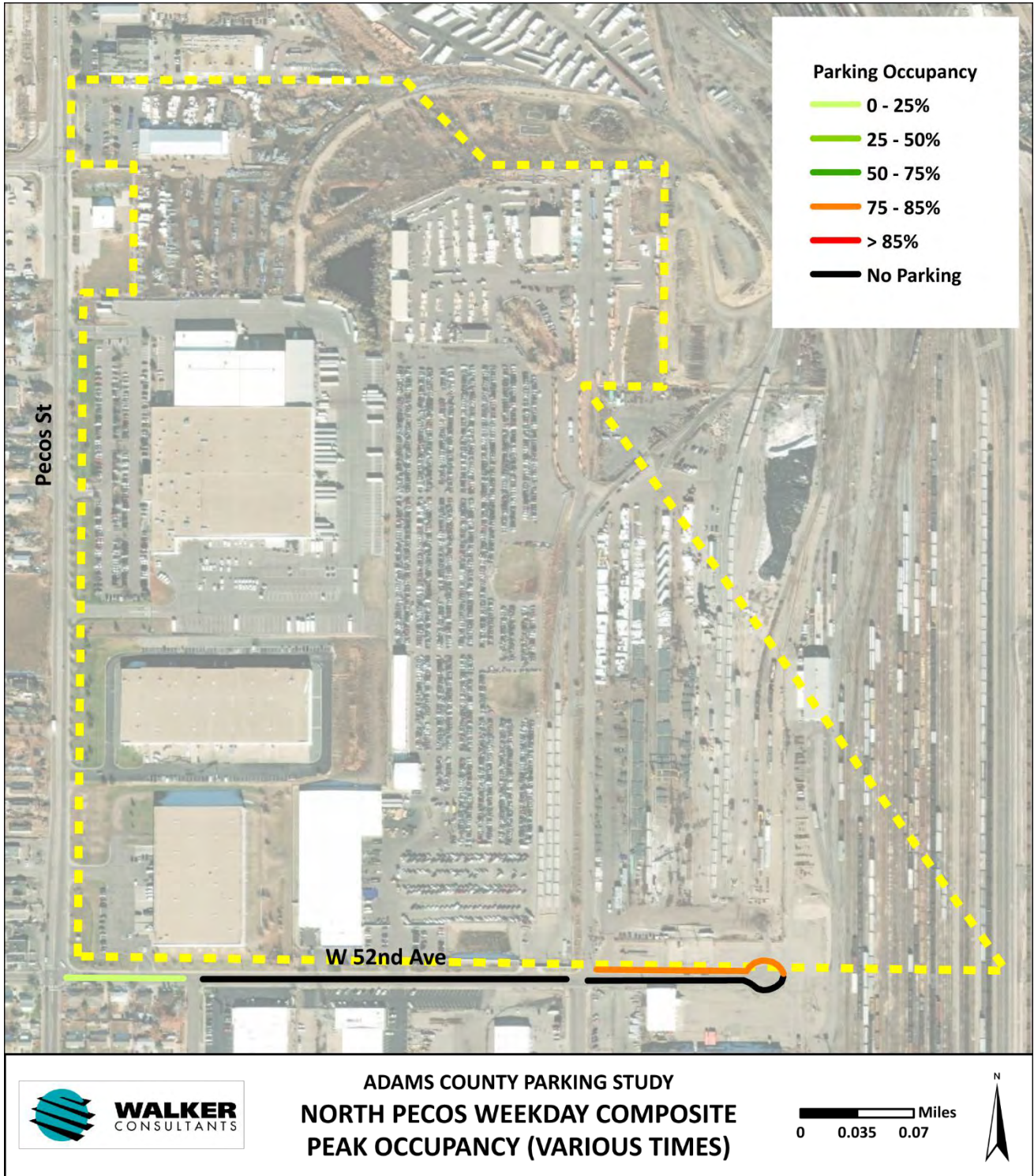
Table 80: North Pecos Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Peak Weekday % Occupancy (Across All Times)	Time that Peak Occurred
W 52nd Ave	Cul de sac	Kalamath St	N	10	8	0	0	0	8	80%	11:00:00 PM
	Kalamath St	Cul de sac	S	0	0	0	0	0	0	0%	
	N Pecos St	Osage St	S	10	2	0	0	0	2	20%	11:00:00 PM
	Osage St	Kalamath St	S	0	0	0	0	0	0	0%	
Total (Weekday Composite)				20	10	0	0	0	10	50%	11:00 PM

Source: Walker Consultants

The following heat map is a composite of peak parking demand for per block face across all collection periods. In this case, the map also represents system-wide peak occupancy, which occurred at 9 PM and 11 PM.

Figure 60: North Pecos Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the North Pecos neighborhood was performed on Saturday, August 31, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 1 vehicle (5% occupied) at 9 AM, 5 PM, and 7 PM to the system-wide peak of 2 vehicles (10% occupied) at 11 AM and 1 PM.

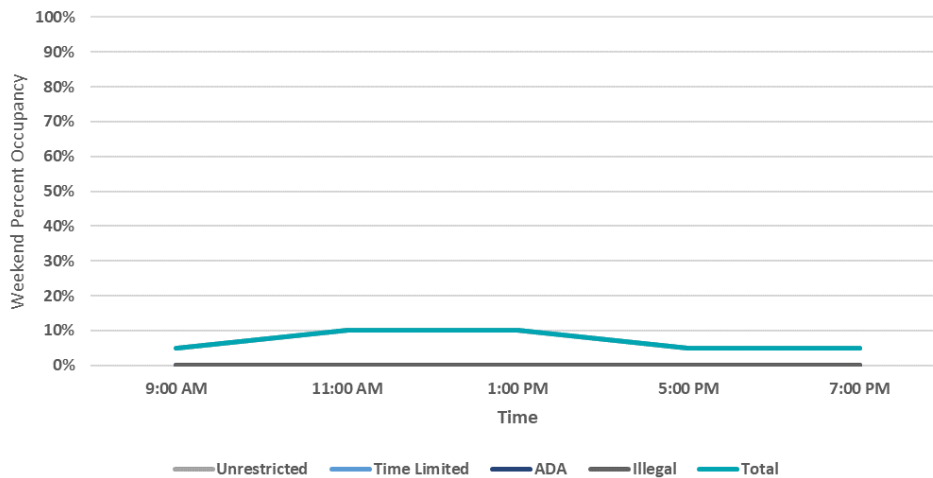
The table below summarizes parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

Table 81: North Pecos Observed Weekend Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	20	1	5%	0	0%	0	0%	0	0%	1	5%
11:00 AM	20	2	10%	0	0%	0	0%	0	0%	2	10%
1:00 PM	20	2	10%	0	0%	0	0%	0	0%	2	10%
5:00 PM	20	1	5%	0	0%	0	0%	0	0%	1	5%
7:00 PM	20	1	5%	0	0%	0	0%	0	0%	1	5%
Average	20	1	7%	0	0%	0	0%	0	0%	1	7%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the North Pecos neighborhood.

Figure 61: North Pecos Total Weekend Parking Occupancy by Percent


Source: Walker Consultants

During the system-wide weekend peak at 11 AM and 1 PM, demand by block face ranged from 0% occupied (three segments) to 20% occupied. The greatest demand was observed on the south side of 52nd Avenue. There were no areas observed above 50% occupancy. There were no illegally parked vehicles observed in North Pecos on the weekend.

Table 82: North Pecos Weekend System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					Total Weekend Demand (System-wide Peak)	Weekend % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
W 52nd Ave	Cul de sac	Kalamath St	N	10	0	0	0	0	0	0	0%
	Kalamath St	Cul de sac	S	0	0	0	0	0	0	0	0%
	N Pecos St	Osage St	S	10	2	0	0	0	2	2	20%
	Osage St	Kalamath St	S	0	0	0	0	0	0	0	0%
Total (Weekend System-wide Peak)				20	2	0	0	0	2	10%	

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (11 AM and 1 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

No block faces were observed with individual peak occupancies greater the system-wide peak.

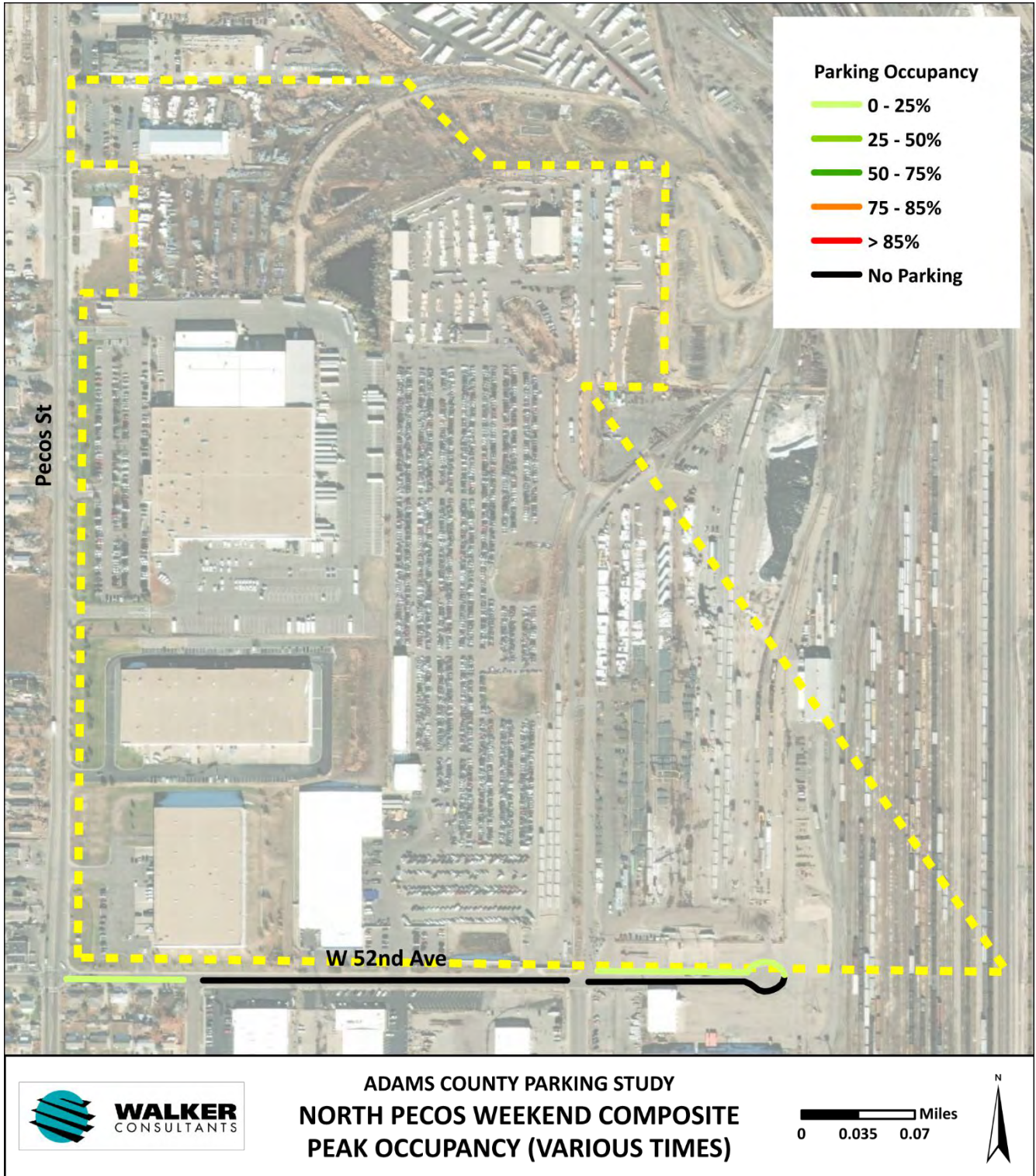
Table 83: North Pecos Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)							Time that Peak Occurred
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)		
W 52nd Ave	Cul de sac	Kalamath St	N	10	0	0	0	0	0	0%	1:00:00 PM	
	Kalamath St	Cul de sac	S	0	0	0	0	0	0	0%		
	N Pecos St	Osage St	S	10	2	0	0	0	2	20%	1:00:00 PM	
	Osage St	Kalamath St	S	0	0	0	0	0	0	0%		
Total (Weekend Composite)				20	2	0	0	0	2	10%	1:00 PM	

Source: Walker Consultants

The following heat map is a composite of peak weekend parking demand per block face across all collection periods. In this case, this map also represents system-wide peak occupancy, which occurred at 11AM and 1 PM.

Figure 62: North Pecos Heat Map of Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants



PERL MACK

A total of 94 block faces were surveyed across 19 streets. One off-street surface lot was also surveyed.

INVENTORY

Walker staff observed a total of approximately 1,337 public on-street parking spaces along all corridors identified for study in the Perl Mack neighborhood, as well as 33 off-street parking spaces at the Anythink Perl Mack Library Lot located at 7672 Hilltop Circle.

The tables below summarize parking supply by street and block face as well as the parking supply for the library.

Table 84: Perl Mack Observed On-street Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
Alan Dr	Jordan Dr	W 69th Ave	W	20	0	0	20
	Jordan Dr	W 72nd Ave	W	45	0	0	45
	W 68th Ave	W 69th Ave	E	27	0	0	27
	W 69th Ave	Jordan Dr	E	19	0	0	19
	W 69th Ave	W 68th Ave	W	19	0	0	19
	W 72nd Ave	Jordan Dr	E	38	0	0	38
Bronco Rd	Cortez St	Cragmore St	W	7	0	0	7
	Cortez St	Mid Block	E	7	0	0	7
	Cragmore St	Cortez St	E	6	0	0	6
	Cragmore St	Cuchara St	W	10	0	0	10
	Cuchara St	Cragmore St	E	5	0	0	5
	Cuchara St	Del Norte St	W	8	0	0	8
	Del Norte St	Cuchara St	E	6	0	0	6
Cortez St	Mid Block	Cortez St	W	8	0	0	8
	Bronco Rd	Dead End	S	4	0	0	4
	Bronco Rd	Greenwood Blvd	N	38	0	0	38
	Dead End	Bronco Rd	N	4	0	0	4
Cragmore St	Greenwood Blvd	Bronco Rd	S	39	0	0	39
	Bronco Rd	Greenwood Blvd	N	31	0	0	31
Cuchara St	Greenwood Blvd	Bronco Rd	S	31	0	0	31
	Bronco Rd	Greenwood Blvd	N	32	0	0	32
Dakin St	Bronco Rd	Greenwood Blvd	S	32	0	0	32
	Cortez St	Del Norte St	W	38	0	0	38
	Del Norte St	Cortez St	E	39	0	0	39
	Del Norte St	Hilltop Cir	W	5	0	0	5
Del Norte St	Hilltop Cir	Del Norte St	E	14	0	0	14
	Drake St	Greenwood Blvd	N	29	0	0	29
El Paso Blvd	Greenwood Blvd	Drake St	S	21	0	0	21
	El Paso Cir	Navajo St	S	16	0	0	16
	Navajo St	El Paso Cir	N	9	0	0	9
	Navajo St	Osage St	S	11	0	0	11
Hilltop Cir	Osage St	Navajo St	N	7	0	0	7
	Bronco Rd	Dakin St	S	19	0	0	19
	Dakin St	Drake St	N	11	0	0	11
	Dakin St	Erie St	W	10	0	0	10
	El Paso Blvd	Elbert St	E	8	0	0	8
	Elbert St	El Paso Blvd	W	8	0	0	8
	Elbert St	Erie St	E	8	0	0	8
Jordan Dr	Erie St	Dakin St	E	6	0	0	6
	Erie St	Elbert St	W	10	0	0	10
	Alan Dr	Raritan St	S	9	0	0	9
	Raritan St	Alan Dr	N	5	0	0	5
Lipan St	Raritan St	Samuel Dr	S	10	0	0	10
	Samuel Dr	Raritan St	N	10	0	0	10
	W 70th Ave	W 70th Pl	E	12	0	0	12
	W 70th Pl	W 70th Ave	W	10	0	0	10
Lipan St	W 70th Pl	W 71st Pl	E	21	0	0	21
	W 71st Pl	W 70th Pl	W	21	0	0	21



Quivas St	Raritan St	W 72nd Ave	W	23	0	0	23
	W 72nd Ave	Raritan St	W	24	0	0	24
Raritan St	Jordan Dr	Quivas St	W	15	0	0	15
	Quivas St	Jordan Dr	E	14	0	0	14
	Quivas St	W 72nd Ave	W	23	0	0	23
	W 72nd Ave	Quivas St	E	15	0	0	15
Samuel Dr	Jordan Dr	N Pecos St	E	15	0	0	15
	Jordan Dr	W 72nd Ave	W	35	0	0	35
	N Pecos St	Jordan Dr	W	12	0	0	12
	Quivas St	W 73rd Ave	E	23	0	0	23
	W 72nd Ave	Jordan Dr	E	39	0	0	39
	W 72nd Ave	W 73rd Ave	W	14	0	0	14
	W 73rd Ave	Quivas St	W	29	0	0	29
W 69th Ave	W 73rd Ave	W 72nd Ave	E	14	0	0	14
	Alan Dr	N Pecos St	S	7	0	0	7
W 70th Ave	N Pecos St	Alan Dr	N	14	0	0	14
	Mariposa St	Lipan St	S	4	0	0	4
	N Pecos St	Osage St	S	3	0	0	3
	Navajo St	Mariposa St	S	4	0	0	4
W 71st Pl	Osage St	Navajo St	S	0	0	0	0
	Lipan St	Mariposa St	N	7	0	0	7
	Mariposa St	Lipan St	S	9	0	0	9
	Mariposa St	N Pecos St	N	21	0	0	21
W 76th Ave	N Pecos St	Mariposa St	S	30	0	0	30
	N Pecos St	Quivas St	N	4	0	0	4
	Quivas St	N Pecos St	S	4	0	0	4
	Quivas St	Turner Dr	N	3	0	0	3
	Raritan St	Shoshone St	N	4	0	0	4
	Raritan St	Turner Dr	S	5	0	0	5
	Shoshone St	Raritan St	S	3	0	0	3
	Shoshone St	Tejon St	N	4	0	0	4
	Tejon St	Shoshone St	S	10	0	0	10
	Tejon St	Umatilla St	N	7	0	0	7
	Turner Dr	Quivas St	S	3	0	0	3
	Turner Dr	Raritan St	N	6	0	0	6
	Umatilla St	Tejon St	S	13	0	0	13
	Umatilla St	Vallejo St	N	7	0	0	7
	Vallejo St	Umatilla St	S	8	0	0	8
	Vallejo St	Wynadot St	N	8	0	0	8
	Wynadot St	Vallejo St	S	12	0	0	12
Wynadot St	Zane St	N	7	0	0	7	
Zane St	Wynadot St	S	10	0	0	10	
Zane St	Zuni St	N	4	0	0	4	
Zuni St	Zane St	S	8	0	0	8	
Zuni St	Dead End	W 76th Ave	E	5	0	0	5
	W 76th Ave	Dead End	W	5	0	0	5
Total On Street Parking				1,337	0	0	1,337

Source: Walker Consultants

Table 85: Perl Mack Observed Off-street Parking Supply by Facility

Off-street Lot	Un-restricted	Time Limited	ADA	Total
Anythink Perl Mack Library	31	0	2	33

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the Perl Mack neighborhood was performed on Wednesday, August 28, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. On-street parking demand for the neighborhood ranged from 392 vehicles (29% occupied) at 9 AM to the system-wide peak of 530 vehicles (40% occupied) at 9 PM. Off-street parking demand at the Perl Mack Library surface lot ranged from 0 vehicles at 9 PM and 11 PM to 12 vehicles (39% occupied) at 9 AM.

The tables below summarize parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

Table 86: Perl Mack Observed Weekday On-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	1,337	392	29%	0	0%	0	0%	6	0%	393	29%
11:00 AM	1,337	412	31%	0	0%	0	0%	5	0%	412	31%
1:00 PM	1,337	398	30%	0	0%	1	0%	5	0%	398	30%
9:00 PM	1,337	530	40%	0	0%	0	0%	9	1%	530	40%
11:00 PM	1,337	518	39%	0	0%	0	0%	9	1%	518	39%
Average	1,337	450	34%	0	0%	0	0%	7	0%	450	34%

Source: Walker Consultants

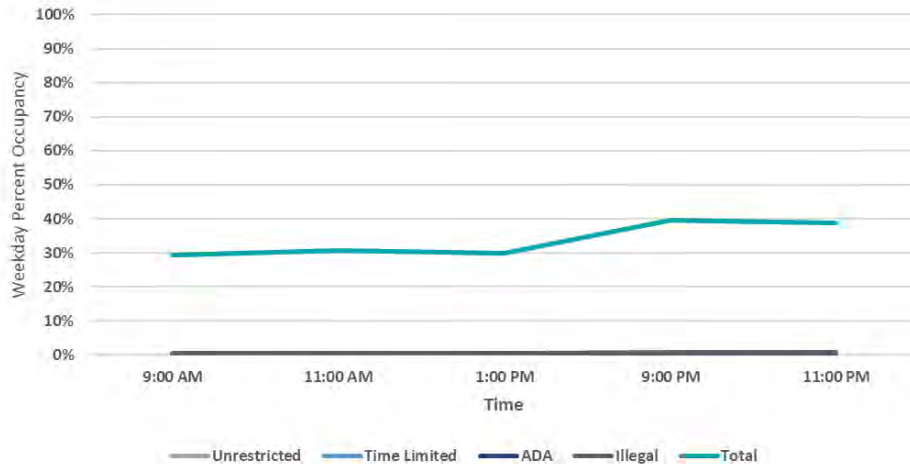
Table 87: Perl Mack Observed Weekday Off-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	31	12	39%	0	0%	0	0%	0	0%	13	42%
11:00 AM	31	7	23%	0	0%	0	0%	0	0%	7	23%
1:00 PM	31	5	16%	0	0%	1	3%	0	0%	5	16%
9:00 PM	31	0	0%	0	0%	0	0%	0	0%	0	0%
11:00 PM	31	0	0%	0	0%	0	0%	0	0%	0	0%
Average	31	5	15%	0	0%	0	1%	0	0%	5	16%

Source: Walker Consultants

The figure below shows total weekday parking occupancy by time for the Perl Mack neighborhood.

Figure 63: Perl Mack Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 9 PM, on-street parking demand by block face ranged from 0% occupied (25 segments) to 100% occupied. Full occupancy was observed along Samuel Drive between Quivas Street and 73rd Avenue, 76th Avenue between Shoshone Street and Raritan Street, and 76th Avenue between Tejon Street and Umatilla Street. Eight block faces were observed with occupancies over 85% during the system-wide peak. An additional 19 block faces were observed with occupancies between 50% and 85% during the peak period.

The block faces at or above 85% occupancy are:

- Bronco Road between Cortez Street and Cragmore Street, Westside
- Dakin Street between Cortez Street and Del Norte Street, Eastside
- Samuel Drive between 72nd Avenue and 73rd Avenue, both sides
- 76th Avenue between Shoshone Street and Raritan Street, Southside
- 76th Avenue between Raritan Street and Tejon Street, Northside
- 76th between Vallejo Street and Umatilla Street, Northside
- 76th Avenue between Umatilla Street and Tejon Street, Southside
- 76th Avenue between Zuni Street and Zane Street, Southside

At the peak, there were nine illegally parked vehicles observed in the Perl Mack neighborhood. These were located on the following block faces:

- Cortez Street between Bronco Road and Greenwood Boulevard, both sides (3 vehicles)
- Cragmore Street between Bronco Road and Greenwood Boulevard, Southside (1 vehicle)
- Cuchara Street between Bronco Road and Greenwood Boulevard, both sides (2 vehicles)
- Samuel Drive between 72nd Avenue and 73rd Avenue, Westside (2 vehicles)
- 71st Place between Mariposa Street and Lipan Street, Southside (1 vehicle)

The following table summarizes parking occupancy by block face at 9 PM.

Table 88: Perl Mack Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					Total Weekday Demand (System-wide Peak)	Weekday % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
Alan Dr	Jordan Dr	W 69th Ave	W	20	9	0	0	0	9	45%	
	Jordan Dr	W 72nd Ave	W	45	14	0	0	0	14	31%	
	W 68th Ave	W 69th Ave	E	27	11	0	0	0	11	41%	
	W 69th Ave	Jordan Dr	E	19	5	0	0	0	5	26%	
	W 69th Ave	W 68th Ave	W	19	13	0	0	0	13	68%	
	W 72nd Ave	Jordan Dr	E	38	16	0	0	0	16	42%	
Bronco Rd	Cortez St	Cragmore St	W	7	6	0	0	0	6	86%	
	Cortez St	Mid Block	E	7	2	0	0	0	2	29%	
	Cragmore St	Cortez St	E	6	2	0	0	0	2	33%	
	Cragmore St	Cuchara St	W	10	4	0	0	0	4	40%	
	Cuchara St	Cragmore St	E	5	1	0	0	0	1	20%	
	Cuchara St	Del Norte St	W	8	5	0	0	0	5	63%	
	Del Norte St	Cuchara St	E	6	1	0	0	0	1	17%	
	Mid Block	Cortez St	W	8	1	0	0	0	1	13%	
Cortez St	Bronco Rd	Dead End	S	4	3	0	0	0	3	75%	
	Bronco Rd	Greenwood Blvd	N	38	19	0	0	2	19	50%	
	Dead End	Bronco Rd	N	4	2	0	0	0	2	50%	
	Greenwood Blvd	Bronco Rd	S	39	16	0	0	1	16	41%	
Cragmore St	Bronco Rd	Greenwood Blvd	N	31	14	0	0	1	14	45%	
	Greenwood Blvd	Bronco Rd	S	31	12	0	0	0	12	39%	
Cuchara St	Bronco Rd	Greenwood Blvd	N	32	14	0	0	1	14	44%	
	Greenwood Blvd	Bronco Rd	S	32	20	0	0	1	20	63%	
Dakin St	Cortez St	Del Norte St	W	38	28	0	0	0	28	74%	
	Del Norte St	Cortez St	E	39	33	0	0	0	33	85%	
	Del Norte St	Hilltop Cir	W	5	9	0	0	0	9	31%	
	Hilltop Cir	Del Norte St	E	14	13	0	0	0	13	62%	
Del Norte St	Drake St	Greenwood Blvd	N	29	0	0	0	0	0	0%	
	Greenwood Blvd	Drake St	S	21	0	0	0	0	0	0%	
El Paso Blvd	El Paso Cir	Navajo St	S	16	0	0	0	0	0	0%	
	Navajo St	El Paso Cir	N	9	0	0	0	0	0	0%	
	Navajo St	Osage St	S	11	12	0	0	0	12	63%	
	Osage St	Navajo St	N	7	3	0	0	0	3	27%	
Hilltop Cir	Bronco Rd	Dakin St	S	19	3	0	0	0	3	30%	
	Dakin St	Drake St	N	11	0	0	0	0	0	0%	
	Dakin St	Erie St	W	10	0	0	0	0	0	0%	
	El Paso Blvd	Elbert St	E	8	1	0	0	0	1	13%	
	Elbert St	El Paso Blvd	W	8	0	0	0	0	0	0%	
	Elbert St	Erie St	E	8	0	0	0	0	0	0%	
	Erie St	Dakin St	E	6	0	0	0	0	0	0%	
	Erie St	Elbert St	W	10	1	0	0	0	1	20%	
Jordan Dr	Alan Dr	Raritan St	S	9	0	0	0	0	0	0%	
	Raritan St	Alan Dr	N	5	0	0	0	0	0	0%	
	Raritan St	Samuel Dr	S	10	8	0	0	0	8	67%	
	Samuel Dr	Raritan St	N	10	4	0	0	0	4	40%	
Lipan St	W 70th Ave	W 70th Pl	E	12	6	0	0	0	6	29%	
	W 70th Pl	W 70th Ave	W	10	9	0	0	0	9	43%	
	W 70th Pl	W 71st Pl	E	21	6	0	0	0	6	26%	
	W 71st Pl	W 70th Pl	W	21	3	0	0	0	3	13%	



Quivas St	Raritan St	W 72nd Ave	W	23	10	0	0	0	10	67%
	W 72nd Ave	Raritan St	W	24	5	0	0	0	5	36%
Raritan St	Jordan Dr	Quivas St	W	15	5	0	0	0	5	22%
	Quivas St	Jordan Dr	E	14	6	0	0	0	6	40%
	Quivas St	W 72nd Ave	W	23	1	0	0	0	1	7%
	W 72nd Ave	Quivas St	E	15	14	0	0	0	14	40%
Samuel Dr	Jordan Dr	N Pecos St	E	15	0	0	0	0	0	0%
	Jordan Dr	W 72nd Ave	W	35	0	0	0	0	0	0%
	N Pecos St	Jordan Dr	W	12	20	0	0	0	20	51%
	Quivas St	W 73rd Ave	E	23	14	0	0	0	14	100%
	W 72nd Ave	Jordan Dr	E	39	0	0	0	0	0	0%
	W 72nd Ave	W 73rd Ave	W	14	11	0	0	2	11	79%
	W 73rd Ave	Quivas St	W	29	2	0	0	0	2	29%
W 69th Ave	W 73rd Ave	W 72nd Ave	E	14	0	0	0	0	0	0%
	Alan Dr	N Pecos St	S	7	2	0	0	0	2	14%
W 70th Ave	N Pecos St	Alan Dr	N	14	0	0	0	0	0	0%
	Mariposa St	Lipan St	S	4	0	0	0	0	0	0%
	N Pecos St	Osage St	S	3	0	0	0	0	0	0%
	Navajo St	Mariposa St	S	4	0	0	0	0	0	0%
W 71st Pl	Osage St	Navajo St	S	0	3	0	0	0	3	43%
	Lipan St	Mariposa St	N	7	3	0	0	0	3	33%
	Mariposa St	Lipan St	S	9	9	0	0	1	9	43%
	Mariposa St	N Pecos St	N	21	19	0	0	0	19	63%
	N Pecos St	Mariposa St	S	30	2	0	0	0	2	50%
W 76th Ave	N Pecos St	Quivas St	N	4	0	0	0	0	0	0%
	Quivas St	N Pecos St	S	4	0	0	0	0	0	0%
	Quivas St	Turner Dr	N	3	1	0	0	0	1	25%
	Raritan St	Shoshone St	N	4	2	0	0	0	2	40%
	Raritan St	Turner Dr	S	5	1	0	0	0	1	33%
	Shoshone St	Raritan St	S	3	4	0	0	0	4	100%
	Shoshone St	Tejon St	N	4	9	0	0	0	9	90%
	Tejon St	Shoshone St	S	10	5	0	0	0	5	71%
	Tejon St	Umatilla St	N	7	0	0	0	0	0	0%
	Turner Dr	Quivas St	S	3	2	0	0	0	2	33%
	Turner Dr	Raritan St	N	6	12	0	0	0	12	92%
	Umatilla St	Tejon St	S	13	7	0	0	0	7	100%
	Umatilla St	Vallejo St	N	7	6	0	0	0	6	75%
	Vallejo St	Umatilla St	S	8	5	0	0	0	5	63%
	Vallejo St	Wynadot St	N	8	8	0	0	0	8	67%
	Wynadot St	Vallejo St	S	12	2	0	0	0	2	29%
	Wynadot St	Zane St	N	7	3	0	0	0	3	30%
	Zane St	Wynadot St	S	10	1	0	0	0	1	25%
	Zane St	Zuni St	N	4	7	0	0	0	7	88%
	Zuni St	Zane St	S	8	0	0	0	0	0	0%
Zuni St	Dead End	W 76th Ave	E	5	0	0	0	0	0	0%
	W 76th Ave	Dead End	W	5	0	0	0	0	0	0%
Total (Weekday System-wide Peak)				1,337	530	0	0	9	530	40%

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (9 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

31 block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. Many of these areas peaked at 11 PM. For the weekday, the overall composite peak was approximately 4% higher than the system peak.



Table 89: Perl Mack Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Peak Weekday % Occupancy (Across All Times)	Time that Peak Occurred
Alan Dr	Jordan Dr	W 69th Ave	W	20	9	0	0	0	9	45%	9:00:00 PM
	Jordan Dr	W 72nd Ave	W	45	15	0	0	0	15	33%	11:00:00 AM
	W 68th Ave	W 69th Ave	E	27	11	0	0	0	11	41%	9:00:00 PM
	W 69th Ave	Jordan Dr	E	19	7	0	0	0	7	37%	11:00:00 PM
	W 69th Ave	W 68th Ave	W	19	14	0	0	0	14	74%	11:00:00 PM
	W 72nd Ave	Jordan Dr	E	38	17	0	0	0	17	45%	11:00:00 PM
Bronco Rd	Cortez St	Cragmore St	W	7	6	0	0	0	6	86%	9:00:00 PM
	Cortez St	Mid Block	E	7	2	0	0	0	2	29%	9:00:00 PM
	Cragmore St	Cortez St	E	6	2	0	0	0	2	33%	9:00:00 PM
	Cragmore St	Cuchara St	W	10	5	0	0	0	5	50%	11:00:00 PM
	Cuchara St	Cragmore St	E	5	1	0	0	0	1	20%	9:00:00 PM
	Cuchara St	Del Norte St	W	8	5	0	0	0	5	63%	9:00:00 PM
	Del Norte St	Cuchara St	E	6	1	0	0	0	1	17%	9:00:00 PM
Cortez St	Mid Block	Cortez St	W	8	1	0	0	0	1	13%	9:00:00 PM
	Bronco Rd	Dead End	S	4	3	0	0	0	3	75%	9:00:00 PM
	Bronco Rd	Greenwood Blvd	N	38	20	0	0	1	20	53%	11:00:00 AM
	Dead End	Bronco Rd	N	4	2	0	0	0	2	50%	1:00:00 PM
Cragmore St	Greenwood Blvd	Bronco Rd	S	39	16	0	0	1	16	41%	9:00:00 PM
	Bronco Rd	Greenwood Blvd	N	31	14	0	0	1	14	45%	9:00:00 PM
Cuchara St	Greenwood Blvd	Bronco Rd	S	31	16	0	0	1	16	52%	11:00:00 AM
	Bronco Rd	Greenwood Blvd	N	32	16	0	0	1	16	50%	11:00:00 PM
Dakin St	Greenwood Blvd	Bronco Rd	S	32	20	0	0	1	20	63%	9:00:00 PM
	Cortez St	Del Norte St	W	38	28	0	0	0	28	74%	9:00:00 PM
	Del Norte St	Cortez St	E	39	33	0	0	0	33	85%	9:00:00 PM
	Del Norte St	Hilltop Cir	W	5	9	0	0	0	9	31%	9:00:00 PM
Del Norte St	Hilltop Cir	Del Norte St	E	14	14	0	0	0	14	67%	11:00:00 PM
	Drake St	Greenwood Blvd	N	29	0	0	0	0	0	0%	
	Greenwood Blvd	Drake St	S	21	0	0	0	0	0	0%	
El Paso Blvd	El Paso Cir	Navajo St	S	16	0	0	0	0	0	0%	
	Navajo St	El Paso Cir	N	9	3	0	0	0	3	43%	11:00:00 AM
	Navajo St	Osage St	S	11	12	0	0	0	12	63%	9:00:00 PM
	Osage St	Navajo St	N	7	5	0	0	0	5	45%	9:00:00 AM
Hilltop Cir	Bronco Rd	Dakin St	S	19	3	0	0	0	3	30%	9:00:00 PM
	Dakin St	Drake St	N	11	0	0	0	0	0	0%	
	Dakin St	Erie St	W	10	1	0	0	0	1	13%	1:00:00 PM
	El Paso Blvd	Elbert St	E	8	1	0	0	0	1	13%	9:00:00 PM
	Elbert St	El Paso Blvd	W	8	0	0	0	0	0	0%	
	Elbert St	Erie St	E	8	3	0	0	0	3	30%	9:00:00 AM
	Erie St	Dakin St	E	6	5	0	0	0	5	56%	11:00:00 AM
	Erie St	Elbert St	W	10	1	0	0	0	1	20%	9:00:00 PM
Jordan Dr	Alan Dr	Raritan St	S	9	5	0	0	0	5	50%	11:00:00 AM
	Raritan St	Alan Dr	N	5	1	0	0	0	1	10%	11:00:00 AM
	Raritan St	Samuel Dr	S	10	8	0	0	0	8	67%	9:00:00 PM
Lipan St	Samuel Dr	Raritan St	N	10	5	0	0	0	5	50%	11:00:00 AM
	W 70th Ave	W 70th Pl	E	12	6	0	0	0	6	29%	9:00:00 PM
	W 70th Pl	W 70th Ave	W	10	9	0	0	0	9	43%	9:00:00 PM
	W 70th Pl	W 71st Pl	E	21	7	0	0	0	7	30%	11:00:00 PM
	W 71st Pl	W 70th Pl	W	21	4	0	0	0	4	17%	11:00:00 PM

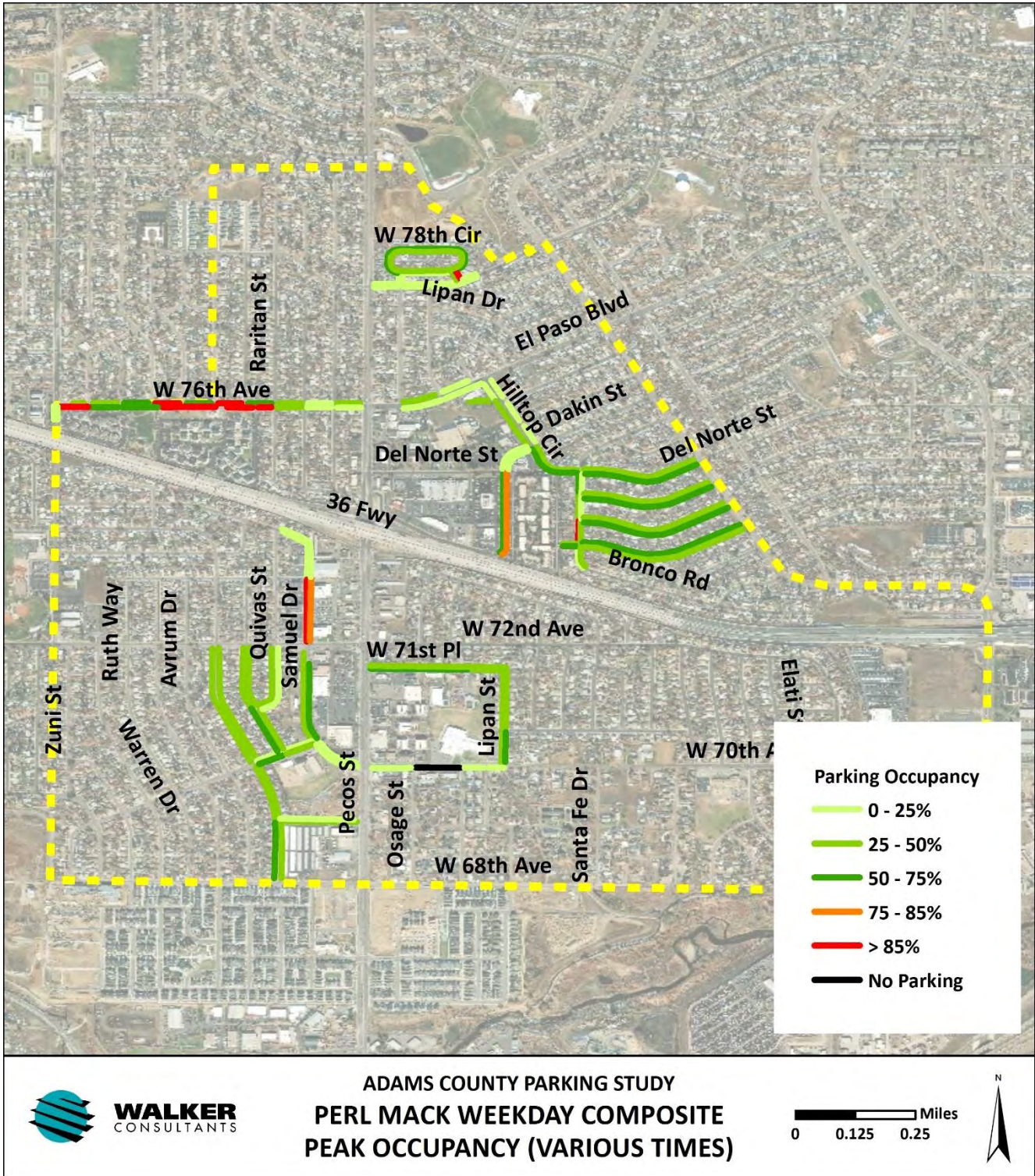


Quivas St	Raritan St	W 72nd Ave	W	23	10	0	0	0	10	67%	9:00:00 PM
	W 72nd Ave	Raritan St	W	24	6	0	0	0	6	43%	1:00:00 PM
Raritan St	Jordan Dr	Quivas St	W	15	6	0	0	0	6	26%	9:00:00 AM
	Quivas St	Jordan Dr	E	14	6	0	0	0	6	40%	9:00:00 PM
	Quivas St	W 72nd Ave	W	23	1	0	0	0	1	7%	9:00:00 PM
Samuel Dr	W 72nd Ave	Quivas St	E	15	14	0	0	0	14	40%	9:00:00 PM
	Jordan Dr	N Pecos St	E	15	0	0	0	0	0	0%	
	Jordan Dr	W 72nd Ave	W	35	3	0	0	0	3	13%	11:00:00 PM
	N Pecos St	Jordan Dr	W	12	20	0	0	0	20	51%	9:00:00 PM
	Quivas St	W 73rd Ave	E	23	14	0	0	0	14	100%	9:00:00 PM
	W 72nd Ave	Jordan Dr	E	39	1	0	0	0	1	3%	9:00:00 AM
	W 72nd Ave	W 73rd Ave	W	14	11	0	0	2	11	79%	9:00:00 PM
	W 73rd Ave	Quivas St	W	29	2	0	0	0	2	29%	9:00:00 PM
W 69th Ave	W 73rd Ave	W 72nd Ave	E	14	4	0	0	0	4	29%	9:00:00 AM
	Alan Dr	N Pecos St	S	7	1	0	0	0	1	7%	9:00:00 PM
W 70th Ave	N Pecos St	Alan Dr	N	14	0	0	0	0	0	0%	
	Mariposa St	Lipan St	S	4	0	0	0	0	0	0%	
	N Pecos St	Osage St	S	3	0	0	0	0	0	0%	
	Navajo St	Mariposa St	S	4	0	0	0	0	0	0%	
W 71st Pl	Osage St	Navajo St	S	0	3	0	0	0	3	43%	9:00:00 PM
	Lipan St	Mariposa St	N	7	3	0	0	0	3	33%	9:00:00 PM
	Mariposa St	Lipan St	S	9	10	0	0	1	10	48%	11:00:00 PM
W 76th Ave	Mariposa St	N Pecos St	N	21	19	0	0	0	19	63%	9:00:00 PM
	N Pecos St	Mariposa St	S	30	2	0	0	0	2	50%	9:00:00 PM
	N Pecos St	Quivas St	N	4	0	0	0	0	0	0%	
	Quivas St	N Pecos St	S	4	0	0	0	0	0	0%	
	Quivas St	Turner Dr	N	3	3	0	0	0	3	75%	11:00:00 PM
	Raritan St	Shoshone St	N	4	2	0	0	0	2	40%	9:00:00 PM
	Raritan St	Turner Dr	S	5	3	0	0	0	3	100%	9:00:00 AM
	Shoshone St	Raritan St	S	3	4	0	0	0	4	100%	9:00:00 AM
	Shoshone St	Tejon St	N	4	9	0	0	0	9	90%	9:00:00 PM
	Tejon St	Shoshone St	S	10	5	0	0	0	5	71%	9:00:00 PM
	Tejon St	Umatilla St	N	7	0	0	0	0	0	0%	
	Turner Dr	Quivas St	S	3	3	0	0	0	3	50%	11:00:00 PM
	Turner Dr	Raritan St	N	6	12	0	0	0	12	92%	9:00:00 PM
	Umatilla St	Tejon St	S	13	7	0	0	0	7	100%	9:00:00 PM
	Umatilla St	Vallejo St	N	7	7	0	0	0	7	88%	11:00:00 PM
	Vallejo St	Umatilla St	S	8	5	0	0	0	5	63%	9:00:00 PM
	Vallejo St	Wynadot St	N	8	8	0	0	0	8	67%	9:00:00 PM
Wynadot St	Vallejo St	S	12	2	0	0	0	2	29%	9:00:00 PM	
Wynadot St	Zane St	N	7	6	0	0	0	6	60%	1:00:00 PM	
Zane St	Wynadot St	S	10	2	0	0	0	2	50%	11:00:00 PM	
Zane St	Zuni St	N	4	7	0	0	0	7	88%	9:00:00 PM	
Zuni St	Zane St	S	8	0	0	0	0	0	0%		
Zuni St	Dead End	W 76th Ave	E	5	0	0	0	0	0	0%	
	W 76th Ave	Dead End	W	5	0	0	0	0	0	0%	
Total (Weekday Composite)				1,337	587	0	0	9	587	44%	9:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekday parking demand per block face across all collection periods.

Figure 64: Perl Mack Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the Perl Mack neighborhood was performed on Saturday, August 31 and Sunday, September 1, 2019. Corridors in and around churches were counted on Sunday whereas all other corridors were counted on Saturday in order to capture maximum potential weekend parking demand for this neighborhood. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 505 vehicles (38% occupied) at 11 AM to the system-wide peak of 544 vehicles (41% occupied) at 7 PM. Off-street demand at the Hidden Lake Lot ranged from 0 vehicles at 9 PM and 11 PM to 5 vehicles at 9 AM and 11 AM.

The tables below summarize parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

Table 90: Perl Mack Observed Weekend On-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	1,337	532	40%	0	0%	0	0%	10	1%	532	40%
11:00 AM	1,337	505	38%	0	0%	0	0%	9	1%	505	38%
1:00 PM	1,337	507	38%	0	0%	1	0%	9	1%	507	38%
5:00 PM	1,337	540	40%	0	0%	0	0%	10	1%	540	40%
7:00 PM	1,337	544	41%	0	0%	0	0%	15	1%	544	41%
Average	1,337	526	39%	0	0%	0	0%	11	1%	526	39%

Source: Walker Consultants

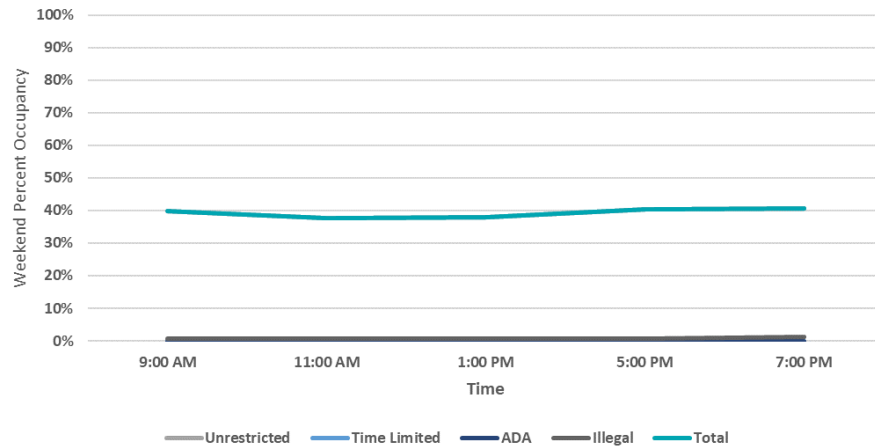
Table 91: Perl Mack Observed Weekend Off-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	31	5	16%	0	0%	0	0%	0	0%	5	16%
11:00 AM	31	5	16%	0	0%	0	0%	0	0%	5	16%
1:00 PM	31	3	10%	0	0%	0	0%	0	0%	3	10%
9:00 PM	31	0	0%	0	0%	0	0%	0	0%	0	0%
11:00 PM	31	0	0%	0	0%	0	0%	0	0%	0	0%
Average	31	3	8%	0	0%	0	0%	0	0%	3	8%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the Perl Mack neighborhood.

Figure 65: Perl Mack Total Weekend Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekend peak at 7 PM, demand by block face ranged from 0% occupied (23 segments) to 100% occupied. Full utilization occurred along Bronco Road between Del Norte Street and Cuchara Street. Seven block faces were observed at or above 85% occupancy during the system peak. An additional 26 block faces were observed between 50% and 85% occupied.

The segments observed at or above 85% occupancy are:

- Bronco Road between Cortez Street and Cragmore Street, Westside
- Bronco Road between Del Norte Street and Cuchara Street, Eastside
- Del Norte Street between Bronco Road and Greenwood Boulevard, Southside
- Dakin Street between Hilltop Road and Del Norte Street, Eastside
- Samuel Drive between 72nd Avenue and 73rd Avenue, Westside
- 76th Avenue between Tejon Street and Umatilla Street, Northside
- 76th Avenue between Umatilla Street and Vallejo Street, Northside
- 76th Avenue between Zane Street and Zuni Street, Southside

During the weekend peak, 15 vehicles were observed to be illegally parked in the Perl Mack neighborhood. These were located on the following segments:

- Alan Drive between Jordan Drive and 72nd Avenue, Westside (1 vehicle)
- Alan Drive between 68th Avenue and 69th Avenue, both sides (3 vehicles)
- Cuchara Street between Greenwood Boulevard and Bronco Road, Southside (1 vehicle)
- Jordan Drive between Samuel Drive and Raritan Street, Southside (1 vehicle)
- Quivas Street between 72nd Avenue and Raritan Street, Westside (1 vehicle)
- Raritan Street between Quivas Street and 72nd Avenue, Westside (1 vehicle)

- Samuel Drive between Jordan Drive and 72nd Avenue, Westside (1 vehicle)
- 76th Avenue between Shoshone Street and Raritan Street, Southside (1 vehicle)
- 76th Avenue between Umatilla Street and Vallejo Street, Northside (1 vehicle)
- 76th Avenue between Vallejo Street and Wyandot Street, Northside (1 vehicle)
- 76th Avenue between Wyandot Street and Zane Street, Northside (1 vehicle)
- 76th Avenue between Zane Street and Zuni Street, Northside (2 vehicles)

The following table summarizes parking occupancy by block face at 7 PM.



Table 92: Perl Mack Weekend System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					Weekend % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal	Total Weekend Demand (System-wide Peak)	
Alan Dr	Jordan Dr	W 69th Ave	W	20	5	0	0	0	5	25%
	Jordan Dr	W 72nd Ave	W	45	18	0	0	1	18	40%
	W 68th Ave	W 69th Ave	E	27	15	0	0	2	15	56%
	W 69th Ave	Jordan Dr	E	19	6	0	0	0	6	32%
	W 69th Ave	W 68th Ave	W	19	14	0	0	1	14	74%
	W 72nd Ave	Jordan Dr	E	38	13	0	0	0	13	34%
Bronco Rd	Cortez St	Cragmore St	W	7	6	0	0	0	6	86%
	Cortez St	Mid Block	E	7	2	0	0	0	2	29%
	Cragmore St	Cortez St	E	6	4	0	0	0	4	67%
	Cragmore St	Cuchara St	W	10	5	0	0	0	5	50%
	Cuchara St	Cragmore St	E	5	1	0	0	0	1	20%
	Cuchara St	Del Norte St	W	8	4	0	0	0	4	50%
	Del Norte St	Cuchara St	E	6	6	0	0	0	6	100%
	Mid Block	Cortez St	W	8	1	0	0	0	1	13%
Cortez St	Bronco Rd	Dead End	S	4	2	0	0	0	2	50%
	Bronco Rd	Greenwood Blvd	N	38	25	0	0	0	25	66%
	Dead End	Bronco Rd	N	4	1	0	0	0	1	25%
	Greenwood Blvd	Bronco Rd	S	39	19	0	0	0	19	49%
Cragmore St	Bronco Rd	Greenwood Blvd	N	31	14	0	0	0	14	45%
	Greenwood Blvd	Bronco Rd	S	31	14	0	0	0	14	45%
Cuchara St	Bronco Rd	Greenwood Blvd	N	32	17	0	0	0	17	53%
	Greenwood Blvd	Bronco Rd	S	32	16	0	0	1	16	50%
Dakin St	Cortez St	Del Norte St	W	38	25	0	0	0	25	66%
	Del Norte St	Cortez St	E	39	32	0	0	0	32	82%
	Del Norte St	Hilltop Cir	W	5	10	0	0	0	10	34%
	Hilltop Cir	Del Norte St	E	14	18	0	0	0	18	86%
Del Norte St	Drake St	Greenwood Blvd	N	29	0	0	0	0	0	0%
	Greenwood Blvd	Drake St	S	21	0	0	0	0	0	0%
El Paso Blvd	El Paso Cir	Navajo St	S	16	0	0	0	0	0	0%
	Navajo St	El Paso Cir	N	9	0	0	0	0	0	0%
	Navajo St	Osage St	S	11	10	0	0	0	10	53%
	Osage St	Navajo St	N	7	0	0	0	0	0	0%
Hilltop Cir	Bronco Rd	Dakin St	S	19	0	0	0	0	0	0%
	Dakin St	Drake St	N	11	0	0	0	0	0	0%
	Dakin St	Erie St	W	10	0	0	0	0	0	0%
	El Paso Blvd	Elbert St	E	8	1	0	0	0	1	13%
	Elbert St	El Paso Blvd	W	8	3	0	0	0	3	50%
	Elbert St	Erie St	E	8	2	0	0	0	2	20%
	Erie St	Dakin St	E	6	0	0	0	0	0	0%
	Erie St	Elbert St	W	10	1	0	0	0	1	20%
Jordan Dr	Alan Dr	Raritan St	S	9	0	0	0	0	0	0%
	Raritan St	Alan Dr	N	5	0	0	0	0	0	0%
	Raritan St	Samuel Dr	S	10	7	0	0	1	7	58%
	Samuel Dr	Raritan St	N	10	3	0	0	0	3	30%
Lipan St	W 70th Ave	W 70th Pl	E	12	5	0	0	0	5	24%
	W 70th Pl	W 70th Ave	W	10	10	0	0	0	10	48%
	W 70th Pl	W 71st Pl	E	21	8	0	0	0	8	35%
	W 71st Pl	W 70th Pl	W	21	5	0	0	0	5	21%



Quivas St	Raritan St	W 72nd Ave	W	23	8	0	0	0	8	53%
	W 72nd Ave	Raritan St	W	24	9	0	0	1	9	64%
Raritan St	Jordan Dr	Quivas St	W	15	5	0	0	0	5	22%
	Quivas St	Jordan Dr	E	14	6	0	0	0	6	40%
	Quivas St	W 72nd Ave	W	23	2	0	0	1	2	13%
	W 72nd Ave	Quivas St	E	15	10	0	0	0	10	29%
Samuel Dr	Jordan Dr	N Pecos St	E	15	1	0	0	0	1	8%
	Jordan Dr	W 72nd Ave	W	35	19	0	0	1	19	83%
	N Pecos St	Jordan Dr	W	12	11	0	0	0	11	28%
	Quivas St	W 73rd Ave	E	23	13	0	0	0	13	93%
	W 72nd Ave	Jordan Dr	E	39	0	0	0	0	0	0%
	W 72nd Ave	W 73rd Ave	W	14	4	0	0	0	4	29%
	W 73rd Ave	Quivas St	W	29	2	0	0	0	2	29%
W 73rd Ave	W 72nd Ave	E	14	0	0	0	0	0	0%	
W 69th Ave	Alan Dr	N Pecos St	S	7	1	0	0	0	1	7%
	N Pecos St	Alan Dr	N	14	0	0	0	0	0	0%
W 70th Ave	Mariposa St	Lipan St	S	4	0	0	0	0	0	0%
	N Pecos St	Osage St	S	3	1	0	0	0	1	25%
	Navajo St	Mariposa St	S	4	0	0	0	0	0	0%
	Osage St	Navajo St	S	0	2	0	0	0	2	29%
W 71st Pl	Lipan St	Mariposa St	N	7	5	0	0	0	5	56%
	Mariposa St	Lipan St	S	9	9	0	0	0	9	43%
	Mariposa St	N Pecos St	N	21	16	0	0	0	16	53%
	N Pecos St	Mariposa St	S	30	1	0	0	0	1	25%
W 76th Ave	N Pecos St	Quivas St	N	4	0	0	0	0	0	0%
	Quivas St	N Pecos St	S	4	0	0	0	0	0	0%
	Quivas St	Turner Dr	N	3	1	0	0	0	1	25%
	Raritan St	Shoshone St	N	4	2	0	0	0	2	40%
	Raritan St	Turner Dr	S	5	0	0	0	0	0	0%
	Shoshone St	Raritan St	S	3	3	0	0	1	3	75%
	Shoshone St	Tejon St	N	4	8	0	0	0	8	80%
	Tejon St	Shoshone St	S	10	6	0	0	0	6	86%
	Tejon St	Umatilla St	N	7	0	0	0	0	0	0%
	Turner Dr	Quivas St	S	3	2	0	0	0	2	33%
	Turner Dr	Raritan St	N	6	10	0	0	0	10	77%
	Umatilla St	Tejon St	S	13	6	0	0	0	6	86%
	Umatilla St	Vallejo St	N	7	5	0	0	1	5	63%
	Vallejo St	Umatilla St	S	8	6	0	0	0	6	75%
	Vallejo St	Wynadot St	N	8	7	0	0	1	7	58%
	Wynadot St	Vallejo St	S	12	2	0	0	0	2	29%
	Wynadot St	Zane St	N	7	3	0	0	1	3	30%
	Zane St	Wynadot St	S	10	3	0	0	0	3	75%
	Zane St	Zuni St	N	4	7	0	0	2	7	88%
	Zuni St	Zane St	S	8	0	0	0	0	0	0%
Zuni St	Dead End	W 76th Ave	E	5	0	0	0	0	0	0%
	W 76th Ave	Dead End	W	5	0	0	0	0	0	0%
Total (Weekend System-wide Peak)				1,337	544	0	0	15	544	41%

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (7 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

41 block faces were observed with individual peak occupancies greater than they exhibited during the system-wide peak. For the weekend, the overall composite peak was approximately 7% higher than the system peak.

Table 93: Perl Mack Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)	Time that Peak Occurred
Alan Dr	Jordan Dr	W 69th Ave	W	20	9	0	0	0	9	45%	9:00:00 AM
	Jordan Dr	W 72nd Ave	W	45	19	0	0	0	19	42%	1:00:00 PM
	W 68th Ave	W 69th Ave	E	27	15	0	0	2	15	56%	7:00:00 PM
	W 69th Ave	Jordan Dr	E	19	8	0	0	0	8	42%	9:00:00 AM
	W 69th Ave	W 68th Ave	W	19	15	0	0	1	15	79%	5:00:00 PM
	W 72nd Ave	Jordan Dr	E	38	13	0	0	1	13	34%	7:00:00 PM
Bronco Rd	Cragmore St	Cragmore St	W	7	6	0	0	0	6	86%	7:00:00 PM
	Cortez St	Mid Block	E	7	2	0	0	0	2	29%	7:00:00 PM
	Cragmore St	Cortez St	E	6	5	0	0	0	5	83%	5:00:00 PM
	Cragmore St	Cuchara St	W	10	6	0	0	0	6	60%	9:00:00 AM
	Cuchara St	Cragmore St	E	5	1	0	0	0	1	20%	7:00:00 PM
	Cuchara St	Del Norte St	W	8	4	0	0	0	4	50%	7:00:00 PM
	Del Norte St	Cuchara St	E	6	6	0	0	0	6	100%	7:00:00 PM
Cortez St	Mid Block	Cortez St	W	8	3	0	0	0	3	38%	1:00:00 PM
	Bronco Rd	Dead End	S	4	3	0	0	0	3	75%	9:00:00 AM
	Bronco Rd	Greenwood Blvd	N	38	25	0	0	0	25	66%	7:00:00 PM
	Dead End	Bronco Rd	N	4	1	0	0	0	1	25%	7:00:00 PM
Cragmore St	Greenwood Blvd	Bronco Rd	S	39	22	0	0	0	22	56%	9:00:00 AM
	Bronco Rd	Greenwood Blvd	N	31	14	0	0	0	14	45%	7:00:00 PM
Cuchara St	Greenwood Blvd	Bronco Rd	S	31	15	0	0	0	15	48%	9:00:00 AM
	Bronco Rd	Greenwood Blvd	N	32	21	0	0	0	21	66%	1:00:00 PM
Dakin St	Greenwood Blvd	Bronco Rd	S	32	18	0	0	1	18	56%	9:00:00 AM
	Cortez St	Del Norte St	W	38	25	0	0	0	25	66%	7:00:00 PM
	Del Norte St	Cortez St	E	39	32	0	0	0	32	82%	7:00:00 PM
	Del Norte St	Hilltop Cir	W	5	10	0	0	0	10	34%	7:00:00 PM
Hilltop Cir	Hilltop Cir	Del Norte St	E	14	18	0	0	0	18	86%	7:00:00 PM
	Drake St	Greenwood Blvd	N	29	1	0	0	0	1	6%	9:00:00 AM
El Paso Blvd	Greenwood Blvd	Drake St	S	21	0	0	0	0	0	0%	
	El Paso Cir	Navajo St	S	16	0	0	0	0	0	0%	
	Navajo St	El Paso Cir	N	9	0	0	0	0	0	0%	
	Navajo St	Osage St	S	11	12	0	0	0	12	63%	5:00:00 PM
	Osage St	Navajo St	N	7	6	0	0	0	6	55%	9:00:00 AM
Hilltop Cir	Bronco Rd	Dakin St	S	19	0	0	0	0	0	0%	
	Dakin St	Drake St	N	11	0	0	0	0	0	0%	
	Dakin St	Erie St	W	10	2	0	0	0	2	25%	1:00:00 PM
	El Paso Blvd	Elbert St	E	8	1	0	0	0	1	13%	7:00:00 PM
	Elbert St	El Paso Blvd	W	8	3	0	0	0	3	50%	7:00:00 PM
	Elbert St	Erie St	E	8	2	0	0	0	2	20%	7:00:00 PM
	Erie St	Dakin St	E	6	1	0	0	0	1	11%	1:00:00 PM
Jordan Dr	Erie St	Elbert St	W	10	2	0	0	0	2	40%	9:00:00 AM
	Alan Dr	Raritan St	S	9	0	0	0	0	0	0%	
	Raritan St	Alan Dr	N	5	0	0	0	0	0	0%	
	Raritan St	Samuel Dr	S	10	7	0	0	0	7	58%	7:00:00 PM
Lipan St	Samuel Dr	Raritan St	N	10	3	0	0	0	3	30%	7:00:00 PM
	W 70th Ave	W 70th Pl	E	12	5	0	0	0	5	24%	7:00:00 PM
	W 70th Pl	W 70th Ave	W	10	10	0	0	0	10	48%	7:00:00 PM
	W 70th Pl	W 71st Pl	E	21	12	0	0	0	12	52%	9:00:00 AM
	W 71st Pl	W 70th Pl	W	21	9	0	0	0	9	38%	1:00:00 PM



Quivas St	Raritan St	W 72nd Ave	W	23	8	0	0	0	8	53%	7:00:00 PM
	W 72nd Ave	Raritan St	W	24	9	0	0	1	9	64%	7:00:00 PM
Raritan St	Jordan Dr	Quivas St	W	15	6	0	0	0	6	26%	5:00:00 PM
	Quivas St	Jordan Dr	E	14	6	0	0	0	6	40%	7:00:00 PM
	Quivas St	W 72nd Ave	W	23	2	0	0	0	2	13%	7:00:00 PM
Samuel Dr	W 72nd Ave	Quivas St	E	15	14	0	0	0	14	40%	1:00:00 PM
	Jordan Dr	N Pecos St	E	15	1	0	0	0	1	8%	7:00:00 PM
	Jordan Dr	W 72nd Ave	W	35	19	0	0	1	19	83%	7:00:00 PM
	N Pecos St	Jordan Dr	W	12	19	0	0	0	19	49%	9:00:00 AM
	Quivas St	W 73rd Ave	E	23	13	0	0	0	13	93%	7:00:00 PM
	W 72nd Ave	Jordan Dr	E	39	1	0	0	0	1	3%	9:00:00 AM
	W 72nd Ave	W 73rd Ave	W	14	11	0	0	0	11	79%	9:00:00 AM
	W 73rd Ave	Quivas St	W	29	2	0	0	1	2	29%	7:00:00 PM
W 69th Ave	W 73rd Ave	W 72nd Ave	E	14	3	0	0	0	3	21%	9:00:00 AM
	Alan Dr	N Pecos St	S	7	1	0	0	0	1	7%	7:00:00 PM
W 70th Ave	N Pecos St	Alan Dr	N	14	0	0	0	0	0	0%	
	Mariposa St	Lipan St	S	4	0	0	0	0	0	0%	
	N Pecos St	Osage St	S	3	1	0	0	0	1	25%	7:00:00 PM
	Navajo St	Mariposa St	S	4	0	0	0	0	0	0%	
W 71st Pl	Osage St	Navajo St	S	0	6	0	0	0	6	86%	5:00:00 PM
	Lipan St	Mariposa St	N	7	7	0	0	0	7	78%	1:00:00 PM
	Mariposa St	Lipan St	S	9	10	0	0	0	10	48%	9:00:00 AM
	Mariposa St	N Pecos St	N	21	20	0	0	0	20	67%	11:00:00 AM
W 76th Ave	N Pecos St	Mariposa St	S	30	1	0	0	0	1	25%	7:00:00 PM
	N Pecos St	Quivas St	N	4	0	0	0	0	0	0%	
	Quivas St	N Pecos St	S	4	0	0	0	0	0	0%	
	Quivas St	Turner Dr	N	3	2	0	0	0	2	50%	1:00:00 PM
	Raritan St	Shoshone St	N	4	3	0	0	0	3	60%	9:00:00 AM
	Raritan St	Turner Dr	S	5	2	0	0	0	2	67%	9:00:00 AM
	Shoshone St	Raritan St	S	3	4	0	0	1	4	100%	9:00:00 AM
	Shoshone St	Tejon St	N	4	10	0	0	0	10	100%	9:00:00 AM
	Tejon St	Shoshone St	S	10	6	0	0	0	6	86%	7:00:00 PM
	Tejon St	Umatilla St	N	7	1	0	0	0	1	33%	9:00:00 AM
	Turner Dr	Quivas St	S	3	4	0	0	0	4	67%	9:00:00 AM
	Turner Dr	Raritan St	N	6	11	0	0	0	11	85%	5:00:00 PM
	Umatilla St	Tejon St	S	13	6	0	0	0	6	86%	7:00:00 PM
	Umatilla St	Vallejo St	N	7	5	0	0	1	5	63%	7:00:00 PM
	Vallejo St	Umatilla St	S	8	6	0	0	0	6	75%	7:00:00 PM
	Vallejo St	Wynadot St	N	8	10	0	0	0	10	83%	11:00:00 AM
	Wynadot St	Vallejo St	S	12	3	0	0	0	3	43%	9:00:00 AM
	Wynadot St	Zane St	N	7	7	0	0	0	7	70%	9:00:00 AM
	Zane St	Wynadot St	S	10	3	0	0	0	3	75%	7:00:00 PM
	Zane St	Zuni St	N	4	7	0	0	2	7	88%	7:00:00 PM
Zuni St	Zuni St	Zane St	S	8	0	0	0	0	0	0%	
	Dead End	W 76th Ave	E	5	0	0	0	0	0	0%	
	W 76th Ave	Dead End	W	5	0	0	0	0	0	0%	
Total (Weekend Composite)				1,337	642	0	0	12	642	48%	11:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekend parking demand per block face across all collection periods.

Figure 66: Perl Mack Heat Map of Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

SHAW HEIGHTS

A total of 58 block faces were surveyed across eight streets.

INVENTORY

Walker staff observed a total of approximately 533 public on-street parking spaces along all corridors identified for study in the Shaw Heights neighborhood.

The table on the next page summarizes parking supply by street and block face.

Table 94: Shaw Heights Observed Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
Circle Dr	Cherry Ln	Concord Ln	E	8	0	0	8
	Cherry Ln	Norwich Way	W	12	0	0	12
	Concord Ln	Cherry Ln	W	7	0	0	7
	Concord Ln	W 88th Ave	E	5	0	0	5
	Norwich Way	Cherry Ln	E	8	0	0	8
	Norwich Way	Shaw Blvd	W	7	0	0	7
	Shaw Blvd	Mid Block	W	10	0	0	10
	Shaw Blvd	Norwich Way	E	7	0	0	7
	Shaw Blvd	Shaw Blvd	E	16	0	0	16
W 88th Ave	Concord Ln	W	16	0	0	16	
Highland Pl	Knox Ct	La Salle Pl	E	9	0	0	9
	La Salle Pl	Knox Ct	W	8	0	0	8
La Salle Pl	Highland Pl	Lowell Blvd	S	6	0	0	6
	Lowell Blvd	Highland Pl	N	9	0	0	9
Shaw Blvd	Cedar Ln	Cherry Ln	S	5	0	0	5
	Cedar Ln	Circle Dr	N	7	0	0	7
	Cherry Ln	Cedar Ln	N	7	0	0	7
	Cherry Ln	Concord Ln	S	6	0	0	6
	Circle Dr	Cedar Ln	S	6	0	0	6
	Concord Ln	Cherry Ln	N	4	0	0	4
	Concord Ln	Crescent Dr	S	5	0	0	5
	Crescent Dr	Concord Ln	N	5	0	0	5
	Crescent Dr	Lowell Blvd	S	16	0	0	16
Lowell Blvd	Crescent Dr	N	9	0	0	9	
W 88th Ave	Circle Dr	Norwich St	N	11	0	0	11
	Circle Dr	W 88th Way	S	20	0	0	20
	Lowell Blvd	W 88th Way	N	7	0	0	7
	Norwich St	Circle Dr	S	7	0	0	7
	Seton St	Utica St	N	21	0	0	21
	Utica St	Seton St	S	7	0	0	7
	Utica St	Vrain St	N	10	0	0	10
	Vrain St	Utica St	S	7	0	0	7
	Vrain St	Winona Ct	N	3	0	0	3
	W 88th Way	Circle Dr	N	8	0	0	8
	W 88th Way	Lowell Blvd	S	7	0	0	7
	Wagner Dr	Winona Ct	S	6	0	0	6
	Winona Ct	Vrain St	S	5	0	0	5
	Winona Ct	Wagner St	N	4	0	0	4
W 88th Way	Lowell Blvd	Mid Block	N	6	0	0	6
	Mid Block	Lowell Blvd	S	7	0	0	7
	Mid Block	W 88th Ave	E	8	0	0	8
	W 88th Ave	Mid Block	W	10	0	0	10
Wagner Dr	Dixon Dr	Shaw Blvd	E	5	0	0	5
	Dixon Dr	Wiley Cir	W	0	0	0	0
	Shaw Blvd	Dixon Dr	W	10	0	0	10
	Shaw Blvd	W 87th Ave	E	10	0	0	10
	W 87th Ave	Shaw Blvd	W	10	0	0	10
	W 87th Ave	W 87th Pl	E	26	0	0	26
	W 87th Pl	W 87th Ave	W	34	0	0	34
	W 87th Pl	W 88th Ave	E	12	0	0	12
	W 88th Ave	W 87th Pl	W	12	0	0	12
	Wiley Cir	Dixon Dr	E	7	0	0	7
Wiley Cir	Wiley Cir	E	6	0	0	6	
Wiley Cir	Wiley Cir	W	0	0	0	0	
Winona Ct	W 88th Ave	Wagner Ln	E	6	0	0	6
	W 89th Way	Wagner Ln	W	18	0	0	18
	Wagner Ln	W 88th Ave	W	18	0	0	18
	Wagner Ln	W 89th Way	E	7	0	0	7
Total On Street Parking				533	0	0	533

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the Shaw Heights neighborhood was performed on Tuesday, September 3, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. On-street parking demand for the neighborhood ranged from 84 vehicles (16% occupied) at 1 PM to a system-wide peak of 105 vehicles (20% occupied) at 9 PM and 11 PM.

The table below summarizes parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

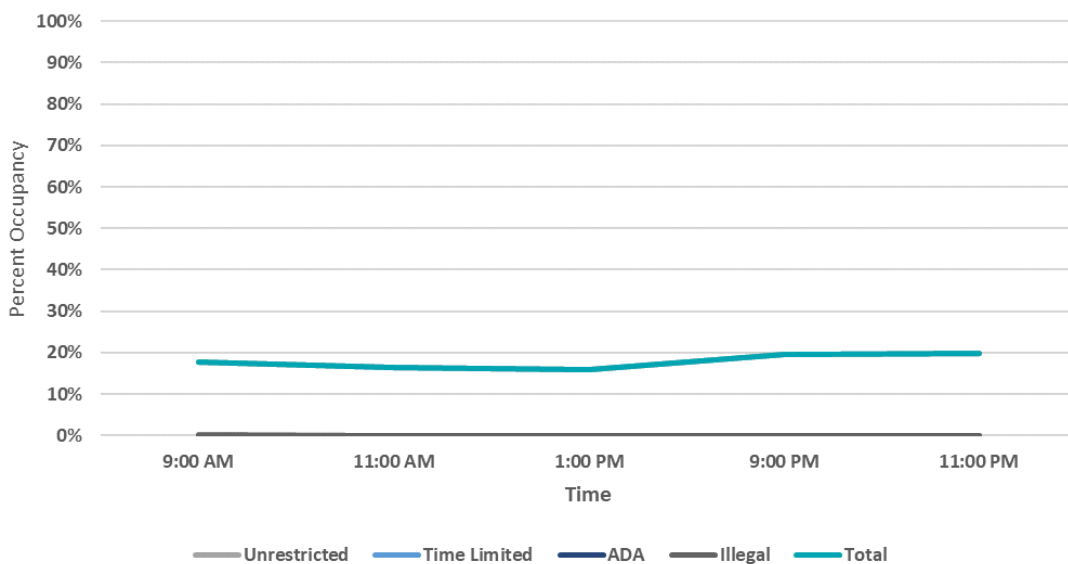
Table 95: Shaw Heights Observed Weekday Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	533	94	18%	0	0%	0	0%	1	0%	94	18%
11:00 AM	533	87	16%	0	0%	0	0%	0	0%	87	16%
1:00 PM	533	84	16%	0	0%	0	0%	0	0%	84	16%
9:00 PM	533	104	20%	0	0%	0	0%	0	0%	104	20%
11:00 PM	533	105	20%	0	0%	0	0%	0	0%	105	20%
Average	533	95	18%	0	0%	0	0%	0	0%	95	18%

Source: Walker Consultants

The figure below shows total weekday parking occupancy by time for the Shaw Heights neighborhood.

Figure 67: Shaw Heights Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 9 PM and 11 PM, on-street parking demand by block face ranged from 0% occupied (24 segments) to 86% occupied along 88th Avenue between Winona Court and Vrain Street, and along Wagner Drive between Dixon Drive and Wiley Circle. In addition to these two block faces observed above 85% occupancy, an additional seven block faces were observed between 50% & 85% occupancy during the peak.

One illegally parked vehicle was observed in the Shaw Heights neighborhood at 9 AM. This occurred on Circle Drive between 88th Avenue and Concord Lane.

The following table summarizes parking occupancy by street block face at 11 PM.

Table 96: Shaw Heights Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					Total Weekday Demand (System-wide Peak)	Weekday % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
Circle Dr	Cherry Ln	Concord Ln	E	8	0	0	0	0	0	0%	
	Cherry Ln	Norwich Way	W	12	4	0	0	0	4	40%	
	Concord Ln	Cherry Ln	W	7	1	0	0	0	1	14%	
	Concord Ln	W 88th Ave	E	5	0	0	0	0	0	0%	
	Norwich Way	Cherry Ln	E	8	1	0	0	0	1	10%	
	Norwich Way	Shaw Blvd	W	7	0	0	0	0	0	0%	
	Shaw Blvd	Mid Block	W	10	0	0	0	0	0	0%	
	Shaw Blvd	Norwich Way	E	7	2	0	0	0	2	20%	
	Shaw Blvd	Shaw Blvd	E	16	0	0	0	0	0	0%	
Highland Pl	W 88th Ave	Concord Ln	W	16	7	0	0	0	7	33%	
	Knox Ct	La Salle Pl	E	9	0	0	0	0	0	0%	
La Salle Pl	La Salle Pl	Knox Ct	W	8	0	0	0	0	0	0%	
	Highland Pl	Lowell Blvd	S	6	4	0	0	0	4	22%	
Shaw Blvd	Lowell Blvd	Highland Pl	N	9	1	0	0	0	1	6%	
	Cedar Ln	Cherry Ln	S	5	2	0	0	0	2	29%	
	Cedar Ln	Circle Dr	N	7	0	0	0	0	0	0%	
	Cherry Ln	Cedar Ln	N	7	0	0	0	0	0	0%	
	Cherry Ln	Concord Ln	S	6	2	0	0	0	2	33%	
	Circle Dr	Cedar Ln	S	6	3	0	0	0	3	38%	
	Concord Ln	Cherry Ln	N	4	1	0	0	0	1	13%	
	Concord Ln	Crescent Dr	S	5	0	0	0	0	0	0%	
	Crescent Dr	Concord Ln	N	5	2	0	0	0	2	29%	
W 88th Ave	Crescent Dr	Lowell Blvd	S	16	2	0	0	0	2	50%	
	Lowell Blvd	Crescent Dr	N	9	1	0	0	0	1	17%	
	Circle Dr	Norwich St	N	11	2	0	0	0	2	29%	
	Circle Dr	W 88th Way	S	20	0	0	0	0	0	0%	
	Lowell Blvd	W 88th Way	N	7	2	0	0	0	2	18%	
	Norwich St	Circle Dr	S	7	0	0	0	0	0	0%	
	Seton St	Utica St	N	21	2	0	0	0	2	50%	
	Utica St	Seton St	S	7	0	0	0	0	0	0%	
	Utica St	Vrain St	N	10	1	0	0	0	1	20%	
W 88th Way	Vrain St	Utica St	S	7	5	0	0	0	5	83%	
	Vrain St	Winona Ct	N	3	1	0	0	0	1	20%	
	W 88th Way	Circle Dr	N	8	0	0	0	0	0	0%	
	W 88th Way	Lowell Blvd	S	7	0	0	0	0	0	0%	
	Wagner Dr	Winona Ct	S	6	0	0	0	0	0	0%	
	Winona Ct	Vrain St	S	5	6	0	0	0	6	86%	
	Winona Ct	Wagner St	N	4	0	0	0	0	0	0%	
	Lowell Blvd	Mid Block	N	6	5	0	0	0	5	71%	
	Mid Block	Lowell Blvd	S	7	3	0	0	0	3	25%	
Wagner Dr	Mid Block	W 88th Ave	E	8	1	0	0	0	1	17%	
	W 88th Ave	Mid Block	W	10	0	0	0	0	0	0%	
	Dixon Dr	Shaw Blvd	E	5	0	0	0	0	0	0%	
	Dixon Dr	Wiley Cir	W	0	6	0	0	0	6	86%	
	Shaw Blvd	Dixon Dr	W	10	2	0	0	0	2	20%	
	Shaw Blvd	W 87th Ave	E	10	0	0	0	0	0	0%	
	W 87th Ave	Shaw Blvd	W	10	0	0	0	0	0	0%	
	W 87th Ave	W 87th Pl	E	26	0	0	0	0	0	0%	
	W 87th Pl	W 87th Ave	W	34	0	0	0	0	0	0%	
Winona Ct	W 87th Pl	W 88th Ave	E	12	1	0	0	0	1	20%	
	W 88th Ave	W 87th Pl	W	12	3	0	0	0	3	60%	
	Wiley Cir	Dixon Dr	E	7	0	0	0	0	0	0%	
	Wiley Cir	Wiley Cir	E	6	7	0	0	0	7	44%	
	Wiley Cir	Wiley Cir	W	0	5	0	0	0	5	31%	
Wagner Ln	W 88th Ave	Wagner Ln	E	6	10	0	0	0	10	63%	
	W 89th Way	Wagner Ln	W	18	2	0	0	0	2	25%	
	Wagner Ln	W 88th Ave	W	18	6	0	0	0	6	50%	
	Wagner Ln	W 89th Way	E	7	2	0	0	0	2	22%	
Total (Weekday System-wide Peak)				533	105	0	0	0	105	20%	

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (9 PM and 11 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

18 block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. For the weekday, the overall composite peak was approximately 7% higher than the system peak.

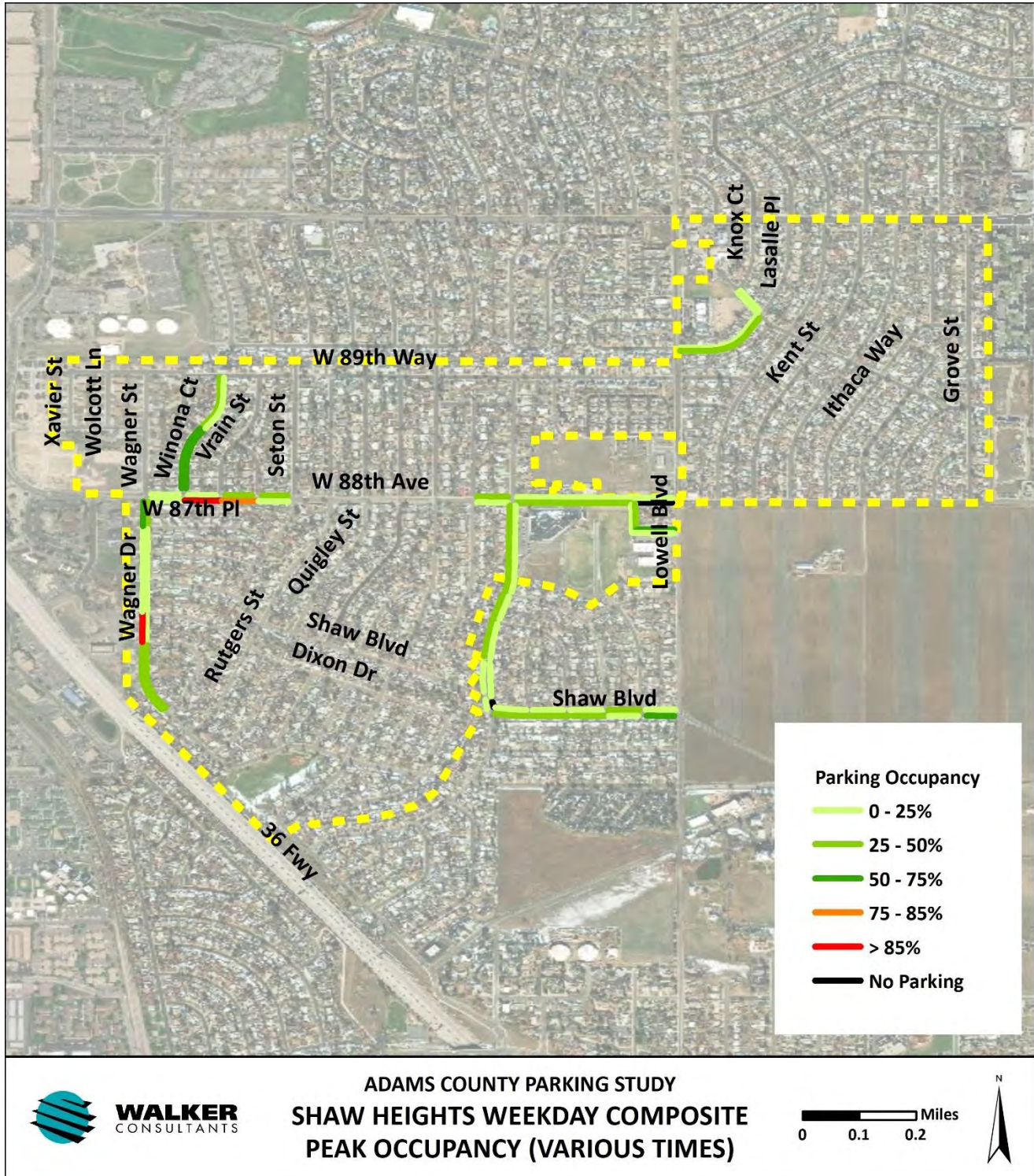
Table 97: Shaw Heights Localized Weekday Peak Occupancy by Block face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Peak % Occupancy (Across All Times)	Time that Peak Occurred
Circle Dr	Cherry Ln	Concord Ln	E	8	0	0	0	0	0	0%	
	Cherry Ln	Norwich Way	W	12	4	0	0	0	4	40%	11:00:00 PM
	Concord Ln	Cherry Ln	W	7	2	0	0	0	2	29%	9:00:00 AM
	Concord Ln	W 88th Ave	E	5	1	0	0	0	1	4%	9:00:00 AM
	Norwich Way	Cherry Ln	E	8	2	0	0	0	2	20%	11:00:00 AM
	Norwich Way	Shaw Blvd	W	7	0	0	0	0	0	0%	
	Shaw Blvd	Mid Block	W	10	0	0	0	0	0	0%	
	Shaw Blvd	Norwich Way	E	7	2	0	0	0	2	20%	11:00:00 PM
	Shaw Blvd	Shaw Blvd	E	16	0	0	0	0	0	0%	
Highland Pl	W 88th Ave	Concord Ln	W	16	8	0	0	0	8	38%	9:00:00 PM
	Knox Ct	La Salle Pl	E	9	1	0	0	0	1	17%	9:00:00 AM
La Salle Pl	La Salle Pl	Knox Ct	W	8	1	0	0	0	1	14%	1:00:00 PM
	Highland Pl	Lowell Blvd	S	6	5	0	0	0	5	28%	9:00:00 AM
Shaw Blvd	Lowell Blvd	Highland Pl	N	9	1	0	0	0	1	6%	11:00:00 PM
	Cedar Ln	Cherry Ln	S	5	2	0	0	0	2	29%	11:00:00 PM
	Cedar Ln	Circle Dr	N	7	0	0	0	0	0	0%	
	Cherry Ln	Cedar Ln	N	7	0	0	0	0	0	0%	
	Cherry Ln	Concord Ln	S	6	2	0	0	0	2	33%	11:00:00 PM
	Circle Dr	Cedar Ln	S	6	3	0	0	0	3	38%	11:00:00 PM
	Concord Ln	Cherry Ln	N	4	1	0	0	0	1	13%	11:00:00 PM
	Concord Ln	Crescent Dr	S	5	1	0	0	0	1	20%	9:00:00 AM
	Crescent Dr	Concord Ln	N	5	2	0	0	0	2	29%	11:00:00 PM
	Crescent Dr	Lowell Blvd	S	16	3	0	0	0	3	75%	9:00:00 PM
W 88th Ave	Lowell Blvd	Crescent Dr	N	9	1	0	0	0	1	17%	11:00:00 PM
	Circle Dr	Norwich St	N	11	2	0	0	0	2	29%	11:00:00 PM
	Circle Dr	W 88th Way	S	20	16	0	0	0	16	47%	9:00:00 AM
	Lowell Blvd	W 88th Way	N	7	2	0	0	0	2	18%	11:00:00 PM
	Norwich St	Circle Dr	S	7	0	0	0	0	0	0%	
	Seton St	Utica St	N	21	2	0	0	0	2	50%	11:00:00 PM
	Utica St	Seton St	S	7	0	0	0	0	0	0%	
	Utica St	Vrain St	N	10	2	0	0	0	2	40%	9:00:00 AM
	Vrain St	Utica St	S	7	5	0	0	0	5	83%	11:00:00 PM
	Vrain St	Winona Ct	N	3	1	0	0	0	1	20%	11:00:00 PM
	W 88th Way	Circle Dr	N	8	2	0	0	0	2	10%	11:00:00 PM
	W 88th Way	Lowell Blvd	S	7	0	0	0	0	0	0%	
	Wagner Dr	Winona Ct	S	6	1	0	0	0	1	14%	1:00:00 PM
W 88th Way	Winona Ct	Vrain St	S	5	7	0	0	0	7	100%	9:00:00 PM
	Winona Ct	Wagner St	N	4	0	0	0	0	0	0%	
	Lowell Blvd	Mid Block	N	6	5	0	0	0	5	71%	11:00:00 PM
	Mid Block	Lowell Blvd	S	7	3	0	0	0	3	25%	11:00:00 PM
Wagner Dr	Mid Block	W 88th Ave	E	8	2	0	0	0	2	33%	9:00:00 AM
	W 88th Ave	Mid Block	W	10	0	0	0	0	0	0%	
	Dixon Dr	Shaw Blvd	E	5	0	0	0	0	0	0%	
	Dixon Dr	Wiley Cir	W	0	6	0	0	0	6	86%	11:00:00 PM
	Shaw Blvd	Dixon Dr	W	10	2	0	0	0	2	20%	11:00:00 PM
	Shaw Blvd	W 87th Ave	E	10	0	0	0	0	0	0%	
	W 87th Ave	Shaw Blvd	W	10	0	0	0	0	0	0%	
	W 87th Ave	W 87th Pl	E	26	0	0	0	0	0	0%	
	W 87th Pl	W 87th Ave	W	34	0	0	0	0	0	0%	
	W 87th Pl	W 88th Ave	E	12	2	0	0	0	2	40%	9:00:00 AM
Winona Ct	W 88th Ave	W 87th Pl	W	12	3	0	0	0	3	60%	11:00:00 PM
	Wiley Cir	Dixon Dr	E	7	0	0	0	0	0	0%	
	Wiley Cir	Wiley Cir	E	6	7	0	0	0	7	44%	11:00:00 PM
	Wiley Cir	Wiley Cir	W	0	5	0	0	0	5	31%	11:00:00 PM
	W 88th Ave	Wagner Ln	E	6	12	0	0	0	12	75%	9:00:00 PM
	W 89th Way	Wagner Ln	W	18	3	0	0	0	3	38%	9:00:00 PM
Wagner Ln	Wagner Ln	W 88th Ave	W	18	8	0	0	0	8	67%	9:00:00 PM
	Wagner Ln	W 89th Way	E	7	2	0	0	0	2	22%	11:00:00 PM
Total (Weekday Composite)				533	142	0	0	0	142	27%	11:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekday parking demand per block face across all collection periods.

Figure 68: Shaw Heights Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the Shaw Heights neighborhood on Saturday, August 31, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 94 vehicles (18% occupied) at 11 AM to a system-wide peak of 105 vehicles (20% occupied) at 1 PM and 5 PM.

The table below summarizes parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

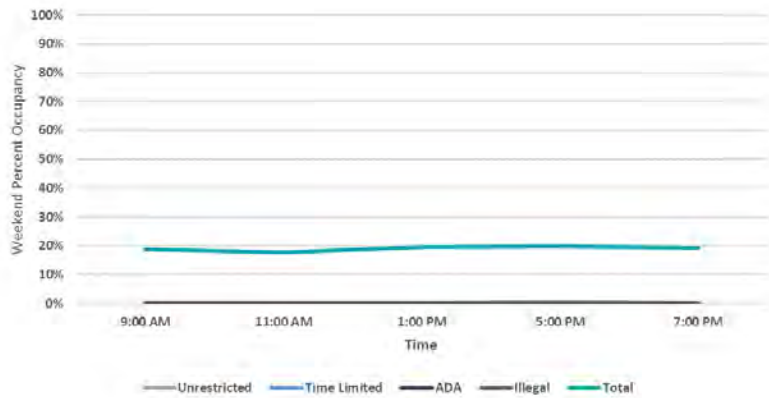
Table 98: Shaw Heights Observed Weekend Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	533	100	19%	0	0%	0	0%	1	0%	100	19%
11:00 AM	533	94	18%	0	0%	0	0%	1	0%	94	18%
1:00 PM	533	104	20%	0	0%	0	0%	1	0%	104	20%
5:00 PM	533	105	20%	0	0%	0	0%	2	0%	105	20%
7:00 PM	533	102	19%	0	0%	0	0%	1	0%	102	19%
Average	533	101	19%	0	0%	0	0%	1	0%	101	19%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the Shaw Heights neighborhood.

Figure 69: Shaw Heights Total Weekend Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekend peak at 1 PM and 5 PM, demand by block face ranged from 0% occupied (25 segments) to 100% occupied. The block face observed to experience full utilization was 88th Avenue between Seton Street and Utica Street. There were two block faces at or above 85% occupancy during the 5 PM collection period, during which time an additional seven block faces were observed between 50% and 85% occupied.

There were two illegally parked vehicles observed in the Shaw Heights neighborhood on the weekend. These were recorded along Circle Drive and 88th Way between 88th Avenue and mid-block.

The following table summarizes weekend parking occupancy by block face at 5 PM.

Table 99: Shaw Heights Weekend System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					Total Weekend Demand (System-wide Peak)	Weekend % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
Circle Dr	Cherry Ln	Concord Ln	E	10	0	0	0	0	0	0	0%
	Cherry Ln	Norwich Way	W	10	4	0	0	0	0	4	40%
	Concord Ln	Cherry Ln	W	7	1	0	0	0	0	1	14%
	Concord Ln	W 88th Ave	E	26	1	0	0	0	0	1	4%
	Norwich Way	Cherry Ln	E	10	0	0	0	0	0	0	0%
	Norwich Way	Shaw Blvd	W	7	0	0	0	0	0	0	0%
	Shaw Blvd	Mid Block	W	3	0	0	0	0	0	0	0%
	Shaw Blvd	Norwich Way	E	10	1	0	0	0	0	1	10%
	Shaw Blvd	Shaw Blvd	E	0	0	0	0	0	1	0	0%
	W 88th Ave	Concord Ln	W	21	5	0	0	0	0	5	24%
Highland Pl	Knox Ct	La Salle Pl	E	6	0	0	0	0	0	0	0%
	La Salle Pl	Knox Ct	W	7	0	0	0	0	0	0	0%
La Salle Pl	Highland Pl	Lowell Blvd	S	18	2	0	0	0	0	2	11%
	Lowell Blvd	Highland Pl	N	18	0	0	0	0	0	0	0%
Shaw Blvd	Cedar Ln	Cherry Ln	S	7	2	0	0	0	0	2	29%
	Cedar Ln	Circle Dr	N	5	0	0	0	0	0	0	0%
	Cherry Ln	Cedar Ln	N	10	0	0	0	0	0	0	0%
	Cherry Ln	Concord Ln	S	6	3	0	0	0	0	3	50%
	Circle Dr	Cedar Ln	S	8	3	0	0	0	0	3	38%
	Concord Ln	Cherry Ln	N	8	1	0	0	0	0	1	13%
	Concord Ln	Crescent Dr	S	5	0	0	0	0	0	0	0%
	Crescent Dr	Concord Ln	N	7	4	0	0	0	0	4	57%
	Crescent Dr	Lowell Blvd	S	4	1	0	0	0	0	1	25%
	Lowell Blvd	Crescent Dr	N	6	0	0	0	0	0	0	0%
W 88th Ave	Circle Dr	Norwich St	N	7	3	0	0	0	0	3	43%
	Circle Dr	W 88th Way	S	34	0	0	0	0	0	0	0%
	Lowell Blvd	W 88th Way	N	11	1	0	0	0	0	1	9%
	Norwich St	Circle Dr	S	7	0	0	0	0	0	0	0%
	Seton St	Utica St	N	4	5	0	0	0	0	5	125%
	Utica St	Seton St	S	6	2	0	0	0	0	2	33%
	Utica St	Vrain St	N	5	1	0	0	0	0	1	20%
	Vrain St	Utica St	S	6	3	0	0	0	0	3	50%
	Vrain St	Winona Ct	N	5	1	0	0	0	0	1	20%
	W 88th Way	Circle Dr	N	20	0	0	0	0	0	0	0%
	W 88th Way	Lowell Blvd	S	0	0	0	0	0	0	0	0%
	Wagner Dr	Winona Ct	S	7	0	0	0	0	0	0	0%
	Winona Ct	Vrain St	S	7	5	0	0	0	0	5	71%
	Winona Ct	Wagner St	N	7	0	0	0	0	0	0	0%
W 88th Way	Lowell Blvd	Mid Block	N	7	6	0	0	0	0	6	86%
	Mid Block	Lowell Blvd	S	12	2	0	0	0	0	2	17%
	Mid Block	W 88th Ave	E	6	2	0	0	0	1	2	33%
W 88th Ave	Mid Block	W	12	0	0	0	0	0	0	0%	
Wagner Dr	Dixon Dr	Shaw Blvd	E	8	0	0	0	0	0	0	0%
	Dixon Dr	Wiley Cir	W	7	4	0	0	0	0	4	57%
	Shaw Blvd	Dixon Dr	W	10	0	0	0	0	0	0	0%
	Shaw Blvd	W 87th Ave	E	6	0	0	0	0	0	0	0%
	W 87th Ave	Shaw Blvd	W	7	0	0	0	0	0	0	0%
	W 87th Ave	W 87th Pl	E	9	0	0	0	0	0	0	0%
	W 87th Pl	W 87th Ave	W	8	2	0	0	0	0	2	25%
	W 87th Pl	W 88th Ave	E	5	2	0	0	0	0	2	40%
	W 88th Ave	W 87th Pl	W	5	2	0	0	0	0	2	40%
	Wiley Cir	Dixon Dr	E	9	0	0	0	0	0	0	0%
Wiley Cir	Wiley Cir	E	16	7	0	0	0	0	7	44%	
Wiley Cir	Wiley Cir	W	16	11	0	0	0	0	11	69%	
Winona Ct	W 88th Ave	Wagner Ln	E	16	9	0	0	0	0	9	56%
	W 89th Way	Wagner Ln	W	8	3	0	0	0	0	3	38%
	Wagner Ln	W 88th Ave	W	12	5	0	0	0	0	5	42%
	Wagner Ln	W 89th Way	E	9	1	0	0	0	0	1	11%
Total (Weekend System-wide Peak)				533	105	0	0	2	105	20%	

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (5 PM) are highlighted in blue. Block faces with 0% occupancy across all times are highlighted in green.

15 block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. For the weekend, the overall composite peak was approximately 4% higher than the system peak.

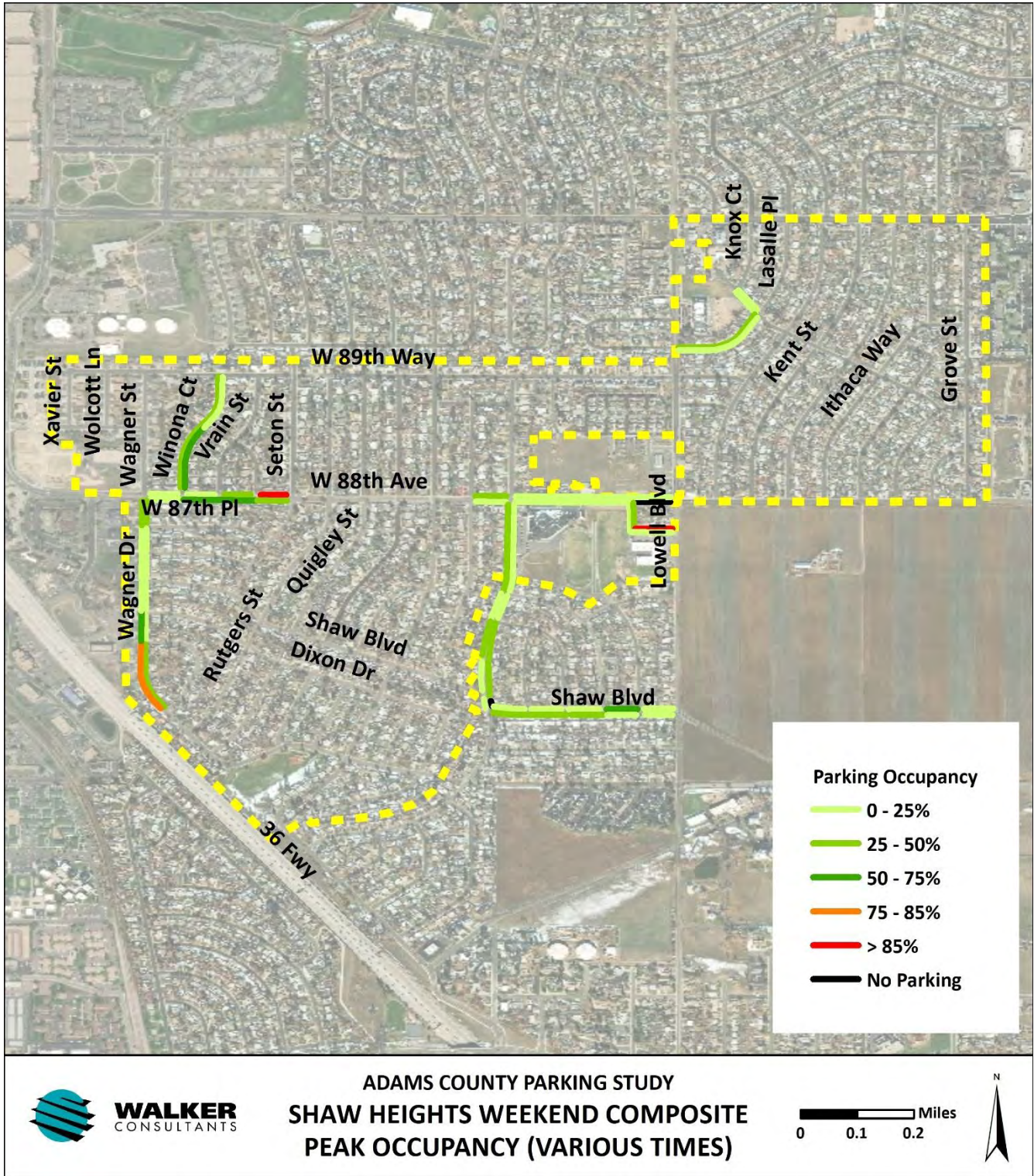
Table 100: Shaw Heights Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)	Time that Peak Occurred
Circle Dr	Cherry Ln	Concord Ln	E	10	0	0	0	0	0	0%	
	Cherry Ln	Norwich Way	W	10	4	0	0	0	4	40%	5:00:00 PM
	Concord Ln	Cherry Ln	W	7	1	0	0	0	1	14%	9:00:00 AM
	Concord Ln	W 88th Ave	E	26	1	0	0	0	1	4%	5:00:00 PM
	Norwich Way	Cherry Ln	E	10	3	0	0	0	3	30%	9:00:00 AM
	Norwich Way	Shaw Blvd	W	7	0	0	0	0	0	0%	
	Shaw Blvd	Mid Block	W	3	0	0	0	0	0	0%	
	Shaw Blvd	Norwich Way	E	10	3	0	0	0	3	30%	9:00:00 AM
Highland Pl	Shaw Blvd	Shaw Blvd	E	0	0	0	0	0	0	0%	
	W 88th Ave	Concord Ln	W	21	8	0	0	0	8	38%	11:00:00 AM
La Salle Pl	Knox Ct	La Salle Pl	E	6	0	0	0	0	0	0%	
	La Salle Pl	Knox Ct	W	7	0	0	0	0	0	0%	
La Salle Pl	Highland Pl	Lowell Blvd	S	18	2	0	0	0	2	11%	5:00:00 PM
	Lowell Blvd	Highland Pl	N	18	5	0	0	0	5	28%	1:00:00 PM
Shaw Blvd	Cedar Ln	Cherry Ln	S	7	3	0	0	0	3	43%	11:00:00 AM
	Cedar Ln	Circle Dr	N	5	0	0	0	0	0	0%	
	Cherry Ln	Cedar Ln	N	10	0	0	0	0	0	0%	
	Cherry Ln	Concord Ln	S	6	3	0	0	0	3	50%	5:00:00 PM
	Circle Dr	Cedar Ln	S	8	3	0	0	0	3	38%	5:00:00 PM
	Concord Ln	Cherry Ln	N	8	1	0	0	0	1	13%	5:00:00 PM
	Concord Ln	Crescent Dr	S	5	0	0	0	0	0	0%	
	Crescent Dr	Concord Ln	N	7	4	0	0	0	4	57%	5:00:00 PM
W 88th Ave	Crescent Dr	Lowell Blvd	S	4	1	0	0	0	1	25%	5:00:00 PM
	Lowell Blvd	Crescent Dr	N	6	0	0	0	0	0	0%	
	Circle Dr	Norwich St	N	7	3	0	0	0	3	43%	5:00:00 PM
	Circle Dr	W 88th Way	S	34	0	0	0	0	0	0%	
	Lowell Blvd	W 88th Way	N	11	1	0	0	0	1	9%	5:00:00 PM
	Norwich St	Circle Dr	S	7	0	0	0	0	0	0%	
	Seton St	Utica St	N	4	5	0	0	0	5	125%	5:00:00 PM
	Utica St	Seton St	S	6	2	0	0	0	2	33%	5:00:00 PM
	Utica St	Vrain St	N	5	2	0	0	0	2	40%	9:00:00 AM
	Vrain St	Utica St	S	6	4	0	0	0	4	67%	9:00:00 AM
	Vrain St	Winona Ct	N	5	1	0	0	0	1	20%	5:00:00 PM
	W 88th Way	Circle Dr	N	20	1	0	0	0	1	5%	9:00:00 AM
W 88th Way	W 88th Way	Lowell Blvd	S	0	0	0	0	0	0	0%	
	Wagner Dr	Winona Ct	S	7	0	0	0	0	0	0%	
	Winona Ct	Vrain St	S	7	5	0	0	0	5	71%	5:00:00 PM
	Winona Ct	Wagner St	N	7	0	0	0	0	0	0%	
	Lowell Blvd	Mid Block	N	7	6	0	0	0	6	86%	5:00:00 PM
	Mid Block	Lowell Blvd	S	12	3	0	0	0	3	25%	1:00:00 PM
	Mid Block	W 88th Ave	E	6	2	0	0	1	2	33%	5:00:00 PM
	W 88th Ave	Mid Block	W	12	0	0	0	0	0	0%	
Wagner Dr	Dixon Dr	Shaw Blvd	E	8	1	0	0	0	1	13%	1:00:00 PM
	Dixon Dr	Wiley Cir	W	7	4	0	0	0	4	57%	5:00:00 PM
	Shaw Blvd	Dixon Dr	W	10	1	0	0	0	1	10%	5:00:00 PM
	Shaw Blvd	W 87th Ave	E	6	0	0	0	0	0	0%	
	W 87th Ave	Shaw Blvd	W	7	0	0	0	0	0	0%	
	W 87th Ave	W 87th Pl	E	9	0	0	0	0	0	0%	
	W 87th Pl	W 87th Ave	W	8	2	0	0	0	2	25%	5:00:00 PM
	W 87th Pl	W 88th Ave	E	5	2	0	0	0	2	40%	5:00:00 PM
	W 88th Ave	W 87th Pl	W	5	2	0	0	0	2	40%	5:00:00 PM
	Wiley Cir	Dixon Dr	E	9	0	0	0	0	0	0%	
Winona Ct	Wiley Cir	Wiley Cir	E	16	8	0	0	0	8	50%	1:00:00 PM
	Wiley Cir	Wiley Cir	W	16	13	0	0	0	13	81%	1:00:00 PM
	W 88th Ave	Wagner Ln	E	16	9	0	0	0	9	56%	5:00:00 PM
	W 89th Way	Wagner Ln	W	8	3	0	0	0	3	38%	5:00:00 PM
Winona Ct	Wagner Ln	W 88th Ave	W	12	6	0	0	0	6	50%	7:00:00 PM
	Wagner Ln	W 89th Way	E	9	2	0	0	0	2	22%	9:00:00 AM
Total (Weekend Composite)				533	130	0	0	1	130	24%	5:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekend parking demand per block face across all collection periods.

Figure 70: Shaw Heights Heat Map of Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

SHERRELWOOD

A total of 104 block faces were surveyed across 19 streets.

INVENTORY

Walker staff observed a total of approximately 934 public on-street parking spaces along all corridors identified for study in the Sherrelwood neighborhood.

The table below summarizes parking supply by street and block face.

Table 101: Sherrelwood Observed On-street Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
Broadway	73rd Ave	Bowie Ct	E	5	0	0	5
	Bowie Ct	Campo St	E	9	0	0	9
	Bowie Ct	Greenwood Blvd	W	0	0	0	0
	Campo St	Bowie Ct	W	4	0	0	4
	Campo St	Cortez St	E	3	0	0	3
	Cortez St	Campo St	W	0	0	0	0
	Cortez St	Cragmore St	E	6	0	0	6
	Cragmore St	Cortez St	W	10	0	0	10
	Cragmore St	E Del Norte St	E	11	0	0	11
Conifer Rd	E Del Norte St	Cragmore St	W	18	0	0	18
	Dakin St	Drake St	W	6	0	0	6
	Dakin St	W 78th Pl	E	6	0	0	6
	Delta St	Drake St	E	6	0	0	6
	Delta St	Durango St	W	14	0	0	14
	Drake St	Dakin St	E	5	0	0	5
	Drake St	Delta St	W	16	0	0	16
	Durango St	Delta St	E	6	0	0	6
	Durango St	Linda Ln	W	2	0	0	2
	Ellen Ln	Mid Block	E	4	0	0	4
	Linda Ln	Ellen Ln	S	1	0	0	1
	Mid Block	Durango St	E	2	0	0	2
	W 78th Pl	Dakin St	W	3	0	0	3
Dakin St	Conifer Rd	Linda Ln	N	17	0	0	17
	Linda Ln	Conifer Rd	S	22	0	0	22
Douglas Dr	Albert Ct	Mitze Dr	S	11	0	0	11
	Kalamath St	Mitze Dr	N	14	0	0	14
	Mitze Dr	Albert Ct	N	9	0	0	9
	Mitze Dr	Kalamath St	S	16	0	0	16
Drake St	Fernando Rd	Linda Ln	S	16	0	0	16
	Linda Ln	Fernando Rd	N	11	0	0	11
E Del Norte St	Broadway	Grace Pl	S	7	0	0	7
	Grace Pl	Conifer Rd	N	3	0	0	3
	Grace Pl	Sherman Pl	S	3	0	0	3
	Sherman Pl	Grace Pl	N	9	0	0	9
	Sherman Pl	Sherman St	S	9	0	0	9
	Sherman St	Sherman Pl	N	9	0	0	9
Elmwood Ln	Mariposa St	Mid Block	N	10	0	0	10
	Mariposa St	Mid Block	S	5	0	0	5
	Mid Block	Mariposa St	N	5	0	0	5
	Mid Block	Osage St	N	9	0	0	9
	N Pecos St	Osage St	S	7	0	0	7
	Navajo St	Mariposa St	S	6	0	0	6
	Osage St	N Pecos St	N	8	0	0	8
Fernando Rd	Osage St	Navajo St	S	8	0	0	8
	Del Norte St	W Delta St	E	11	0	0	11
	Drake St	W Delta St	W	6	0	0	6
	W Delta St	Del Norte St	W	6	0	0	6
Grace Pl	W Delta St	Drake St	E	9	0	0	9
	E Del Norte St	Mid Block	E	4	0	0	4
Sherman St	Sherman St	Mid Block	N	4	0	0	4



Greenwood Blvd	Bountiful Ct	Bronco Rd	W	6	0	0	6
	Bountiful Ct	Campo St	E	7	0	0	7
	Bronco Rd	Bountiful Ct	E	11	0	0	11
	Campo St	Bountiful Ct	W	8	0	0	8
	Campo St	Cortez St	E	9	0	0	9
	Cortez St	Campo St	W	6	0	0	6
	Cortez St	Cragmore St	E	8	0	0	8
	Cragmore St	Cortez St	W	6	0	0	6
	Cragmore St	Cuchara St	E	7	0	0	7
	Cuchara St	Cragmore St	W	4	0	0	4
	Cuchara St	Del Norte St	E	4	0	0	4
	Dakin St	Drake St	W	5	0	0	5
	Dakin St	Erie St	E	6	0	0	6
	Del Norte St	Cuchara St	W	6	0	0	6
	Del Norte St	W Delta St	E	5	0	0	5
	Drake St	Dakin St	E	7	0	0	7
	Drake St	W Delta St	W	5	0	0	5
	El Paso Blvd	Elbert St	W	5	0	0	5
	Elbert St	El Paso Blvd	E	6	0	0	6
	Elbert St	Erie St	W	8	0	0	8
	Erie St	Dakin St	W	5	0	0	5
	Erie St	Elbert St	E	6	0	0	6
	W Delta St	Del Norte St	W	8	0	0	8
W Delta St	Drake St	E	8	0	0	8	
Kalamath St	Douglas Dr	Greenwood Blvd	W	25	0	0	25
	Greenwood Blvd	Douglas Dr	E	27	0	0	27
Linda Ln	Bannock St	Conifer Rd	E	11	0	0	11
	Bannock St	Drake St	N	21	0	0	21
	Conifer Rd	Bannock St	W	12	0	0	12
	Dakin St	Drake St	W	6	0	0	6
	Dakin St	Erie St	E	4	0	0	4
	Drake St	Bannock St	S	31	0	0	31
	Drake St	Dakin St	E	9	0	0	9
	Erie St	Dakin St	W	4	0	0	4
	Erie St	W 78th Pl	E	5	0	0	5
W 78th Pl	Erie St	W	5	0	0	5	
Mariposa St	Elmwood Ln	W 78th Cir	E	3	0	0	3
	W 78th Cir	Elmwood Ln	W	3	0	0	3
Osage St	Elmwood Ln	W 78th Cir	E	2	0	0	2
	W 78th Cir	Elmwood Ln	W	3	0	0	3
Sherman St	E Del Norte St	Grace Pl	W	6	0	0	6
	Grace Pl	E Del Norte St	E	17	0	0	17
	Grace Pl	Mid Block	W	9	0	0	9
	Mid Block	Grace Pl	E	11	0	0	11
Sherrelwood Dr	Mid Block	Zuni St	N	18	0	0	18
	Zuni St	Mid Block	S	14	0	0	14
W 78th Cir	Mariposa St	Osage St	N	23	0	0	23
	Mariposa St	Osage St	N	12	0	0	12
	Osage St	Mariposa St	S	36	0	0	36
	Osage St	Mariposa St	S	11	0	0	11
W 78th Pl	Conifer Rd	Linda Ln	N	13	0	0	13
	Linda Ln	Conifer Rd	S	13	0	0	13
Zuni St	Mid Block	Sherrelwood Dr	W	20	0	0	20
	Sherrelwood Dr	Mid Block	E	13	0	0	13
Total On Street Parking				934	0	0	934

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the Sherrelwood neighborhood was performed on Tuesday, September 3, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. On-street parking demand for the neighborhood ranged from 151 vehicles (16% occupied) at 11 AM to a system-wide peak of 197 vehicles (21% occupied) at 9 PM.

The table below summarizes parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

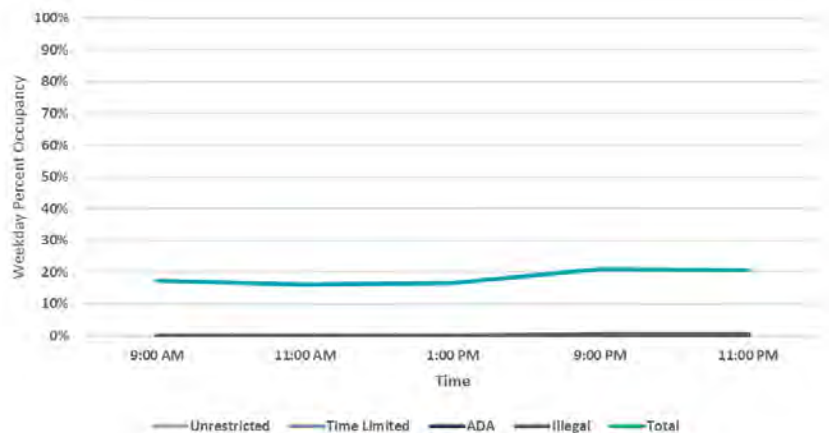
Table 102: Sherrelwood Observed Weekday Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	934	162	17%	0	0%	0	0%	1	0%	162	17%
11:00 AM	934	151	16%	0	0%	0	0%	1	0%	151	16%
1:00 PM	934	156	17%	0	0%	0	0%	1	0%	156	17%
9:00 PM	934	197	21%	0	0%	0	0%	4	0%	197	21%
11:00 PM	934	193	21%	0	0%	0	0%	4	0%	193	21%
Average	934	172	18%	0	0%	0	0%	2	0%	172	18%

Source: Walker Consultants

The figure below shows total weekday parking occupancy by time for the Sherrelwood neighborhood.

Figure 71: Sherrelwood Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 9 PM, on-street parking demand by block face ranged from 0% occupied (51 segments) to 75% occupied along three block faces. These higher utilizations were observed at Linda Lane between Erie Street and Dakin Street, Grace Place between Sherman Street and mid-block, and Greenwood Boulevard between Delta Street and Drake Street. 21 block faces with no observed parking demand

were along Broadway and Conifer Road. Overall, there were zero block faces above 85% occupancy at the system-wide peak and 13 block faces between 50% and 85%.

During the the peak, there were four illegally parked vehicles observed in the Sherrelwood neighborhood. These were located on the following segments:

- Grace Place between Del Norte Street and mid-block, Eastside (3 vehicles)
- Sherman Street between Del Norte Street and Grace Place, Westside (1 vehicle)

The following table summarizes parking occupancy by block face at 9 PM.

Table 103: Sherrelwood Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					Total Weekday Demand (System-wide Peak)	Weekday % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
Broadway	73rd Ave	Bowie Ct	E	5	0	0	0	0	0	0%	
	Bowie Ct	Campo St	E	9	0	0	0	0	0	0%	
	Bowie Ct	Greenwood Blvd	W	0	0	0	0	0	0	0%	
	Campo St	Bowie Ct	W	4	0	0	0	0	0	0%	
	Campo St	Cortez St	E	3	0	0	0	0	0	0%	
	Cortez St	Campo St	W	0	0	0	0	0	0	0%	
	Cortez St	Cragmore St	E	6	0	0	0	0	0	0%	
	Cragmore St	Cortez St	W	10	0	0	0	0	0	0%	
	Cragmore St	E Del Norte St	E	11	0	0	0	0	0	0%	
Conifer Rd	E Del Norte St	Cragmore St	W	18	0	0	0	0	0	0%	
	Dakin St	Drake St	W	6	0	0	0	0	0	0%	
	Dakin St	W 78th Pl		6	0	0	0	0	0	0%	
	Delta St	Drake St	E	6	0	0	0	0	0	0%	
	Delta St	Durango St	W	14	0	0	0	0	0	0%	
	Drake St	Dakin St	E	5	0	0	0	0	0	0%	
	Drake St	Delta St	W	16	0	0	0	0	0	0%	
	Durango St	Delta St	E	6	0	0	0	0	0	0%	
	Durango St	Linda Ln	W	2	0	0	0	0	0	0%	
	Ellen Ln	Mid Block	E	4	0	0	0	0	0	0%	
	Linda Ln	Ellen Ln	S	1	0	0	0	0	0	0%	
	Mid Block	Durango St	E	2	0	0	0	0	0	0%	
	W 78th Pl	Dakin St	W	3	0	0	0	0	0	0%	
Dakin St	Conifer Rd	Linda Ln	N	17	3	0	0	0	3	18%	
Douglas Dr	Linda Ln	Conifer Rd	S	22	0	0	0	0	0	0%	
	Albert Ct	Mitze Dr	S	11	2	0	0	0	2	18%	
	Kalamath St	Mitze Dr	N	14	5	0	0	0	5	36%	
	Mitze Dr	Albert Ct	N	9	3	0	0	0	3	33%	
Drake St	Mitze Dr	Kalamath St	S	16	0	0	0	0	0	0%	
	Fernando Rd	Linda Ln	S	16	0	0	0	0	0	0%	
	Linda Ln	Fernando Rd	N	11	2	0	0	0	2	18%	
E Del Norte St	Broadway	Grace Pl	S	7	4	0	0	0	4	57%	
	Grace Pl	Conifer Rd	N	3	0	0	0	0	0	0%	
	Grace Pl	Sherman Pl	S	3	2	0	0	0	2	67%	
	Sherman Pl	Grace Pl	N	9	6	0	0	0	6	67%	
	Sherman Pl	Sherman St	S	9	2	0	0	0	2	22%	
	Sherman St	Sherman Pl	N	9	4	0	0	0	4	44%	
Elmwood Ln	Mariposa St	Mid Block	N	10	0	0	0	0	0	0%	
	Mariposa St	Mid Block	S	5	0	0	0	0	0	0%	
	Mid Block	Mariposa St	N	5	0	0	0	0	0	0%	
	Mid Block	Osage St	N	9	0	0	0	0	0	0%	
	N Pecos St	Osage St	S	7	0	0	0	0	0	0%	
	Navajo St	Mariposa St	S	6	0	0	0	0	0	0%	
	Osage St	N Pecos St	N	8	0	0	0	0	0	0%	
	Osage St	Navajo St	S	8	1	0	0	0	1	13%	
Fernando Rd	Del Norte St	W Delta St	E	11	0	0	0	0	0	0%	
	Drake St	W Delta St	W	6	2	0	0	0	2	33%	
	W Delta St	Del Norte St	W	6	0	0	0	0	0	0%	
	W Delta St	Drake St	E	9	2	0	0	0	2	22%	
Grace Pl	E Del Norte St	Mid Block	E	4	0	0	0	3	0	0%	
	Sherman St	Mid Block	N	4	3	0	0	0	3	75%	



Greenwood Blvd	Bountiful Ct	Bronco Rd	W	6	1	0	0	0	1	17%
	Bountiful Ct	Campo St	E	7	3	0	0	0	3	43%
	Bronco Rd	Bountiful Ct	E	11	1	0	0	0	1	9%
	Campo St	Bountiful Ct	W	8	1	0	0	0	1	13%
	Campo St	Cortez St	E	9	0	0	0	0	0	0%
	Cortez St	Campo St	W	6	0	0	0	0	0	0%
	Cortez St	Cragmore St	E	8	3	0	0	0	3	38%
	Cragmore St	Cortez St	W	6	0	0	0	0	0	0%
	Cragmore St	Cuchara St	E	7	1	0	0	0	1	14%
	Cuchara St	Cragmore St	W	4	1	0	0	0	1	25%
	Cuchara St	Del Norte St	E	4	0	0	0	0	0	0%
	Dakin St	Drake St	W	5	0	0	0	0	0	0%
	Dakin St	Erie St	E	6	3	0	0	0	3	50%
	Del Norte St	Cuchara St	W	6	0	0	0	0	0	0%
	Del Norte St	W Delta St	E	5	2	0	0	0	2	40%
	Drake St	Dakin St	E	7	0	0	0	0	0	0%
	Drake St	W Delta St	W	5	0	0	0	0	0	0%
	El Paso Blvd	Elbert St	W	5	0	0	0	0	0	0%
	Elbert St	El Paso Blvd	E	6	4	0	0	0	4	67%
	Elbert St	Erie St	W	8	0	0	0	0	0	0%
Erie St	Dakin St	W	5	1	0	0	0	1	20%	
Erie St	Elbert St	E	6	4	0	0	0	4	67%	
W Delta St	Del Norte St	W	8	1	0	0	0	1	13%	
W Delta St	Drake St	E	8	6	0	0	0	6	75%	
Kalamath St	Douglas Dr	Greenwood Blvd	W	25	2	0	0	0	2	8%
	Greenwood Blvd	Douglas Dr	E	27	6	0	0	0	6	22%
Linda Ln	Bannock St	Conifer Rd	E	11	3	0	0	0	3	27%
	Bannock St	Drake St	N	21	7	0	0	0	7	33%
	Conifer Rd	Bannock St	W	12	0	0	0	0	0	0%
	Dakin St	Drake St	W	6	1	0	0	0	1	17%
	Dakin St	Erie St	E	4	1	0	0	0	1	25%
	Drake St	Bannock St	S	31	7	0	0	0	7	23%
	Drake St	Dakin St	E	9	0	0	0	0	0	0%
	Erie St	Dakin St	W	4	3	0	0	0	3	75%
Erie St	W 78th Pl	E	5	1	0	0	0	1	20%	
W 78th Pl	Erie St	W	5	0	0	0	0	0	0%	
Mariposa St	Elmwood Ln	W 78th Cir	E	3	2	0	0	0	2	67%
	W 78th Cir	Elmwood Ln	W	3	0	0	0	0	0	0%
Osage St	Elmwood Ln	W 78th Cir	E	2	0	0	0	0	0	0%
	W 78th Cir	Elmwood Ln	W	3	0	0	0	0	0	0%
Sherman St	E Del Norte St	Grace Pl	W	6	1	0	0	1	1	17%
	Grace Pl	E Del Norte St	E	17	11	0	0	0	11	65%
	Grace Pl	Mid Block	W	9	6	0	0	0	6	67%
	Mid Block	Grace Pl	E	11	8	0	0	0	8	73%
Sherrelwood Dr	Mid Block	Zuni St	N	18	1	0	0	0	1	6%
	Zuni St	Mid Block	S	14	5	0	0	0	5	36%
W 78th Cir	Mariposa St	Osage St	N	23	15	0	0	0	15	65%
	Mariposa St	Osage St	N	12	6	0	0	0	6	50%
	Osage St	Mariposa St	S	36	17	0	0	0	17	47%
	Osage St	Mariposa St	S	11	2	0	0	0	2	18%
W 78th Pl	Conifer Rd	Linda Ln	N	13	9	0	0	0	9	69%
	Linda Ln	Conifer Rd	S	13	5	0	0	0	5	38%
Zuni St	Mid Block	Sherrelwood Dr	W	20	2	0	0	0	2	10%
	Sherrelwood Dr	Mid Block	E	13	3	0	0	0	3	23%
Total (Weekday System-wide Peak)				934	197	0	0	4	197	21%

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (9 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

26 block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. For the weekday, the overall composite peak was approximately 5% higher than the system peak.



Table 104: Sherrelwood Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Peak Weekday % Occupancy (Across All Times)	Time that Peak Occurred
Broadway	73rd Ave	Bowie Ct	E	5	0	0	0	0	0	0%	
	Bowie Ct	Campo St	E	9	3	0	0	0	3	33%	11:00:00 AM
	Bowie Ct	Greenwood Blvd	W	0	0	0	0	0	0	0%	
	Campo St	Bowie Ct	W	4	0	0	0	0	0	0%	
	Campo St	Cortez St	E	3	2	0	0	0	2	67%	9:00:00 AM
	Cortez St	Campo St	W	0	0	0	0	0	0	0%	
	Cortez St	Cragmore St	E	6	2	0	0	0	2	33%	9:00:00 AM
	Cragmore St	Cortez St	W	10	2	0	0	0	2	20%	11:00:00 AM
	Cragmore St	E Del Norte St	E	11	0	0	0	0	0	0%	
Conifer Rd	E Del Norte St	Cragmore St	W	18	0	0	0	0	0	0%	
	Dakin St	Drake St	W	6	5	0	0	0	5	83%	1:00:00 PM
	Dakin St	W 78th Pl		6	0	0	0	0	0	0%	
	Delta St	Drake St	E	6	2	0	0	0	2	33%	9:00:00 AM
	Delta St	Durango St	W	14	0	0	0	0	0	0%	
	Drake St	Dakin St	E	5	0	0	0	0	0	0%	
	Drake St	Delta St	W	16	0	0	0	0	0	0%	
	Durango St	Delta St	E	6	4	0	0	0	4	67%	9:00:00 AM
	Durango St	Linda Ln	W	2	0	0	0	0	0	0%	
	Ellen Ln	Mid Block	E	4	0	0	0	0	0	0%	
	Linda Ln	Ellen Ln	S	1	0	0	0	0	0	0%	
	Mid Block	Durango St	E	2	0	0	0	0	0	0%	
	W 78th Pl	Dakin St	W	3	0	0	0	0	0	0%	
Dakin St	Conifer Rd	Linda Ln	N	17	3	0	0	0	3	18%	9:00:00 PM
	Linda Ln	Conifer Rd	S	22	0	0	0	0	0	0%	
Douglas Dr	Albert Ct	Mitze Dr	S	11	2	0	0	0	2	18%	9:00:00 PM
	Kalamath St	Mitze Dr	N	14	5	0	0	0	5	36%	9:00:00 PM
	Mitze Dr	Albert Ct	N	9	3	0	0	0	3	33%	9:00:00 PM
	Mitze Dr	Kalamath St	S	16	2	0	0	0	2	13%	11:00:00 AM
Drake St	Fernando Rd	Linda Ln	S	16	0	0	0	0	0	0%	
	Linda Ln	Fernando Rd	N	11	2	0	0	0	2	18%	9:00:00 PM
E Del Norte St	Broadway	Grace Pl	S	7	4	0	0	0	4	57%	9:00:00 PM
	Grace Pl	Conifer Rd	N	3	0	0	0	0	0	0%	
	Grace Pl	Sherman Pl	S	3	2	0	0	0	2	67%	9:00:00 PM
	Sherman Pl	Grace Pl	N	9	6	0	0	0	6	67%	9:00:00 PM
	Sherman Pl	Sherman St	S	9	2	0	0	0	2	22%	9:00:00 PM
	Sherman St	Sherman Pl	N	9	4	0	0	0	4	44%	9:00:00 PM
Elmwood Ln	Mariposa St	Mid Block	N	10	1	0	0	0	1	10%	11:00:00 AM
	Mariposa St	Mid Block	S	5	0	0	0	0	0	0%	
	Mid Block	Mariposa St	N	5	0	0	0	0	0	0%	
	Mid Block	Osage St	N	9	0	0	0	0	0	0%	
	N Pecos St	Osage St	S	7	0	0	0	0	0	0%	
	Navajo St	Mariposa St	S	6	0	0	0	0	0	0%	
	Osage St	N Pecos St	N	8	0	0	0	0	0	0%	
Fernando Rd	Osage St	Navajo St	S	8	2	0	0	0	2	25%	9:00:00 AM
	Del Norte St	W Delta St	E	11	1	0	0	0	1	9%	1:00:00 PM
	Drake St	W Delta St	W	6	2	0	0	0	2	33%	9:00:00 PM
	W Delta St	Del Norte St	W	6	0	0	0	0	0	0%	
Grace Pl	W Delta St	Drake St	E	9	2	0	0	0	2	22%	9:00:00 AM
	E Del Norte St	Mid Block	E	4	0	0	0	1	0	0%	
	Sherman St	Mid Block	N	4	3	0	0	0	3	75%	9:00:00 PM

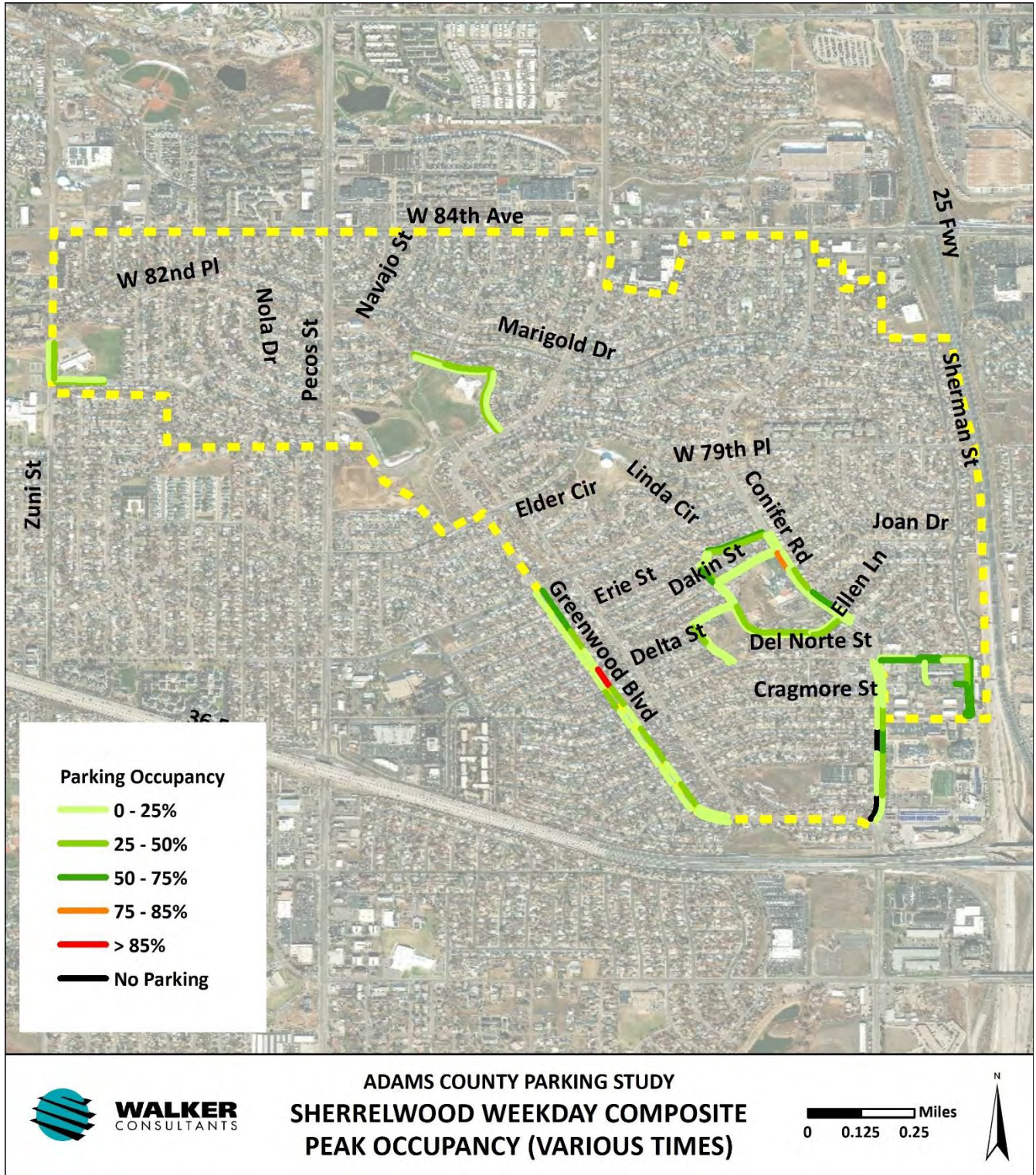


Greenwood Blvd	Bountiful Ct	Bronco Rd	W	6	1	0	0	0	1	17%	9:00:00 PM
	Bountiful Ct	Campo St	E	7	3	0	0	0	3	43%	9:00:00 PM
	Bronco Rd	Bountiful Ct	E	11	2	0	0	0	2	18%	9:00:00 AM
	Campo St	Bountiful Ct	W	8	2	0	0	0	2	25%	9:00:00 AM
	Campo St	Cortez St	E	9	1	0	0	0	1	11%	9:00:00 AM
	Cortez St	Campo St	W	6	0	0	0	0	0	0%	
	Cortez St	Cragmore St	E	8	3	0	0	0	3	38%	9:00:00 PM
	Cragmore St	Cortez St	W	6	0	0	0	0	0	0%	
	Cragmore St	Cuchara St	E	7	2	0	0	0	2	29%	9:00:00 AM
	Cuchara St	Cragmore St	W	4	1	0	0	0	1	25%	9:00:00 PM
	Cuchara St	Del Norte St	E	4	0	0	0	0	0	0%	
	Dakin St	Drake St	W	5	0	0	0	0	0	0%	
	Dakin St	Erie St	E	6	3	0	0	0	3	50%	9:00:00 PM
	Del Norte St	Cuchara St	W	6	0	0	0	0	0	0%	
	Del Norte St	W Delta St	E	5	2	0	0	0	2	40%	9:00:00 PM
	Drake St	Dakin St	E	7	1	0	0	0	1	14%	11:00:00 PM
	Drake St	W Delta St	W	5	1	0	0	0	1	20%	1:00:00 PM
	El Paso Blvd	Elbert St	W	5	0	0	0	0	0	0%	
	Elbert St	El Paso Blvd	E	6	4	0	0	0	4	67%	9:00:00 PM
	Elbert St	Erie St	W	8	0	0	0	0	0	0%	
	Erie St	Dakin St	W	5	1	0	0	0	1	20%	9:00:00 PM
	Erie St	Elbert St	E	6	4	0	0	0	4	67%	9:00:00 PM
	W Delta St	Del Norte St	W	8	3	0	0	0	3	38%	1:00:00 PM
	W Delta St	Drake St	E	8	7	0	0	0	7	88%	11:00:00 PM
Kalamath St	Douglas Dr	Greenwood Blvd	W	25	7	0	0	0	7	28%	9:00:00 AM
	Greenwood Blvd	Douglas Dr	E	27	6	0	0	0	6	22%	9:00:00 PM
Linda Ln	Bannock St	Conifer Rd	E	11	3	0	0	0	3	27%	9:00:00 PM
	Bannock St	Drake St	N	21	7	0	0	0	7	33%	9:00:00 PM
	Conifer Rd	Bannock St	W	12	0	0	0	0	0	0%	
	Dakin St	Drake St	W	6	1	0	0	0	1	17%	9:00:00 PM
	Dakin St	Erie St	E	4	2	0	0	0	2	50%	9:00:00 AM
	Drake St	Bannock St	S	31	7	0	0	0	7	23%	9:00:00 PM
	Drake St	Dakin St	E	9	0	0	0	0	0	0%	
	Erie St	Dakin St	W	4	3	0	0	0	3	75%	9:00:00 PM
	Erie St	W 78th Pl	E	5	1	0	0	0	1	20%	9:00:00 PM
Mariposa St	W 78th Pl	Erie St	W	5	0	0	0	0	0	0%	
	Elmwood Ln	W 78th Cir	E	3	3	0	0	0	3	100%	9:00:00 AM
Osage St	W 78th Cir	Elmwood Ln	W	3	1	0	0	0	1	33%	9:00:00 PM
	Elmwood Ln	W 78th Cir	E	2	0	0	0	0	0	0%	
Sherman St	W 78th Cir	Elmwood Ln	W	3	0	0	0	0	0	0%	
	E Del Norte St	Grace Pl	W	6	3	0	0	0	3	50%	9:00:00 AM
	Grace Pl	E Del Norte St	E	17	11	0	0	0	11	65%	9:00:00 PM
	Grace Pl	Mid Block	W	9	6	0	0	0	6	67%	9:00:00 PM
Sherrelwood Dr	Mid Block	Grace Pl	E	11	8	0	0	0	8	73%	9:00:00 PM
	Mid Block	Zuni St	N	18	1	0	0	0	1	6%	9:00:00 AM
	Zuni St	Mid Block	S	14	5	0	0	0	5	36%	9:00:00 PM
W 78th Cir	Mariposa St	Osage St	N	23	15	0	0	0	15	65%	9:00:00 PM
	Mariposa St	Osage St	N	12	5	0	0	0	5	42%	9:00:00 PM
	Osage St	Mariposa St	S	36	18	0	0	0	18	50%	11:00:00 AM
	Osage St	Mariposa St	S	11	2	0	0	0	2	18%	9:00:00 PM
W 78th Pl	Conifer Rd	Linda Ln	N	13	9	0	0	0	9	69%	9:00:00 PM
	Linda Ln	Conifer Rd	S	13	5	0	0	0	5	38%	9:00:00 PM
Zuni St	Mid Block	Sherrelwood Dr	W	20	3	0	0	0	3	15%	1:00:00 PM
	Sherrelwood Dr	Mid Block	E	13	4	0	0	0	4	31%	9:00:00 AM
Total (Weekday Composite)				934	243	0	0	1	243	26%	

Source: Walker Consultants

The following heat map is a composite of peak weekday parking demand per block face across all collection periods.

Figure 72: Sherrelwood Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the Sherrelwood neighborhood on Sunday, September 1, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 184 vehicles (20% occupied) at 11 AM to a system-wide peak of 208 vehicles (22% occupied) at 5 PM.

The table below summarizes parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

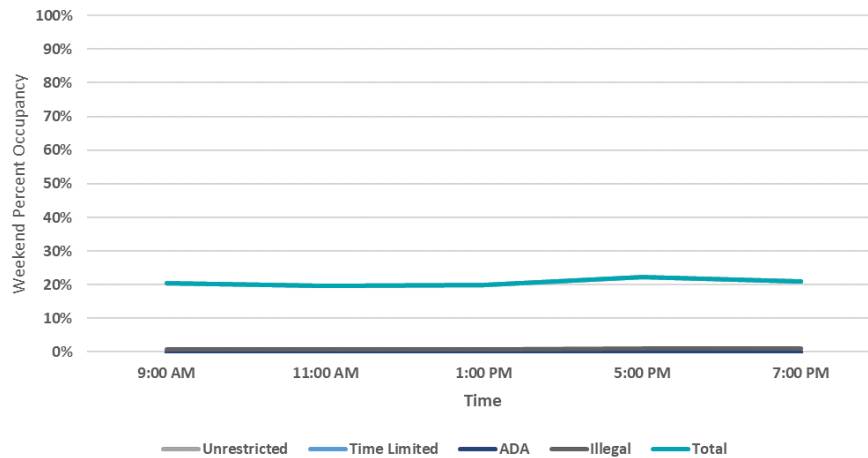
Table 105: Sherrelwood Observed Weekend Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	934	192	21%	0	0%	0	0%	8	1%	192	21%
11:00 AM	934	184	20%	0	0%	0	0%	7	1%	184	20%
1:00 PM	934	186	20%	0	0%	0	0%	8	1%	186	20%
5:00 PM	934	208	22%	0	0%	0	0%	10	1%	208	22%
7:00 PM	934	196	21%	0	0%	0	0%	10	1%	196	21%
Average	934	193	21%	0	0%	0	0%	9	0%	193	21%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the Sherrelwood neighborhood.

Figure 73: Sherrelwood Total Weekend Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekend peak at 5 PM, demand by block face ranged from 0% occupied (48 segments) to 100% occupied along Linda Lane between Erie Street and Dakin Street, and along Del Norte Street between Grace Place and Sherman Street. Three block faces were observed at or above 85% occupancy during the system-wide peak, during which time an additional 15 block faces were observed between 50% and 85% occupied.

The segments at or above 85% occupancy are:

- Linda Lane between Erie Street and Dakin Street, Westside
- Del Norte Street between Grace Place and Sherman Street, both sides

During the weekend peak, there were 2 illegally parked vehicles observed in the Sherrelwood neighborhood. These were located on the following segments:

- Linda Lane between Dakin Street and Erie Street, Eastside (1 vehicle)
- Sherman Street between Del Norte Street and Grace Place, Westside (1 vehicle)
- 78th Circle between Mariposa Street and Osage Street, both sides (7 vehicles)

The following table summarizes parking occupancy by block face at 5 PM.



Table 106: Sherrelwood Weekend System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					Weekend % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal	Total Weekend Demand (System-wide Peak)	
Broadway	73rd Ave	Bowie Ct	E	5	0	0	0	0	0	0%
	Bowie Ct	Campo St	E	9	0	0	0	0	0	0%
	Bowie Ct	Greenwood Blvd	W	0	0	0	0	0	0	0%
	Campo St	Bowie Ct	W	4	0	0	0	0	0	0%
	Campo St	Cortez St	E	3	0	0	0	0	0	0%
	Cortez St	Campo St	W	0	0	0	0	0	0	0%
	Cortez St	Cragmore St	E	6	0	0	0	0	0	0%
	Cragmore St	Cortez St	W	10	0	0	0	0	0	0%
	Cragmore St	E Del Norte St	E	11	0	0	0	0	0	0%
E Del Norte St	Cragmore St	W	18	0	0	0	0	0	0%	
Conifer Rd	Dakin St	Drake St	W	6	0	0	0	0	0	0%
	Dakin St	W 78th Pl		6	0	0	0	0	0	0%
	Delta St	Drake St	E	6	0	0	0	0	0	0%
	Delta St	Durango St	W	14	0	0	0	0	0	0%
	Drake St	Dakin St	E	5	0	0	0	0	0	0%
	Drake St	Delta St	W	16	0	0	0	0	0	0%
	Durango St	Delta St	E	6	1	0	0	0	1	17%
	Durango St	Linda Ln	W	2	0	0	0	0	0	0%
	Ellen Ln	Mid Block	E	4	0	0	0	0	0	0%
	Linda Ln	Ellen Ln	S	1	0	0	0	0	0	0%
Dakin St	W 78th Pl	Dakin St	W	3	0	0	0	0	0	0%
	Conifer Rd	Linda Ln	N	17	3	0	0	0	3	18%
Douglas Dr	Linda Ln	Conifer Rd	S	22	0	0	0	0	0	0%
	Albert Ct	Mitze Dr	S	11	2	0	0	0	2	18%
	Kalamath St	Mitze Dr	N	14	6	0	0	0	6	43%
Drake St	Mitze Dr	Albert Ct	N	9	5	0	0	0	5	56%
	Mitze Dr	Kalamath St	S	16	0	0	0	0	0	0%
Fernando Rd	Fernando Rd	Linda Ln	S	16	0	0	0	0	0	0%
	Linda Ln	Fernando Rd	N	11	4	0	0	0	4	36%
E Del Norte St	Broadway	Grace Pl	S	7	3	0	0	0	3	43%
	Grace Pl	Conifer Rd	N	3	1	0	0	0	1	33%
	Grace Pl	Sherman Pl	S	3	3	0	0	0	3	100%
	Sherman Pl	Grace Pl	N	9	8	0	0	0	8	89%
	Sherman Pl	Sherman St	S	9	4	0	0	0	4	44%
	Sherman St	Sherman Pl	N	9	4	0	0	0	4	44%
Elmwood Ln	Mariposa St	Mid Block	N	10	0	0	0	0	0	0%
	Mariposa St	Mid Block	S	5	0	0	0	0	0	0%
	Mid Block	Mariposa St	N	5	0	0	0	0	0	0%
	Mid Block	Osage St	N	9	0	0	0	0	0	0%
	N Pecos St	Osage St	S	7	1	0	0	0	1	14%
	Navajo St	Mariposa St	S	6	2	0	0	1	2	33%
	Osage St	N Pecos St	N	8	0	0	0	0	0	0%
Fernando Rd	Osage St	Navajo St	S	8	1	0	0	0	1	13%
	Del Norte St	W Delta St	E	11	0	0	0	0	0	0%
	Drake St	W Delta St	W	6	4	0	0	0	4	67%
	W Delta St	Del Norte St	W	6	0	0	0	0	0	0%
Grace Pl	W Delta St	Drake St	E	9	1	0	0	0	1	11%
	E Del Norte St	Mid Block	E	4	3	0	0	0	3	75%
	Sherman St	Mid Block	N	4	3	0	0	0	3	75%



Greenwood Blvd	Bountiful Ct	Bronco Rd	W	6	0	0	0	0	0	0%
	Bountiful Ct	Campo St	E	7	4	0	0	0	4	57%
	Bronco Rd	Bountiful Ct	E	11	1	0	0	0	1	9%
	Campo St	Bountiful Ct	W	8	0	0	0	0	0	0%
	Campo St	Cortez St	E	9	0	0	0	0	0	0%
	Cortez St	Campo St	W	6	0	0	0	0	0	0%
	Cortez St	Cragmore St	E	8	2	0	0	0	2	25%
	Cragmore St	Cortez St	W	6	0	0	0	0	0	0%
	Cragmore St	Cuchara St	E	7	1	0	0	0	1	14%
	Cuchara St	Cragmore St	W	4	2	0	0	0	2	50%
	Cuchara St	Del Norte St	E	4	1	0	0	0	1	25%
	Dakin St	Drake St	W	5	0	0	0	0	0	0%
	Dakin St	Erie St	E	6	3	0	0	0	3	50%
	Del Norte St	Cuchara St	W	6	0	0	0	0	0	0%
	Del Norte St	W Delta St	E	5	1	0	0	0	1	20%
	Drake St	Dakin St	E	7	2	0	0	0	2	29%
	Drake St	W Delta St	W	5	1	0	0	0	1	20%
	El Paso Blvd	Elbert St	W	5	0	0	0	0	0	0%
	Elbert St	El Paso Blvd	E	6	4	0	0	0	4	67%
	Elbert St	Erie St	W	8	0	0	0	0	0	0%
Erie St	Dakin St	W	5	0	0	0	0	0	0%	
Erie St	Elbert St	E	6	4	0	0	0	4	67%	
W Delta St	Del Norte St	W	8	1	0	0	0	1	13%	
W Delta St	Drake St	E	8	6	0	0	0	6	75%	
Kalamath St	Douglas Dr	Greenwood Blvd	W	25	4	0	0	0	4	16%
	Greenwood Blvd	Douglas Dr	E	27	8	0	0	0	8	30%
Linda Ln	Bannock St	Conifer Rd	E	11	3	0	0	0	3	27%
	Bannock St	Drake St	N	21	7	0	0	0	7	33%
	Conifer Rd	Bannock St	W	12	0	0	0	0	0	0%
	Dakin St	Drake St	W	6	2	0	0	0	2	33%
	Dakin St	Erie St	E	4	0	0	0	1	0	0%
	Drake St	Bannock St	S	31	4	0	0	0	4	13%
	Drake St	Dakin St	E	9	0	0	0	0	0	0%
	Erie St	Dakin St	W	4	4	0	0	0	4	100%
	Erie St	W 78th Pl	E	5	1	0	0	0	1	20%
W 78th Pl	Erie St	W	5	0	0	0	0	0	0%	
Mariposa St	Elmwood Ln	W 78th Cir	E	3	2	0	0	0	2	67%
	W 78th Cir	Elmwood Ln	W	3	2	0	0	0	2	67%
Osage St	Elmwood Ln	W 78th Cir	E	2	0	0	0	0	0	0%
	W 78th Cir	Elmwood Ln	W	3	0	0	0	0	0	0%
Sherman St	E Del Norte St	Grace Pl	W	6	4	0	0	1	4	67%
	Grace Pl	E Del Norte St	E	17	11	0	0	0	11	65%
	Grace Pl	Mid Block	W	9	4	0	0	0	4	44%
Sherrelwood Dr	Mid Block	Grace Pl	E	11	6	0	0	0	6	55%
	Mid Block	Zuni St	N	18	0	0	0	0	0	0%
W 78th Cir	Zuni St	Mid Block	S	14	5	0	0	0	5	36%
	Mariposa St	Osage St	N	23	16	0	0	6	16	70%
	Mariposa St	Osage St	N	12	5	0	0	0	5	42%
	Osage St	Mariposa St	S	36	15	0	0	1	15	42%
W 78th Pl	Osage St	Mariposa St	S	11	4	0	0	0	4	36%
	Conifer Rd	Linda Ln	N	13	3	0	0	0	3	23%
Zuni St	Linda Ln	Conifer Rd	S	13	5	0	0	0	5	38%
	Mid Block	Sherrelwood Dr	W	20	0	0	0	0	0	0%
	Sherrelwood Dr	Mid Block	E	13	1	0	0	0	1	8%
Total (Weekend System-wide Peak)				934	208	0	0	10	208	22%

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (5 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

33 block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak. For the weekend, the overall composite peak was approximately 5% higher than the system peak.



Table 107: Sherrelwood Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)	Time that Peak Occurred
Broadway	73rd Ave	Bowie Ct	E	5	0	0	0	0	0	0%	
	Bowie Ct	Campo St	E	9	0	0	0	0	0	0%	
	Bowie Ct	Greenwood Blvd	W	0	0	0	0	0	0	0%	
	Campo St	Bowie Ct	W	4	0	0	0	0	0	0%	
	Campo St	Cortez St	E	3	0	0	0	0	0	0%	
	Cortez St	Campo St	W	0	0	0	0	0	0	0%	
	Cortez St	Cragmore St	E	6	0	0	0	0	0	0%	
	Cragmore St	Cortez St	W	10	0	0	0	0	0	0%	
	Cragmore St	E Del Norte St	E	11	0	0	0	0	0	0%	
Conifer Rd	E Del Norte St	Cragmore St	W	18	0	0	0	0	0	0%	
	Dakin St	Drake St	W	6	0	0	0	0	0	0%	
	Dakin St	W 78th Pl		6	0	0	0	0	0	0%	
	Delta St	Drake St	E	6	0	0	0	0	0	0%	
	Delta St	Durango St	W	14	0	0	0	0	0	0%	
	Drake St	Dakin St	E	5	0	0	0	0	0	0%	
	Drake St	Delta St	W	16	0	0	0	0	0	0%	
	Durango St	Delta St	E	6	1	0	0	0	1	17%	5:00:00 PM
	Durango St	Linda Ln	W	2	0	0	0	0	0	0%	
	Ellen Ln	Mid Block	E	4	0	0	0	0	0	0%	
	Linda Ln	Ellen Ln	S	1	1	0	0	0	1	100%	9:00:00 AM
	Mid Block	Durango St	E	2	0	0	0	0	0	0%	
	W 78th Pl	Dakin St	W	3	0	0	0	0	0	0%	
Dakin St	Conifer Rd	Linda Ln	N	17	4	0	0	0	4	24%	7:00:00 PM
	Linda Ln	Conifer Rd	S	22	1	0	0	0	1	5%	1:00:00 PM
Douglas Dr	Albert Ct	Mitze Dr	S	11	2	0	0	0	2	18%	5:00:00 PM
	Kalamath St	Mitze Dr	N	14	6	0	0	0	6	43%	5:00:00 PM
	Mitze Dr	Albert Ct	N	9	5	0	0	0	5	56%	5:00:00 PM
	Mitze Dr	Kalamath St	S	16	0	0	0	0	0	0%	
Drake St	Fernando Rd	Linda Ln	S	16	3	0	0	0	3	19%	9:00:00 AM
	Linda Ln	Fernando Rd	N	11	4	0	0	0	4	36%	5:00:00 PM
E Del Norte St	Broadway	Grace Pl	S	7	3	0	0	0	3	43%	5:00:00 PM
	Grace Pl	Conifer Rd	N	3	1	0	0	0	1	33%	5:00:00 PM
	Grace Pl	Sherman Pl	S	3	3	0	0	0	3	100%	5:00:00 PM
	Sherman Pl	Grace Pl	N	9	8	0	0	0	8	89%	5:00:00 PM
	Sherman Pl	Sherman St	S	9	5	0	0	0	5	56%	9:00:00 AM
	Sherman St	Sherman Pl	N	9	4	0	0	0	4	44%	5:00:00 PM
Elmwood Ln	Mariposa St	Mid Block	N	10	0	0	0	0	0	0%	
	Mariposa St	Mid Block	S	5	1	0	0	0	1	20%	9:00:00 AM
	Mid Block	Mariposa St	N	5	0	0	0	0	0	0%	
	Mid Block	Osage St	N	9	0	0	0	0	0	0%	
	N Pecos St	Osage St	S	7	2	0	0	0	2	29%	1:00:00 PM
	Navajo St	Mariposa St	S	6	2	0	0	1	2	33%	5:00:00 PM
	Osage St	N Pecos St	N	8	0	0	0	0	0	0%	
Fernando Rd	Osage St	Navajo St	S	8	2	0	0	0	2	25%	11:00:00 AM
	Del Norte St	W Delta St	E	11	0	0	0	0	0	0%	
	Drake St	W Delta St	W	6	4	0	0	0	4	67%	5:00:00 PM
	W Delta St	Del Norte St	W	6	0	0	0	0	0	0%	
Grace Pl	W Delta St	Drake St	E	9	1	0	0	0	1	11%	5:00:00 PM
	E Del Norte St	Mid Block	E	4	3	0	0	0	3	75%	5:00:00 PM
	Sherman St	Mid Block	N	4	3	0	0	0	3	75%	5:00:00 PM

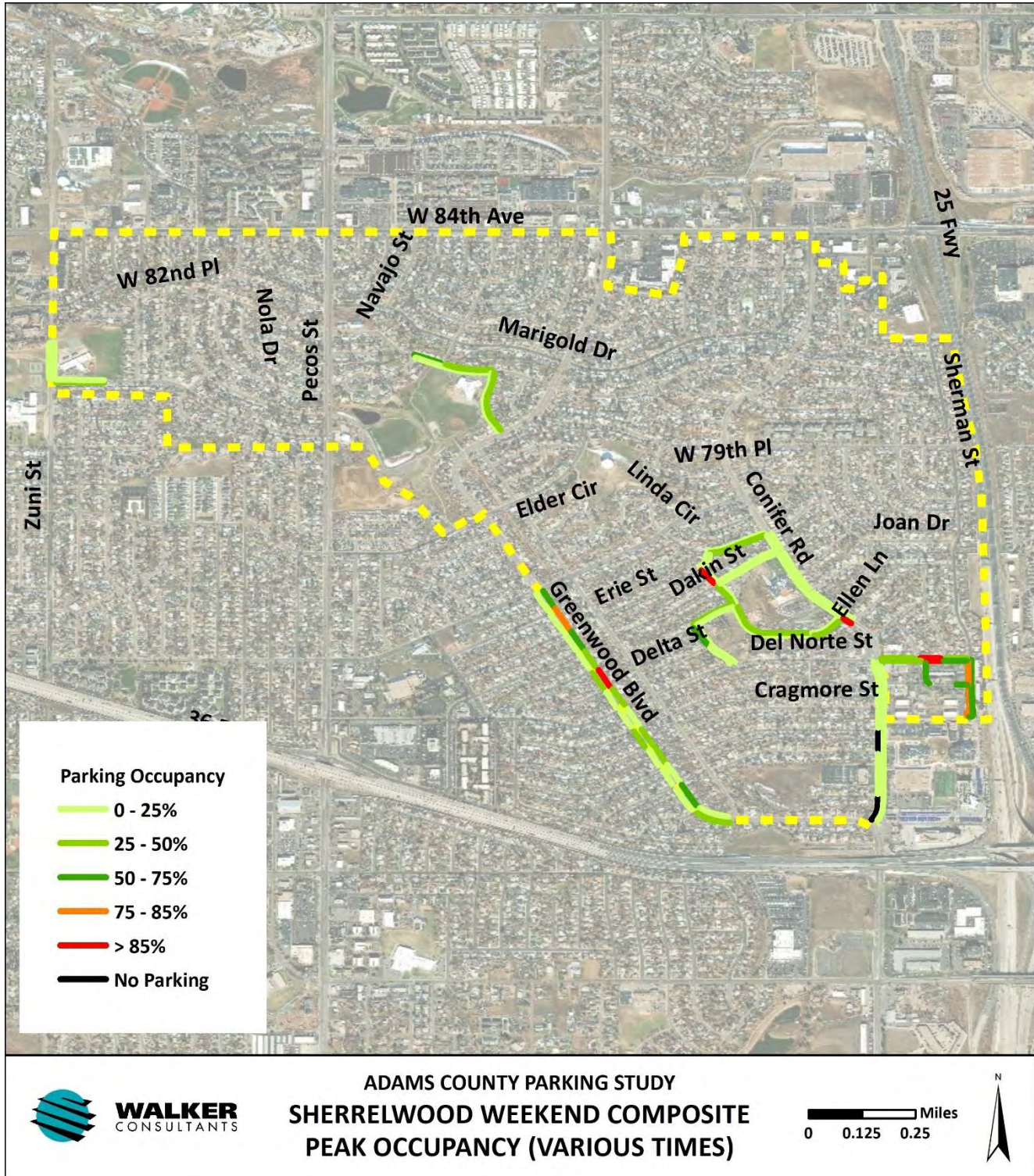


Greenwood Blvd	Bountiful Ct	Bronco Rd	W	6	2	0	0	0	2	33%	9:00:00 AM
	Bountiful Ct	Campo St	E	7	4	0	0	0	4	57%	5:00:00 PM
	Bronco Rd	Bountiful Ct	E	11	1	0	0	0	1	9%	5:00:00 PM
	Campo St	Bountiful Ct	W	8	2	0	0	0	2	25%	9:00:00 AM
	Campo St	Cortez St	E	9	0	0	0	0	0	0%	
	Cortez St	Campo St	W	6	0	0	0	0	0	0%	
	Cortez St	Cragmore St	E	8	4	0	0	0	4	50%	7:00:00 PM
	Cragmore St	Cortez St	W	6	1	0	0	0	1	17%	9:00:00 AM
	Cragmore St	Cuchara St	E	7	2	0	0	0	2	29%	7:00:00 PM
	Cuchara St	Cragmore St	W	4	2	0	0	0	2	50%	5:00:00 PM
	Cuchara St	Del Norte St	E	4	1	0	0	0	1	25%	5:00:00 PM
	Dakin St	Drake St	W	5	1	0	0	0	1	20%	9:00:00 AM
	Dakin St	Erie St	E	6	4	0	0	0	4	67%	9:00:00 AM
	Del Norte St	Cuchara St	W	6	0	0	0	0	0	0%	
	Del Norte St	W Delta St	E	5	2	0	0	0	2	40%	11:00:00 AM
	Drake St	Dakin St	E	7	2	0	0	0	2	29%	1:00:00 PM
	Drake St	W Delta St	W	5	2	0	0	0	2	40%	7:00:00 PM
	El Paso Blvd	Elbert St	W	5	0	0	0	0	0	0%	
	Elbert St	El Paso Blvd	E	6	4	0	0	0	4	67%	5:00:00 PM
	Elbert St	Erie St	W	8	0	0	0	0	0	0%	
	Erie St	Dakin St	W	5	0	0	0	0	0	0%	
	Erie St	Elbert St	E	6	5	0	0	0	5	83%	7:00:00 PM
	W Delta St	Del Norte St	W	8	1	0	0	0	1	13%	5:00:00 PM
W Delta St	Drake St	E	8	7	0	0	0	7	88%	9:00:00 AM	
Kalamath St	Douglas Dr	Greenwood Blvd	W	25	4	0	0	0	4	16%	5:00:00 PM
	Greenwood Blvd	Douglas Dr	E	27	8	0	0	0	8	30%	5:00:00 PM
Linda Ln	Bannock St	Conifer Rd	E	11	3	0	0	0	3	27%	5:00:00 PM
	Bannock St	Drake St	N	21	8	0	0	0	8	38%	7:00:00 PM
	Conifer Rd	Bannock St	W	12	0	0	0	0	0	0%	
	Dakin St	Drake St	W	6	2	0	0	0	2	33%	5:00:00 PM
	Dakin St	Erie St	E	4	1	0	0	1	1	25%	9:00:00 AM
	Drake St	Bannock St	S	31	5	0	0	0	5	16%	9:00:00 AM
	Drake St	Dakin St	E	9	1	0	0	0	1	11%	7:00:00 PM
	Erie St	Dakin St	W	4	4	0	0	0	4	100%	5:00:00 PM
	Erie St	W 78th Pl	E	5	1	0	0	0	1	20%	5:00:00 PM
	W 78th Pl	Erie St	W	5	0	0	0	0	0	0%	
Mariposa St	Elmwood Ln	W 78th Cir	E	3	3	0	0	0	3	100%	11:00:00 AM
	W 78th Cir	Elmwood Ln	W	3	2	0	0	0	2	67%	5:00:00 PM
Osage St	Elmwood Ln	W 78th Cir	E	2	0	0	0	0	0	0%	
	W 78th Cir	Elmwood Ln	W	3	0	0	0	0	0	0%	
Sherman St	E Del Norte St	Grace Pl	W	6	5	0	0	1	5	83%	7:00:00 PM
	Grace Pl	E Del Norte St	E	17	11	0	0	0	11	65%	5:00:00 PM
	Grace Pl	Mid Block	W	9	7	0	0	0	7	78%	9:00:00 AM
	Mid Block	Grace Pl	E	11	7	0	0	0	7	64%	7:00:00 PM
Sherrelwood Dr	Mid Block	Zuni St	N	18	1	0	0	0	1	6%	11:00:00 AM
	Zuni St	Mid Block	S	14	5	0	0	0	5	36%	5:00:00 PM
W 78th Cir	Mariposa St	Osage St	N	23	17	0	0	6	17	74%	11:00:00 AM
	Mariposa St	Osage St	N	12	13	0	0	6	13	57%	9:00:00 AM
	Osage St	Mariposa St	S	36	6	0	0	0	6	55%	9:00:00 AM
	Osage St	Mariposa St	S	11	14	0	0	0	14	39%	7:00:00 PM
W 78th Pl	Conifer Rd	Linda Ln	N	13	3	0	0	0	3	23%	5:00:00 PM
	Linda Ln	Conifer Rd	S	13	5	0	0	0	5	38%	5:00:00 PM
Zuni St	Mid Block	Sherrelwood Dr	W	20	0	0	0	0	0	0%	
	Sherrelwood Dr	Mid Block	E	13	1	0	0	0	1	8%	5:00:00 PM
Total (Weekend Composite)				934	253	0	0	15	253	27%	5:00 PM

Source: Walker Consultants

The following heat map is a composite of peak parking demand per block face across all collection periods.

Figure 74: Sherrelwood Heat Map of Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

UTAH JUNCTION

A total of 26 block faces were surveyed across seven streets.

INVENTORY

Walker staff observed a total of approximately 277 public on-street parking spaces along all corridors identified for study. The table below summarizes parking supply by street and block face:

Table 108: Utah Junction Observed Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
Columbine Ln	Primerose Ln	W 54th Ave	N	7	0	0	7
	Primrose Ln	Federal Blvd	N	9	0	0	9
	Rosemary Ln	Lilac Ln	N	12	0	0	12
	Rosemary Ln	Lilac Ln	S	16	0	0	16
	W 54th Ave	Primrose Ln	S	19	0	0	19
Primrose Ln	Columbine Ln	W 54th Ave	E	8	0	0	8
	Columbine Ln	W 54th Ave	W	10	0	0	10
Rosemary Ln	Columbine Ln	W 54th Ave	N	10	0	0	10
	Columbine Ln	W 54th Ave	S	14	0	0	14
W 52nd Pl	N Pecos St	Quivas St	N	6	0	0	6
	Quivas St	N Pecos St	S	5	0	0	5
	Quivas St	Raritan St	N	13	0	0	13
	Raritan St	Quivas St	S	15	0	0	15
	Raritan St	Raritan Way	N	6	0	0	6
	Raritan Way	Raritan St	S	9	0	0	9
	Raritan Way	Tejon St	N	14	0	0	14
W 53rd Ave	Wyandot St	Vallejo Way	N	2	0	0	2
	Wyandot St	Vallejo Way	S	3	0	0	3
W 54th Ave	Rosemary Ln	Columbine Ln	S	3	0	0	3
Zuni St	W 53rd Ave	W 52nd Ave	E	12	0	0	12
	W 53rd Ave	W 52nd Ave	W	19	0	0	19
	W 53rd Ave	W 53rd Pl	E	4	0	0	4
	W 53rd Pl	W 53rd Ave	W	11	0	0	11
	W 53rd Pl	W 54th Ave	E	15	0	0	15
	W 54th Ave	W 53rd Pl	W	18	0	0	18
Total On Street Parking				277	0	0	277

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the Utah Junction neighborhood was performed on Wednesday, August 28, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. Parking demand for the neighborhood ranged from 24 vehicles (9% occupied) at 1 PM to a system-wide peak of 46 vehicles (17% occupied) at 11 PM.

There were two illegally parked vehicles observed during the weekend in the Utah Junction neighborhood. These occurred on Columbine Lane between Rosemary Lane and Lilac Lane, and Zuni Street between 52nd Avenue and 53rd Avenue, both during the 7 PM collection.

The table below summarizes parking occupancy by user group per collection period across the Utah Junction neighborhood.

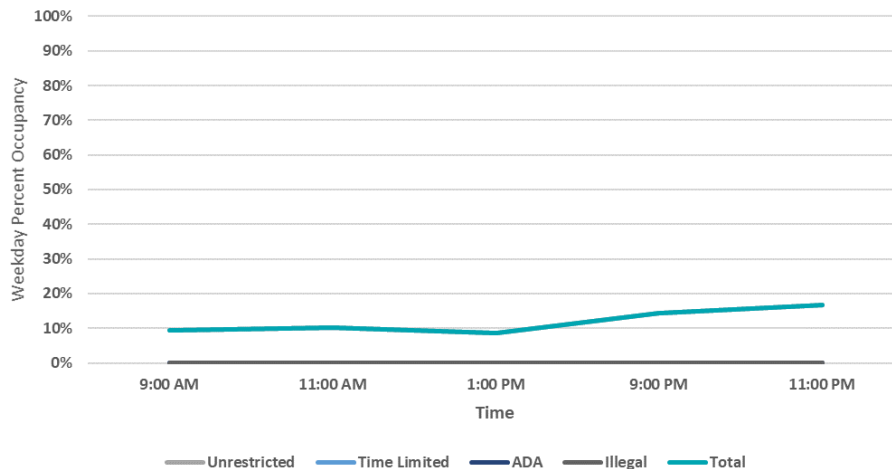
Table 109: Utah Junction Observed Weekday Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	277	26	9%	0	0%	0	0%	0	0%	26	9%
11:00 AM	277	28	10%	0	0%	0	0%	0	0%	28	10%
1:00 PM	277	24	9%	0	0%	0	0%	0	0%	24	9%
9:00 PM	277	40	14%	0	0%	0	0%	0	0%	40	14%
11:00 PM	277	46	17%	0	0%	0	0%	0	0%	46	17%
Average	277	33	12%	0	0%	0	0%	0	0%	33	12%

Source: Walker Consultants

The figure below shows total weekday parking occupancy by time for the Utah Junction neighborhood.

Figure 75: Utah Junction Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 11 PM, demand by block face ranged from 0% occupied (multiple segments) to 58% occupied on Zuni Street between 52nd Avenue and 53rd Avenue. Overall, there were eight block faces with occupancies of 0% at the system-wide weekday peak and three block faces that were at or above 50% occupied. There were no areas above 85% occupied. Parking demand during the system-wide weekday peak was largely concentrated along Columbine Road, Primrose Lane, and Zuni Street.

The following table summarizes parking occupancy by block face at 11 PM.

Table 110: Utah Junction Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					Total Weekday Demand (System-wide Peak)	Weekday % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
Columbine Ln	Primerose Ln	W 54th Ave	N	7	4	0	0	0	4	57%	
	Primrose Ln	Federal Blvd	N	9	1	0	0	0	1	11%	
	Rosemary Ln	Lilac Ln	N	12	1	0	0	0	1	8%	
	Rosemary Ln	Lilac Ln	S	16	2	0	0	0	2	13%	
	W 54th Ave	Primrose Ln	S	19	0	0	0	0	0	0%	
Primrose Ln	Columbine Ln	W 54th Ave	E	8	1	0	0	0	1	13%	
	Columbine Ln	W 54th Ave	W	10	4	0	0	0	4	40%	
Rosemary Ln	Columbine Ln	W 54th Ave	N	10	1	0	0	0	1	10%	
	Columbine Ln	W 54th Ave	S	14	1	0	0	0	1	7%	
W 52nd Pl	N Pecos St	Quivas St	N	6	2	0	0	0	2	33%	
	Quivas St	N Pecos St	S	5	0	0	0	0	0	0%	
	Quivas St	Raritan St	N	13	2	0	0	0	2	15%	
	Raritan St	Quivas St	S	15	0	0	0	0	0	0%	
	Raritan St	Raritan Way	N	6	0	0	0	0	0	0%	
	Raritan Way	Raritan St	S	9	0	0	0	0	0	0%	
	Raritan Way	Tejon St	N	14	3	0	0	0	3	21%	
W 53rd Ave	Tejon St	Raritan Way	S	17	0	0	0	0	0	0%	
	Wyandot St	Vallejo Way	N	2	0	0	0	0	0	0%	
W 54th Ave	Wyandot St	Vallejo Way	S	3	0	0	0	0	0	0%	
	Rosemary Ln	Columbine Ln	S	3	0	0	0	0	0	0%	
Zuni St	W 53rd Ave	W 52nd Ave	E	12	7	0	0	0	7	58%	
	W 53rd Ave	W 52nd Ave	W	19	1	0	0	0	1	5%	
	W 53rd Ave	W 53rd Pl	E	4	2	0	0	0	2	50%	
	W 53rd Pl	W 53rd Ave	W	11	1	0	0	0	1	9%	
	W 53rd Pl	W 54th Ave	E	15	7	0	0	0	7	47%	
	W 54th Ave	W 53rd Pl	W	18	6	0	0	0	6	33%	
Total (Weekday System-wide Peak)				277	46	0	0	0	46	17%	

Source: Walker Consultant

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (11 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

Three block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak.

Table 111: Utah Junction Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)

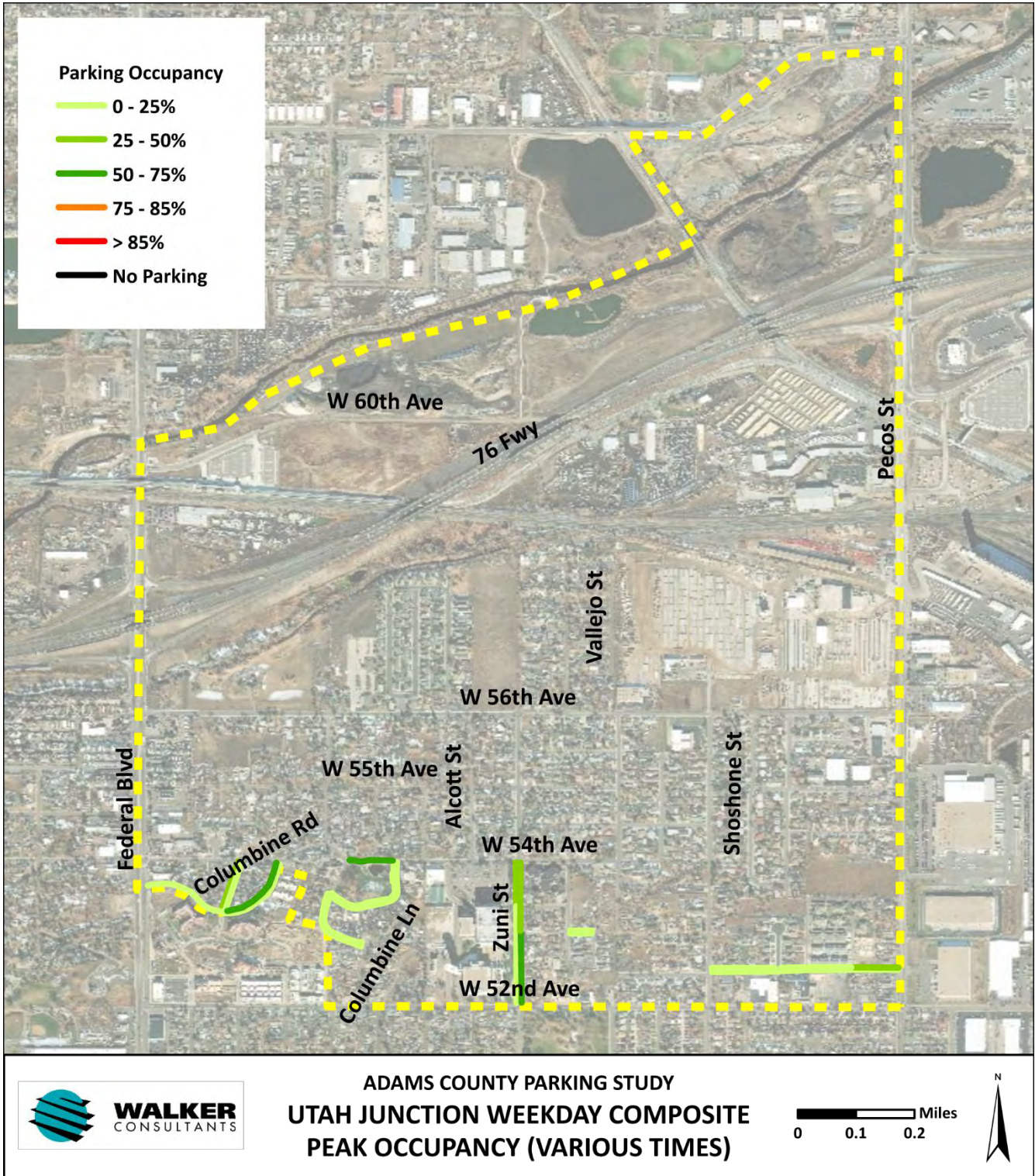
Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)							
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Peak % Occupancy (Across All Times)	Time that Peak Occurred	
Columbine Ln	Primrose Ln	W 54th Ave	N	7	4	0	0	0	0	4	57%	11:00:00 PM
	Primrose Ln	Federal Blvd	N	9	2	0	0	0	0	2	22%	9:00:00 AM
	Rosemary Ln	Lilac Ln	N	12	1	0	0	0	0	1	8%	11:00:00 PM
	Rosemary Ln	Lilac Ln	S	16	2	0	0	0	0	2	13%	11:00:00 PM
Primrose Ln	W 54th Ave	Primrose Ln	S	19	2	0	0	0	0	2	11%	1:00:00 PM
	Columbine Ln	W 54th Ave	E	8	1	0	0	0	0	1	13%	11:00:00 PM
Rosemary Ln	Columbine Ln	W 54th Ave	W	10	4	0	0	0	0	4	40%	11:00:00 PM
	Columbine Ln	W 54th Ave	N	10	1	0	0	0	0	1	10%	11:00:00 PM
W 52nd Pl	Columbine Ln	W 54th Ave	S	14	1	0	0	0	0	1	7%	11:00:00 PM
	N Pecos St	Quivas St	N	6	2	0	0	0	0	2	33%	11:00:00 PM
	Quivas St	N Pecos St	S	5	0	0	0	0	0	0	0%	
	Quivas St	Raritan St	N	13	2	0	0	0	0	2	15%	11:00:00 PM
	Raritan St	Quivas St	S	15	0	0	0	0	0	0	0%	
	Raritan St	Raritan Way	N	6	0	0	0	0	0	0	0%	
	Raritan Way	Raritan St	S	9	0	0	0	0	0	0	0%	
	Raritan Way	Tejon St	N	14	3	0	0	0	0	3	21%	11:00:00 PM
W 53rd Ave	Tejon St	Raritan Way	S	17	0	0	0	0	0	0	0%	
	Wyandot St	Vallejo Way	N	2	0	0	0	0	0	0	0%	
W 54th Ave	Wyandot St	Vallejo Way	S	3	0	0	0	0	0	0	0%	
	Rosemary Ln	Columbine Ln	S	3	2	0	0	0	0	2	67%	11:00:00 AM
Zuni St	W 53rd Ave	W 52nd Ave	E	12	7	0	0	0	0	7	58%	11:00:00 PM
	W 53rd Ave	W 52nd Ave	W	19	1	0	0	0	0	1	5%	11:00:00 PM
	W 53rd Pl	W 53rd Pl	E	4	2	0	0	0	0	2	50%	11:00:00 PM
	W 53rd Pl	W 53rd Ave	W	11	1	0	0	0	0	1	9%	11:00:00 PM
	W 53rd Pl	W 54th Ave	E	15	7	0	0	0	0	7	47%	11:00:00 PM
	W 54th Ave	W 53rd Pl	W	18	6	0	0	0	0	6	33%	11:00:00 PM
Total (Weekday Composite)				277	51	0	0	0	0	51	18%	11:00 PM

Source: Walker Consultants

The only block face where the individual parking demand was more than 15% greater than demand for that block face during the system-wide peak was the south side of 54th Avenue between Rosemary Lane and Columbine Lane. This block face contains three parking spaces and directly fronts North Pecos Hills Park. Because homes do not front this street on the north side, this demand is likely attributable to early afternoon parkgoers.

The following heat map is a composite of peak parking demand per block face across all collection periods.

Figure 76: Utah Junction Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the Utah Junction neighborhood was performed on Sunday, September 8, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 34 vehicles (12% occupied) at 7 PM to a system-wide peak of 53 vehicles (19% occupied) at 1 PM.

The table below summarizes parking occupancy by user group per collection period across the neighborhood.

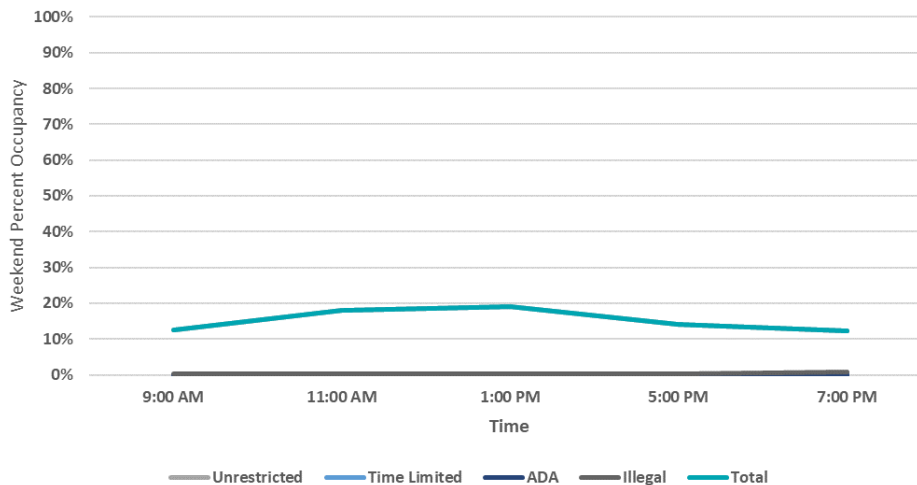
Table 112: Utah Junction Observed Weekend Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	277	35	13%	0	0%	0	0%	1	0%	35	13%
11:00 AM	277	50	18%	0	0%	0	0%	1	0%	50	18%
1:00 PM	277	53	19%	0	0%	0	0%	1	0%	53	19%
5:00 PM	277	39	14%	0	0%	0	0%	1	0%	39	14%
7:00 PM	277	34	12%	0	0%	0	0%	2	1%	34	12%
Average	277	42	15%	0	0%	0	0%	1	0%	42	15%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the Utah Junction neighborhood.

Figure 77: Utah Junction Total Weekend Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekend peak at 1 PM, demand by block face ranged from 0% occupied (10 segments) to 83% occupied on Zuni Street between 52nd Avenue and 53rd Avenue. There were no areas at or above 85% occupancy and two that were between 50% and 85% (both along Zuni Street). Parking demand during the systemwide weekday peak was largely concentrated along the same streets as observed during weekday data collections, with additional demand observed on 52nd Place.

There was one illegally parked vehicle observed in the Utah Junction neighborhood on the weekend. This occurred along Zuni Street between 52nd Avenue and 53rd Avenue.

The following table summarizes parking occupancy by block face at 1 PM on Sunday.

Table 113: Utah Junction Weekend System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					Total Weekend Demand (System-wide Peak)	Weekend % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal			
Columbine Ln	Primrose Ln	W 54th Ave	N	7	0	0	0	0	0	0%	
	Primrose Ln	Federal Blvd	N	9	2	0	0	0	2	22%	
	Rosemary Ln	Lilac Ln	N	12	2	0	0	0	2	17%	
	Rosemary Ln	Lilac Ln	S	16	3	0	0	0	3	19%	
	W 54th Ave	Primrose Ln	S	19	2	0	0	0	2	11%	
Primrose Ln	Columbine Ln	W 54th Ave	E	8	2	0	0	0	2	25%	
	Columbine Ln	W 54th Ave	W	10	1	0	0	0	1	10%	
Rosemary Ln	Columbine Ln	W 54th Ave	N	10	2	0	0	0	2	20%	
	Columbine Ln	W 54th Ave	S	14	0	0	0	0	0	0%	
W 52nd Pl	N Pecos St	Quivas St	N	6	0	0	0	0	0	0%	
	Quivas St	N Pecos St	S	5	0	0	0	0	0	0%	
	Quivas St	Raritan St	N	13	3	0	0	0	3	23%	
	Raritan St	Quivas St	S	15	0	0	0	0	0	0%	
	Raritan St	Raritan Way	N	6	0	0	0	0	0	0%	
	Raritan Way	Raritan St	S	9	0	0	0	0	0	0%	
	Raritan Way	Tejon St	N	14	5	0	0	0	5	36%	
W 53rd Ave	Wyandot St	Vallejo Way	N	2	0	0	0	0	0	0%	
	Wyandot St	Vallejo Way	S	3	0	0	0	0	0	0%	
W 54th Ave	Rosemary Ln	Columbine Ln	S	3	0	0	0	0	0	0%	
Zuni St	W 53rd Ave	W 52nd Ave	E	12	10	0	0	1	10	83%	
	W 53rd Ave	W 52nd Ave	W	19	2	0	0	0	2	11%	
	W 53rd Ave	W 53rd Pl	E	4	2	0	0	0	2	50%	
	W 53rd Pl	W 53rd Ave	W	11	1	0	0	0	1	9%	
	W 53rd Pl	W 54th Ave	E	15	6	0	0	0	6	40%	
	W 54th Ave	W 53rd Pl	W	18	3	0	0	0	3	17%	
Total (Weekend System-wide Peak)				277	53	0	0	1	53	19%	

Source: Walker Consultant

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (1 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

Six block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak of 1 PM.



Table 114: Utah Junction Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)	Time that Peak Occurred
Columbine Ln	Primerose Ln	W 54th Ave	N	7	0	0	0	0	0	0%	
	Primrose Ln	Federal Blvd	N	9	2	0	0	0	2	22%	1:00:00 PM
	Rosemary Ln	Lilac Ln	N	12	2	0	0	0	2	17%	1:00:00 PM
	Rosemary Ln	Lilac Ln	S	16	4	0	0	0	4	25%	11:00:00 AM
Primrose Ln	W 54th Ave	Primrose Ln	S	19	2	0	0	0	2	11%	1:00:00 PM
	Columbine Ln	W 54th Ave	E	8	2	0	0	0	2	25%	1:00:00 PM
Rosemary Ln	Columbine Ln	W 54th Ave	W	10	1	0	0	0	1	10%	1:00:00 PM
	Columbine Ln	W 54th Ave	N	10	2	0	0	0	2	20%	1:00:00 PM
	Columbine Ln	W 54th Ave	S	14	0	0	0	0	0	0%	
W 52nd Pl	N Pecos St	Quivas St	N	6	0	0	0	0	0	0%	
	Quivas St	N Pecos St	S	5	0	0	0	0	0	0%	
	Quivas St	Raritan St	N	13	4	0	0	0	4	31%	11:00:00 AM
	Raritan St	Quivas St	S	15	0	0	0	0	0	0%	
	Raritan St	Raritan Way	N	6	1	0	0	0	1	17%	11:00:00 AM
	Raritan Way	Raritan St	S	9	0	0	0	0	0	0%	
	Raritan Way	Tejon St	N	14	5	0	0	0	5	36%	1:00:00 PM
W 53rd Ave	Tejon St	Raritan Way	S	17	7	0	0	0	7	41%	1:00:00 PM
	Wyandot St	Vallejo Way	N	2	0	0	0	0	0	0%	
W 54th Ave	Wyandot St	Vallejo Way	S	3	0	0	0	0	0	0%	
	Rosemary Ln	Columbine Ln	S	3	0	0	0	0	0	0%	
Zuni St	W 53rd Ave	W 52nd Ave	E	12	10	0	0	1	10	83%	1:00:00 PM
	W 53rd Ave	W 52nd Ave	W	19	4	0	0	0	4	21%	5:00:00 PM
	W 53rd Ave	W 53rd Pl	E	4	2	0	0	0	2	50%	1:00:00 PM
	W 53rd Pl	W 53rd Ave	W	11	1	0	0	0	1	9%	1:00:00 PM
	W 53rd Pl	W 54th Ave	E	15	7	0	0	0	7	47%	9:00:00 AM
	W 54th Ave	W 53rd Pl	W	18	4	0	0	0	4	22%	7:00:00 PM
Total (Weekend Composite)				277	60	0	0	1	60	22%	1:00 PM

Source: Walker Consultants

The following heat map is a composite of peak weekend parking demand for per block face across all collection periods.

Figure 78: Utah Junction Heat Map of Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WELBY

A total of 87 block faces were surveyed across 15 streets. One off-street surface lot was also surveyed.

INVENTORY

Walker staff observed a total of approximately 1,234 public on-street parking spaces along all corridors identified for study in the Welby neighborhood, as well as 153 off-street parking spaces at the Rotella Park Lot located at 1815 Coronado Parkway.

The tables below summarize parking supply by street and block face as well as the parking supply for the Rotella Lot.



Table 115: Welby Observed On-street Parking Supply by Facility

Street	From	To	Side of Street	Un-restricted	Time Limited	ADA	Total
84th Ave	Downing Dr	Franklin Dr	S	11	0	0	11
	Franklin Dr	Downing Dr	N	12	0	0	12
	Franklin Dr	Franklin St	S	9	0	0	9
	Franklin St	Franklin Dr	N	9	0	0	9
City View Dr	Cleo St	Explorador Calle	N	34	0	0	34
	Dawson Dr	De Soto Dr	S	8	0	0	8
	De Soto Dr	Dawson Dr	N	0	0	0	0
	De Soto Dr	Edison St	S	7	0	0	7
	Edison St	De Soto Dr	N	4	0	0	4
	Edison St	Essex Dr	S	8	0	0	8
	Essex Dr	Edison St	N	8	0	0	8
	Explorador Calle	Cleo St	S	27	0	0	27
Cleo St	City View Dr	Ladean St	E	12	0	0	12
	City View Dr	Nueva Vista Dr	W	11	0	0	11
	High St	Ladean St	W	7	0	0	7
	High St	Race St	E	5	0	0	5
	Ladean St	City View Dr	W	15	0	0	15
	Ladean St	High St	E	6	0	0	6
	Nueva Vista Dr	City View Dr	E	8	0	0	8
	Race St	High St	W	4	0	0	4
	Race St	Vine St	S	6	0	0	6
	Vine St	Race St	N	6	0	0	6
Coronado Pkwy	Downing Dr	Downing St	S	17	0	0	17
	Downing Dr	Ogden St	N	22	0	0	22
	Downing St	Lafayette St	S	8	0	0	8
	Driveway	Downing Dr	S	8	0	0	8
	Driveway	Driveway	S	0	0	0	0
	E 81st Pl	Explorador Calle	S	6	0	0	6
	Emerson St	Clarkson St	N	8	0	0	8
	Explorador Calle	Downing Dr	N	37	0	0	37
	Explorador Calle	Nueva Vista Dr	N	16	0	0	16
	Explorador Calle	York St	S	38	0	0	38
	Lafayette St	Niver Ct	S	5	0	0	5
	Niver Ct	E 81st Pl	S	10	0	0	10
	Ogden St	Emerson St	N	9	0	0	9
Dawson Dr	City View Dr	Mid Block	E	27	0	0	27
	Essex Dr	Mid Block	N	13	0	0	13
	Mid Block	City View Dr	W	20	0	0	20
	Mid Block	Essex Dr	S	15	0	0	15
Downing Dr	84th Ave	E 83rd Pl	W	6	0	0	6
	E 83rd Pl	84th Ave	E	4	0	0	4
	E 83rd Pl	Nueva Vista Dr	W	14	0	0	14
	Nueva Vista Dr	E 83rd Pl	E	21	0	0	21



E 73rd Ave	Gilpin Way	Lafayette St	N	35	0	0	35
	Lafayette St	Gilpin Way	S	30	0	0	30
	Lafayette St	Washington St	N	49	0	0	49
	Washington St	Lafayette St	S	41	0	0	41
E 78th Ave	Mid Block	York St	N	15	0	0	15
	York St	Mid Block	S	5	0	0	5
Essex Dr	City View Dr	Dawson Dr	E	19	0	0	19
	Dawson Dr	City View Dr	W	19	0	0	19
Explorador Calle	Coronado Pkwy	Solana Dr	E	9	0	0	9
	Nueva Vista Dr	Quivira Dr	W	8	0	0	8
	Quivira Dr	Nueva Vista Dr	E	8	0	0	8
	Quivira Dr	Solana Dr	W	7	0	0	7
	Solana Dr	Coronado Pkwy	W	6	0	0	6
	Solana Dr	Quivira Dr	E	8	0	0	8
Franklin St	84th Ave	E 83rd Pl	W	9	0	0	9
	E 83rd Ave	E 83rd Dr	E	9	0	0	9
	E 83rd Ave	Nueva Vista Dr	W	12	0	0	12
	E 83rd Dr	E 83rd Ave	W	12	0	0	12
	E 83rd Dr	E 83rd Pl	E	9	0	0	9
	E 83rd Pl	84th Ave	E	11	0	0	11
	E 83rd Pl	E 83rd Dr	W	9	0	0	9
	Nueva Vista Dr	E 83rd Ave	E	8	0	0	8
Gilpin Way	E 70th Ave	E 73rd Ave	E	27	0	0	27
	E 73rd Ave	E 70th Ave	W	30	0	0	30
Nueva Vista Dr	Cleo St	Coronado Pkwy	S	19	0	0	19
	Cleo St	Explorador Calle	N	23	0	0	23
	Coronado Pkwy	Cleo St	N	16	0	0	16
	Downing Dr	Franklin St	S	21	0	0	21
	Explorador Calle	Cleo St	S	33	0	0	33
	Franklin St	Downing Dr	N	27	0	0	27
Rainbow Ave	McDougal St	McElwain Blvd	W	30	0	0	30
	McElwain Blvd	McDougal St	E	37	0	0	37
Sheldon Dr	Corona St	Dawson Dr	S	8	0	0	8
	Dawson Dr	Corona St	N	8	0	0	8
	Dawson Dr	De Soto Dr	S	8	0	0	8
	De Soto Dr	Dawson Dr	N	9	0	0	9
	De Soto Dr	Edison St	S	7	0	0	7
	Edison St	De Soto Dr	N	10	0	0	10
	Edison St	Essex Dr	S	8	0	0	8
	Essex Dr	Edison St	N	10	0	0	10
	Essex Dr	Faraday St	S	10	0	0	10
	Faraday St	Essex Dr	N	10	0	0	10
Faraday St	Franklin St	S	12	0	0	12	
Franklin St	Faraday St	N	12	0	0	12	
Total On Street Parking				1,234	0	0	1,234

Source: Walker Consultants

Table 116: Welby Observed Off-street Parking Supply by Facility

Off-street Lot	Un-restricted	Time Limited	ADA	Total
Rotella Park Lot	153	0	6	159

Source: Walker Consultants

OCCUPANCY

WEEKDAY

The weekday parking survey for the Welby neighborhood was performed on Wednesday, September 4, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 9 PM, and 11 PM. On-street parking demand for the neighborhood ranged from 172 vehicles (14% occupied) at 9 AM and 11 AM to a system-wide peak of 223 vehicles (18%) at 1 PM. Off-street demand at the Rotella Park Lot ranged from 1 vehicle (1% occupied) at 9 PM and 11 PM to 6 vehicles (4% occupied) at 11 AM.

The tables below summarize parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

Table 117: Welby Observed Weekday On-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	1,234	172	14%	0	0%	0	0%	3	0%	172	14%
11:00 AM	1,234	178	14%	0	0%	0	0%	3	0%	178	14%
1:00 PM	1,234	223	18%	0	0%	0	0%	13	1%	223	18%
9:00 PM	1,234	203	16%	0	0%	0	0%	14	1%	203	16%
11:00 PM	1,234	203	16%	0	0%	0	0%	14	1%	203	16%
Average	1,234	196	16%	0	0%	0	0%	9	0%	196	16%

Source: Walker Consultants

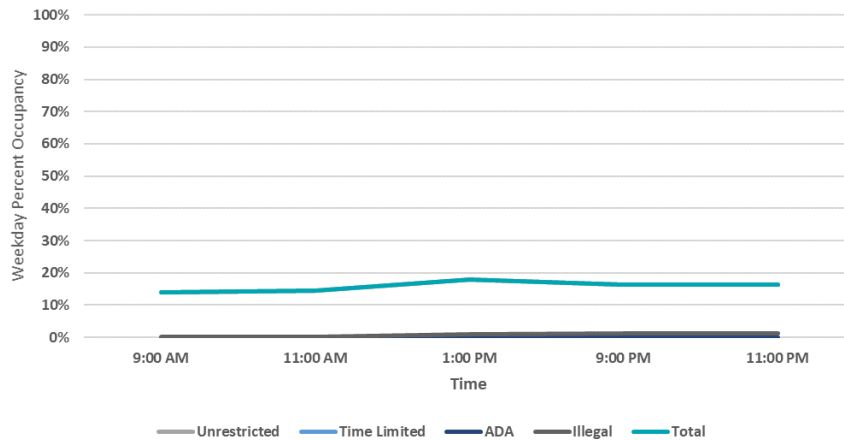
Table 118: Welby Observed Weekday Off-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	153	5	3%	0	0%	0	0%	0	0%	5	3%
11:00 AM	153	6	4%	0	0%	0	0%	0	0%	6	4%
1:00 PM	153	3	2%	0	0%	0	0%	0	0%	3	2%
9:00 PM	153	1	1%	0	0%	0	0%	0	0%	1	1%
11:00 PM	153	1	1%	0	0%	0	0%	0	0%	1	1%
Average	153	3	2%	0	0%	0	0%	0	0%	3	2%

Source: Walker Consultants

The figure below shows total weekday parking occupancy by time for the Welby neighborhood.

Figure 79: Welby Total Weekday Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekday peak at 1 PM, on-street parking demand by block face ranged from 0% occupied (38 segments) to 100% occupied. Full utilization of on-street spaces occurred along Downing Drive between 83rd Place and Nueva Vista Drive. During the peak period, there were two block faces at or above 85% occupancy and an additional 9 block faces between 50% and 85% occupied.

The segments at or above 85% occupancy are:

- Downing Drive between 83rd Place and Nueva Vista Drive, both sides

During the peak, there were 13 illegally parked vehicles observed in the Welby neighborhood. These were located on the following segments:

- 84th Avenue between Downing Drive and Franklin Drive, Southside (1 vehicle)
- Coronado Parkway between Explorador Calle Avenue and Nueva Vista Drive, Northside (6 vehicles)
- Coronado Parkway between Explorador Calle Avenue and York Street, Southside (1 vehicle)
- Dawson Drive between Essex Drive and mid-block, Northside (1 vehicle)
- Downing Drive between 83rd Place and Nueva Vista Drive, both sides (3 vehicles)
- Sheldon Drive between Faraday Street and Franklin Street, Southside (1 vehicle)

The following table summarizes parking occupancy by block face at 1 PM.



Table 119: Welby Weekday System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (System-wide Peak)					Weekday % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal	Total Weekday Demand (System-wide Peak)	
84th Ave	Downing Dr	Franklin Dr	S	11	8	0	0	1	8	73%
	Franklin Dr	Downing Dr	N	12	5	0	0	0	5	42%
	Franklin Dr	Franklin St	S	9	0	0	0	0	0	0%
	Franklin St	Franklin Dr	N	9	1	0	0	0	1	11%
City View Dr	Cleo St	Explorador Calle	N	34	1	0	0	0	1	3%
	Dawson Dr	De Soto Dr	S	8	0	0	0	0	0	0%
	De Soto Dr	Dawson Dr	N	0	0	0	0	0	0	0%
	De Soto Dr	Edison St	S	7	2	0	0	0	2	29%
	Edison St	De Soto Dr	N	4	0	0	0	0	0	0%
	Edison St	Essex Dr	S	8	3	0	0	0	3	38%
	Essex Dr	Edison St	N	8	0	0	0	0	0	0%
	Explorador Calle	Cleo St	S	27	6	0	0	0	6	22%
Cleo St	City View Dr	Ladean St	E	12	0	0	0	0	0	0%
	City View Dr	Nueva Vista Dr	W	11	0	0	0	0	0	0%
	High St	Ladean St	W	7	2	0	0	0	2	29%
	High St	Race St	E	5	1	0	0	0	1	20%
	Ladean St	City View Dr	W	15	4	0	0	0	4	15%
	Ladean St	High St	E	6	1	0	0	0	1	17%
	Nueva Vista Dr	City View Dr	E	8	1	0	0	0	1	13%
	Race St	High St	W	4	0	0	0	0	0	0%
	Race St	Vine St	S	6	1	0	0	0	1	17%
Coronado Pkwy	Vine St	Race St	N	6	1	0	0	0	1	17%
	Downing Dr	Downing St	S	17	4	0	0	0	4	24%
	Downing Dr	Ogden St	N	22	3	0	0	0	3	14%
	Downing St	Lafayette St	S	8	0	0	0	0	0	0%
	Driveway	Downing Dr	S	8	5	0	0	0	5	63%
	Driveway	Driveway	S	0	0	0	0	0	0	0%
	E 81st Pl	Explorador Calle	S	6	0	0	0	0	0	0%
	Emerson St	Clarkson St	N	8	0	0	0	0	0	0%
	Explorador Calle	Downing Dr	N	37	2	0	0	0	2	5%
	Explorador Calle	Nueva Vista Dr	N	16	9	0	0	6	9	56%
	Explorador Calle	York St	S	38	15	0	0	1	15	39%
	Lafayette St	Niver Ct	S	5	0	0	0	0	0	0%
	Niver Ct	E 81st Pl	S	10	0	0	0	0	0	0%
Ogden St	Emerson St	N	9	0	0	0	0	0	0%	
Dawson Dr	City View Dr	Mid Block	E	27	8	0	0	0	8	30%
	Essex Dr	Mid Block	N	13	8	0	0	1	8	62%
	Mid Block	City View Dr	W	20	4	0	0	0	4	20%
	Mid Block	Essex Dr	S	15	7	0	0	0	7	47%
Downing Dr	84th Ave	E 83rd Pl	W	6	5	0	0	0	5	83%
	E 83rd Pl	84th Ave	E	4	2	0	0	0	2	50%
	E 83rd Pl	Nueva Vista Dr	W	14	14	0	0	2	14	100%
	Nueva Vista Dr	E 83rd Pl	E	21	18	0	0	1	18	86%



E 73rd Ave	Gilpin Way	Lafayette St	N	35	0	0	0	0	0	0%
	Lafayette St	Gilpin Way	S	30	0	0	0	0	0	0%
	Lafayette St	Washington St	N	49	0	0	0	0	0	0%
	Washington St	Lafayette St	S	41	0	0	0	0	0	0%
E 78th Ave	Mid Block	York St	N	15	0	0	0	0	0	0%
	York St	Mid Block	S	5	0	0	0	0	0	0%
Essex Dr	City View Dr	Dawson Dr	E	19	3	0	0	0	3	16%
	Dawson Dr	City View Dr	W	19	2	0	0	0	2	11%
Explorador Calle	Coronado Pkwy	Solana Dr	E	9	2	0	0	0	2	22%
	Nueva Vista Dr	Quivira Dr	W	8	0	0	0	0	0	0%
	Quivira Dr	Nueva Vista Dr	E	8	0	0	0	0	0	0%
	Quivira Dr	Solana Dr	W	7	3	0	0	0	3	50%
	Solana Dr	Coronado Pkwy	W	6	2	0	0	0	2	25%
Franklin St	Solana Dr	Quivira Dr	E	8	0	0	0	0	0	0%
	84th Ave	E 83rd Pl	W	9	0	0	0	0	0	0%
	E 83rd Ave	E 83rd Dr	E	9	0	0	0	0	0	0%
	E 83rd Ave	Nueva Vista Dr	W	12	0	0	0	0	0	0%
	E 83rd Dr	E 83rd Ave	W	12	5	0	0	0	5	42%
	E 83rd Dr	E 83rd Pl	E	9	1	0	0	0	1	11%
	E 83rd Pl	84th Ave	E	11	0	0	0	0	0	0%
E 83rd Pl	E 83rd Dr	W	9	6	0	0	0	6	67%	
Gilpin Way	Nueva Vista Dr	E 83rd Ave	E	8	0	0	0	0	0	0%
	E 70th Ave	E 73rd Ave	E	27	3	0	0	0	3	11%
Nueva Vista Dr	E 73rd Ave	E 70th Ave	W	30	0	0	0	0	0	0%
	Cleo St	Coronado Pkwy	S	19	8	0	0	0	8	42%
	Cleo St	Explorador Calle	N	23	10	0	0	0	10	43%
	Coronado Pkwy	Cleo St	N	16	5	0	0	0	5	31%
	Downing Dr	Franklin St	S	21	10	0	0	0	10	48%
	Explorador Calle	Cleo St	S	33	2	0	0	0	2	6%
Rainbow Ave	Franklin St	Downing Dr	N	27	0	0	0	0	0	0%
	McDougal St	McElwain Blvd	W	30	7	0	0	0	7	23%
Sheldon Dr	McElwain Blvd	McDougal St	E	37	0	0	0	0	0	0%
	Corona St	Dawson Dr	S	8	0	0	0	0	0	0%
	Dawson Dr	Corona St	N	8	0	0	0	0	0	0%
	Dawson Dr	De Soto Dr	S	8	1	0	0	0	1	13%
	De Soto Dr	Dawson Dr	N	9	0	0	0	0	0	0%
	De Soto Dr	Edison St	S	7	4	0	0	0	4	57%
	Edison St	De Soto Dr	N	10	0	0	0	0	0	0%
	Edison St	Essex Dr	S	8	2	0	0	0	2	25%
	Essex Dr	Edison St	N	10	0	0	0	0	0	0%
	Essex Dr	Faraday St	S	10	0	0	0	0	0	0%
	Faraday St	Essex Dr	N	10	0	0	0	0	0	0%
	Faraday St	Franklin St	S	12	5	0	0	0	5	42%
Franklin St	Faraday St	N	12	1	0	0	0	1	8%	
Total (Weekday System-wide Peak)				1,234	223	0	0	13	223	18%

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each block face. Peaks for individual block faces that did not occur during the system-wide peak (1 PM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

35 block faces were observed with individual demand peak occupancies greater than they exhibited during the system-wide peak of 1 PM. For the weekday, the overall composite peak was approximately 6% higher than the system peak.



Table 120: Welby Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekday Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekday Demand (Across All Times)	Peak % Occupancy (Across All Times)	Time that Peak Occurred
84th Ave	Downing Dr	Franklin Dr	S	11	8	0	0	1	8	73%	1:00:00 PM
	Franklin Dr	Downing Dr	N	12	5	0	0	0	5	42%	1:00:00 PM
	Franklin Dr	Franklin St	S	9	1	0	0	0	1	11%	9:00:00 PM
	Franklin St	Franklin Dr	N	9	1	0	0	0	1	11%	1:00:00 PM
City View Dr	Cleo St	Explorador Calle	N	34	4	0	0	0	4	12%	9:00:00 PM
	Dawson Dr	De Soto Dr	S	8	3	0	0	0	3	38%	9:00:00 AM
	De Soto Dr	Dawson Dr	N	0	0	0	0	0	0	0%	
	De Soto Dr	Edison St	S	7	3	0	0	0	3	43%	11:00:00 AM
	Edison St	De Soto Dr	N	4	0	0	0	0	0	0%	1:00:00 PM
	Edison St	Essex Dr	S	8	4	0	0	0	4	50%	9:00:00 AM
	Essex Dr	Edison St	N	8	2	0	0	0	2	25%	9:00:00 AM
	Explorador Calle	Cleo St	S	27	8	0	0	0	8	30%	9:00:00 PM
Cleo St	City View Dr	Ladean St	E	12	1	0	0	0	1	8%	9:00:00 PM
	City View Dr	Nueva Vista Dr	W	11	0	0	0	0	0	0%	
	High St	Ladean St	W	7	2	0	0	0	2	29%	1:00:00 PM
	High St	Race St	E	5	3	0	0	0	3	60%	9:00:00 PM
	Ladean St	City View Dr	W	15	4	0	0	0	4	27%	9:00:00 AM
	Ladean St	High St	E	6	1	0	0	0	1	17%	1:00:00 PM
	Nueva Vista Dr	City View Dr	E	8	3	0	0	0	3	38%	9:00:00 PM
	Race St	High St	W	4	0	0	0	0	0	0%	
	Race St	Vine St	S	6	1	0	0	0	1	17%	1:00:00 PM
	Vine St	Race St	N	6	1	0	0	0	1	17%	1:00:00 PM
Coronado Pkwy	Downing Dr	Downing St	S	17	4	0	0	0	4	24%	1:00:00 PM
	Downing Dr	Ogden St	N	22	3	0	0	0	3	14%	1:00:00 PM
	Downing St	Lafayette St	S	8	6	0	0	0	6	75%	9:00:00 PM
	Driveway	Downing Dr	S	8	7	0	0	0	7	88%	9:00:00 AM
	Driveway	Driveway	S	0	0	0	0	0	0	0%	
	E 81st Pl	Explorador Calle	S	6	0	0	0	0	0	0%	
	Emerson St	Clarkson St	N	8	0	0	0	0	0	0%	
	Explorador Calle	Downing Dr	N	37	2	0	0	0	2	5%	1:00:00 PM
	Explorador Calle	Nueva Vista Dr	N	16	11	0	0	0	11	69%	9:00:00 AM
	Explorador Calle	York St	S	38	15	0	0	1	15	39%	1:00:00 PM
	Lafayette St	Niver Ct	S	5	0	0	0	0	0	0%	
	Niver Ct	E 81st Pl	S	10	0	0	0	0	0	0%	
	Ogden St	Emerson St	N	9	0	0	0	0	0	0%	
Dawson Dr	City View Dr	Mid Block	E	27	8	0	0	0	8	30%	1:00:00 PM
	Essex Dr	Mid Block	N	13	8	0	0	1	8	62%	1:00:00 PM
	Mid Block	City View Dr	W	20	8	0	0	1	8	40%	11:00:00 PM
	Mid Block	Essex Dr	S	15	9	0	0	1	9	60%	9:00:00 PM
Downing Dr	84th Ave	E 83rd Pl	W	6	5	0	0	0	5	83%	1:00:00 PM
	E 83rd Pl	84th Ave	E	4	2	0	0	0	2	50%	1:00:00 PM
	E 83rd Pl	Nueva Vista Dr	W	14	14	0	0	2	14	100%	1:00:00 PM
	Nueva Vista Dr	E 83rd Pl	E	21	18	0	0	1	18	86%	1:00:00 PM

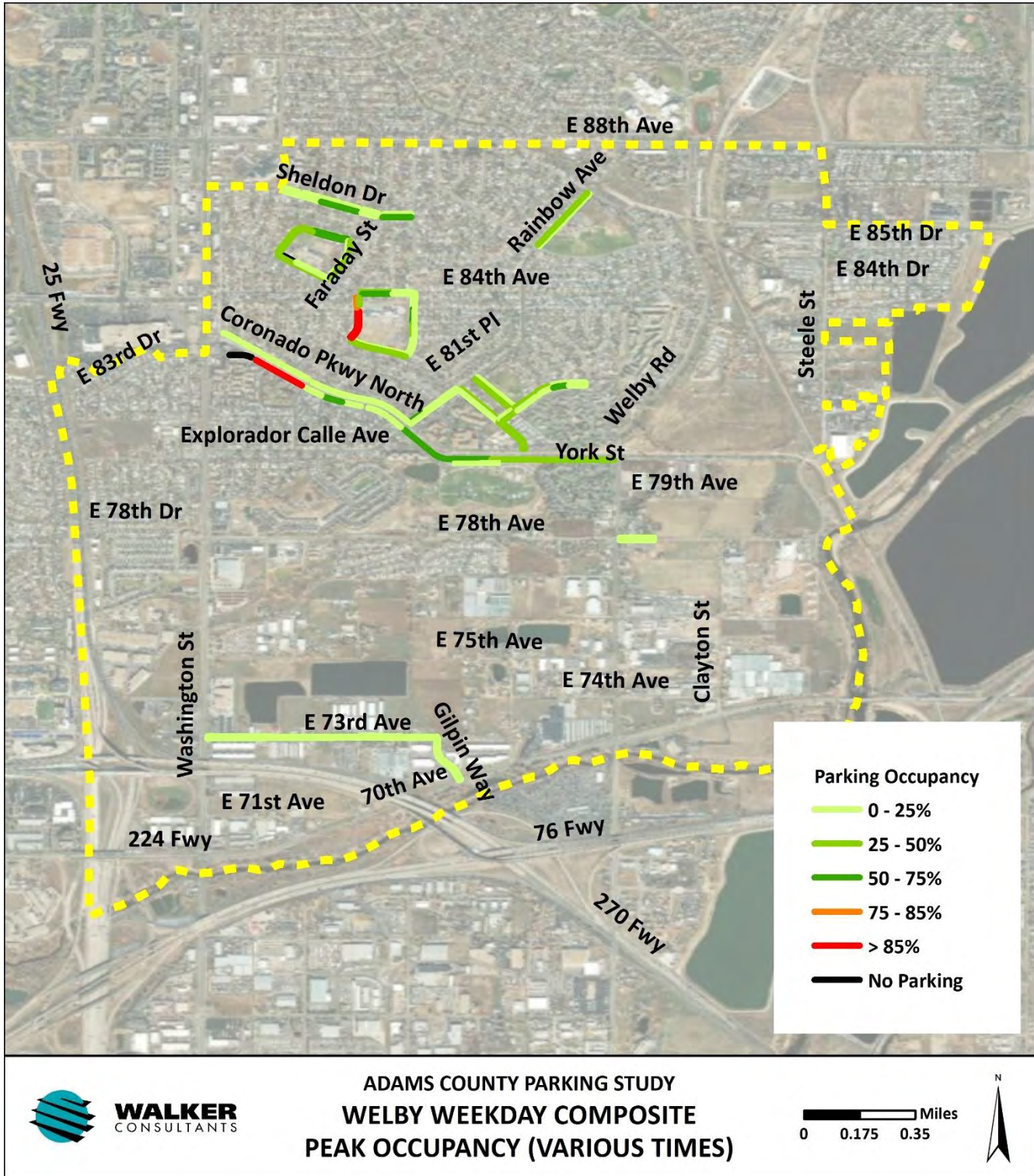


E 73rd Ave	Gilpin Way	Lafayette St	N	35	1	0	0	0	1	3%	11:00:00 AM
	Lafayette St	Gilpin Way	S	30	0	0	0	0	0	0%	
	Lafayette St	Washington St	N	49	0	0	0	0	0	0%	
E 78th Ave	Washington St	Lafayette St	S	41	0	0	0	0	0	0%	
	Mid Block	York St	N	15	0	0	0	0	0	0%	
Essex Dr	York St	Mid Block	S	5	0	0	0	0	0	0%	
	City View Dr	Dawson Dr	E	19	7	0	0	0	7	37%	11:00:00 PM
Explorador Calle	Dawson Dr	City View Dr	W	19	4	0	0	0	4	21%	9:00:00 PM
	Coronado Pkwy	Solana Dr	E	9	2	0	0	0	2	22%	1:00:00 PM
	Nueva Vista Dr	Quivira Dr	W	8	0	0	0	0	0	0%	
	Quivira Dr	Nueva Vista Dr	E	8	0	0	0	0	0	0%	
	Quivira Dr	Solana Dr	W	7	4	0	0	0	4	67%	9:00:00 PM
	Solana Dr	Coronado Pkwy	W	6	5	0	0	0	5	63%	9:00:00 PM
	Solana Dr	Quivira Dr	E	8	2	0	0	0	2	25%	1:00:00 PM
Franklin St	84th Ave	E 83rd Pl	W	9	0	0	0	0	0	0%	
	E 83rd Ave	E 83rd Dr	E	9	0	0	0	0	0	0%	
	E 83rd Ave	Nueva Vista Dr	W	12	0	0	0	0	0	0%	
	E 83rd Dr	E 83rd Ave	W	12	5	0	0	0	5	42%	1:00:00 PM
	E 83rd Dr	E 83rd Pl	E	9	1	0	0	0	1	11%	1:00:00 PM
	E 83rd Pl	84th Ave	E	11	1	0	0	0	1	9%	9:00:00 PM
	E 83rd Pl	E 83rd Dr	W	9	6	0	0	0	6	67%	1:00:00 PM
	Nueva Vista Dr	E 83rd Ave	E	8	2	0	0	0	2	25%	9:00:00 PM
Gilpin Way	E 70th Ave	E 73rd Ave	E	27	6	0	0	0	6	22%	9:00:00 AM
	E 73rd Ave	E 70th Ave	W	30	2	0	0	0	2	7%	9:00:00 PM
Nueva Vista Dr	Cleo St	Coronado Pkwy	S	19	9	0	0	0	9	47%	9:00:00 PM
	Cleo St	Explorador Calle	N	23	11	0	0	0	11	48%	9:00:00 PM
	Coronado Pkwy	Cleo St	N	16	6	0	0	0	6	38%	9:00:00 AM
	Downing Dr	Franklin St	S	21	10	0	0	0	10	48%	1:00:00 PM
	Explorador Calle	Cleo St	S	33	2	0	0	0	2	6%	1:00:00 PM
Rainbow Ave	Franklin St	Downing Dr	N	27	0	0	0	0	0	0%	
	McDougal St	McElwain Blvd	W	30	10	0	0	0	10	33%	9:00:00 PM
Sheldon Dr	McElwain Blvd	McDougal St	E	37	3	0	0	0	3	8%	9:00:00 PM
	Corona St	Dawson Dr	S	8	0	0	0	0	0	0%	
	Dawson Dr	Corona St	N	8	0	0	0	0	0	0%	
	Dawson Dr	De Soto Dr	S	8	1	0	0	0	1	13%	1:00:00 PM
	De Soto Dr	Dawson Dr	N	9	0	0	0	0	0	0%	
	De Soto Dr	Edison St	S	7	4	0	0	0	4	57%	1:00:00 PM
	Edison St	De Soto Dr	N	10	0	0	0	0	0	0%	
	Edison St	Essex Dr	S	8	5	0	0	0	5	63%	9:00:00 PM
	Essex Dr	Edison St	N	10	2	0	0	0	2	20%	9:00:00 PM
	Essex Dr	Faraday St	S	10	0	0	0	0	0	0%	
Faraday St	Faraday St	Essex Dr	N	10	0	0	0	0	0	0%	
	Faraday St	Franklin St	S	12	7	0	0	0	7	58%	9:00:00 PM
	Franklin St	Faraday St	N	12	3	0	0	0	3	25%	9:00:00 PM
Total (Weekday Composite)				1,234	296	0	0	8	296	24%	1:00 PM

Source: Walker Consultants

The following heat map is a composite of peak parking demand per block face across all collection periods.

Figure 80: Welby Heat Map of Localized Weekday Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants

WEEKEND

The weekend parking survey for the Welby neighborhood was performed on Sunday, September 8, 2019. Occupancy data was collected at 9 AM, 11 AM, 1 PM, 5 PM, and 7 PM. Parking demand for the neighborhood ranged from 178 vehicles (14% occupied) at 1 PM to a system-wide peak of 200 vehicles (16% occupied) at 9 AM. Off-street demand at the Rotella Park Lot ranged from 0 vehicles at 9 PM to 13 vehicles (8% occupied) at 9 AM.

The tables below summarize parking occupancy by user group per collection period across the neighborhood. Cells shaded in peach indicate illegally parked vehicles.

Table 121: Welby Observed Weekend On-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	1,234	200	16%	0	0%	0	0%	17	1%	200	16%
11:00 AM	1,234	199	16%	0	0%	0	0%	14	1%	199	16%
1:00 PM	1,234	178	14%	0	0%	0	0%	11	1%	178	14%
5:00 PM	1,234	190	15%	0	0%	0	0%	13	1%	190	15%
7:00 PM	1,234	200	16%	0	0%	0	0%	17	1%	200	16%
Average	1,234	193	16%	0	0%	0	0%	14	0%	193	16%

Source: Walker Consultants

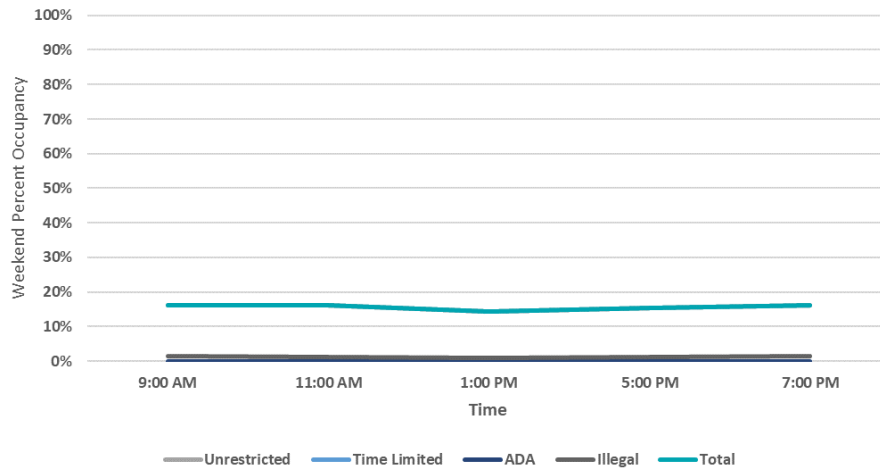
Table 122: Welby Observed Weekend Off-street Parking Occupancy by User Group and Time of Day

Time	Supply	Unrestricted		Time Limited		ADA		Illegal		Total	
		Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy	Parked	Occupancy
9:00 AM	153	13	8%	0	0%	0	0%	0	0%	13	8%
11:00 AM	153	6	4%	0	0%	0	0%	0	0%	6	4%
1:00 PM	153	3	2%	0	0%	0	0%	0	0%	3	2%
9:00 PM	153	0	0%	0	0%	0	0%	0	0%	0	0%
11:00 PM	153	1	1%	0	0%	0	0%	0	0%	1	1%
Average	153	5	3%	0	0%	0	0%	0	0%	5	3%

Source: Walker Consultants

The figure below shows total weekend parking occupancy by time for the Welby neighborhood.

Figure 81: Welby Total Weekend Parking Occupancy by Percent



Source: Walker Consultants

During the system-wide weekend peak at 9 AM, parking demand by block face ranged from 0% occupied (41 segments) to 80% occupied, which was observed along 78th Avenue between York Street and mid-block. There were seven block faces at or above 85% occupancy during the system peak and an additional 13 block faces between 50% and 85% occupancy.

At the weekend peak, there were 17 illegally parked vehicles observed in the Welby neighborhood. These were located on the following segments:

- City View Drive between Explorador Calle Avenue and Cleo Street, Southside (1 vehicle)
- Coronado Parkway between driveway and Downing Drive, Southside (1 vehicle)
- Coronado Parkway between Explorador Calle Avenue and Nueva Vista Drive, Northside (4 vehicles)
- Coronado Parkway between Explorador Calle Avenue and York Street, Southside (10 vehicles)
- 78th Avenue between York Street and mid-block, Southside (1 vehicle)

The following table summarizes parking occupancy by block face at 9 AM.

Table 123: Welby Weekend System-wide Peak Parking Occupancy by Block Face

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (System-wide Peak)					Weekend % Occupancy (System-wide Peak)
					Un-restricted	Time Limited	ADA	Illegal	Total Weekend Demand (System-wide Peak)	
84th Ave	Downing Dr	Franklin Dr	S	11	4	0	0	0	4	36%
	Franklin Dr	Downing Dr	N	12	0	0	0	0	0	0%
	Franklin Dr	Franklin St	S	9	1	0	0	0	1	11%
	Franklin St	Franklin Dr	N	9	0	0	0	0	0	0%
City View Dr	Cleo St	Explorador Calle	N	34	6	0	0	0	6	18%
	Dawson Dr	De Soto Dr	S	8	0	0	0	0	0	0%
	De Soto Dr	Dawson Dr	N	0	0	0	0	0	0	0%
	De Soto Dr	Edison St	S	7	1	0	0	0	1	14%
	Edison St	De Soto Dr	N	4	0	0	0	0	0	0%
	Edison St	Essex Dr	S	8	2	0	0	0	2	25%
	Essex Dr	Edison St	N	8	6	0	0	0	6	75%
Cleo St	Explorador Calle	Cleo St	S	27	8	0	0	1	8	30%
	City View Dr	Ladean St	E	12	0	0	0	0	0	0%
	City View Dr	Nueva Vista Dr	W	11	1	0	0	0	1	9%
	High St	Ladean St	W	7	1	0	0	0	1	14%
	High St	Race St	E	5	3	0	0	0	3	60%
	Ladean St	City View Dr	W	15	2	0	0	0	2	13%
	Ladean St	High St	E	6	2	0	0	0	2	33%
	Nueva Vista Dr	City View Dr	E	8	4	0	0	0	4	50%
	Race St	High St	W	4	0	0	0	0	0	0%
	Race St	Vine St	S	6	2	0	0	0	2	33%
Coronado Pkwy	Vine St	Race St	N	6	2	0	0	0	2	33%
	Downing Dr	Downing St	S	17	8	0	0	0	8	47%
	Downing Dr	Ogden St	N	22	5	0	0	0	5	23%
	Downing St	Lafayette St	S	8	5	0	0	0	5	63%
	Driveway	Downing Dr	S	8	5	0	0	1	5	63%
	Driveway	Driveway	S	0	0	0	0	0	0	0%
	E 81st Pl	Explorador Calle	S	6	0	0	0	0	0	0%
	Emerson St	Clarkson St	N	8	0	0	0	0	0	0%
	Explorador Calle	Downing Dr	N	37	3	0	0	0	3	8%
	Explorador Calle	Nueva Vista Dr	N	16	9	0	0	4	9	56%
	Explorador Calle	York St	S	38	10	0	0	10	10	26%
	Lafayette St	Niver Ct	S	5	0	0	0	0	0	0%
	Niver Ct	E 81st Pl	S	10	0	0	0	0	0	0%
Ogden St	Emerson St	N	9	3	0	0	0	3	33%	
Dawson Dr	City View Dr	Mid Block	E	27	2	0	0	0	2	7%
	Essex Dr	Mid Block	N	13	6	0	0	0	6	46%
	Mid Block	City View Dr	W	20	4	0	0	0	4	20%
	Mid Block	Essex Dr	S	15	7	0	0	0	7	47%
Downing Dr	84th Ave	E 83rd Pl	W	6	0	0	0	0	0	0%
	E 83rd Pl	84th Ave	E	4	0	0	0	0	0	0%
	E 83rd Pl	Nueva Vista Dr	W	14	4	0	0	0	4	29%
	Nueva Vista Dr	E 83rd Pl	E	21	0	0	0	0	0	0%



E 73rd Ave	Gilpin Way	Lafayette St	N	35	0	0	0	0	0	0%
	Lafayette St	Gilpin Way	S	30	0	0	0	0	0	0%
	Lafayette St	Washington St	N	49	0	0	0	0	0	0%
	Washington St	Lafayette St	S	41	0	0	0	0	0	0%
E 78th Ave	Mid Block	York St	N	15	5	0	0	0	5	33%
	York St	Mid Block	S	5	4	0	0	1	4	80%
Essex Dr	City View Dr	Dawson Dr	E	19	6	0	0	0	6	32%
	Dawson Dr	City View Dr	W	19	7	0	0	0	7	37%
Explorador Calle	Coronado Pkwy	Solana Dr	E	9	2	0	0	0	2	22%
	Nueva Vista Dr	Quivira Dr	W	8	0	0	0	0	0	0%
	Quivira Dr	Nueva Vista Dr	E	8	0	0	0	0	0	0%
	Quivira Dr	Solana Dr	W	7	3	0	0	0	3	50%
	Solana Dr	Coronado Pkwy	W	6	5	0	0	0	5	63%
Franklin St	Solana Dr	Quivira Dr	E	8	2	0	0	0	2	25%
	84th Ave	E 83rd Pl	W	9	0	0	0	0	0	0%
	E 83rd Ave	E 83rd Dr	E	9	0	0	0	0	0	0%
	E 83rd Ave	Nueva Vista Dr	W	12	0	0	0	0	0	0%
	E 83rd Dr	E 83rd Ave	W	12	0	0	0	0	0	0%
	E 83rd Dr	E 83rd Pl	E	9	0	0	0	0	0	0%
	E 83rd Pl	84th Ave	E	11	0	0	0	0	0	0%
	E 83rd Pl	E 83rd Dr	W	9	0	0	0	0	0	0%
Gilpin Way	Nueva Vista Dr	E 83rd Ave	E	8	0	0	0	0	0	0%
	E 70th Ave	E 73rd Ave	E	27	0	0	0	0	0	0%
Nueva Vista Dr	E 73rd Ave	E 70th Ave	W	30	2	0	0	0	2	7%
	Cleo St	Coronado Pkwy	S	19	6	0	0	0	6	32%
	Cleo St	Explorador Calle	N	23	12	0	0	0	12	52%
	Coronado Pkwy	Cleo St	N	16	3	0	0	0	3	19%
	Downing Dr	Franklin St	S	21	7	0	0	0	7	33%
	Explorador Calle	Cleo St	S	33	0	0	0	0	0	0%
Rainbow Ave	Franklin St	Downing Dr	N	27	0	0	0	0	0	0%
	McDougal St	McElwain Blvd	W	30	5	0	0	0	5	17%
Sheldon Dr	McElwain Blvd	McDougal St	E	37	0	0	0	0	0	0%
	Corona St	Dawson Dr	S	8	1	0	0	0	1	13%
	Dawson Dr	Corona St	N	8	0	0	0	0	0	0%
	Dawson Dr	De Soto Dr	S	8	0	0	0	0	0	0%
	De Soto Dr	Dawson Dr	N	9	0	0	0	0	0	0%
	De Soto Dr	Edison St	S	7	4	0	0	0	4	57%
	Edison St	De Soto Dr	N	10	0	0	0	0	0	0%
	Edison St	Essex Dr	S	8	0	0	0	0	0	0%
	Essex Dr	Edison St	N	10	0	0	0	0	0	0%
	Essex Dr	Faraday St	S	10	0	0	0	0	0	0%
	Faraday St	Essex Dr	N	10	0	0	0	0	0	0%
	Faraday St	Franklin St	S	12	6	0	0	0	6	50%
Franklin St	Faraday St	N	12	3	0	0	0	3	25%	
Total (Weekend System-wide Peak)				1,234	199	0	0	17	199	16%

Source: Walker Consultants

The next table summarizes a composite of the individual parking peaks for each street block face. Peaks for individual block faces that did not occur during the system-wide peak (9 AM) are highlighted in blue. Block faces with 0% occupancy across all collection periods are highlighted in green.

24 block faces were observed with individual peak occupancies greater than they exhibited during the system-wide peak. For the weekend, the overall composite peak was approximately 5% higher than the system peak.

Table 124: Welby Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)

Street	From	To	Side of Street	Total Supply	Weekend Survey Day Demand (Individual Peak by Segment)						
					Un-restricted	Time Limited	ADA	Illegal	Total Peak Weekend Demand (Across All Times)	Weekend Peak % Occupancy (Across All Times)	Time that Peak Occurred
84th Ave	Downing Dr	Franklin Dr	S	11	6	0	0	0	6	55%	5:00:00 PM
	Franklin Dr	Downing Dr	N	12	2	0	0	0	2	17%	7:00:00 PM
	Franklin Dr	Franklin St	S	9	1	0	0	0	1	11%	9:00:00 AM
	Franklin St	Franklin Dr	N	9	0	0	0	0	0	0%	
City View Dr	Cleo St	Explorador Calle	N	34	7	0	0	0	7	21%	11:00:00 AM
	Dawson Dr	De Soto Dr	S	8	1	0	0	0	1	13%	1:00:00 PM
	De Soto Dr	Dawson Dr	N	0	0	0	0	0	0	0%	
	De Soto Dr	Edison St	S	7	2	0	0	0	2	29%	1:00:00 PM
	Edison St	De Soto Dr	N	4	0	0	0	0	0	0%	
	Edison St	Essex Dr	S	8	2	0	0	0	2	25%	9:00:00 AM
	Essex Dr	Edison St	N	8	6	0	0	0	6	75%	9:00:00 AM
	Explorador Calle	Cleo St	S	27	8	0	0	1	8	30%	9:00:00 AM
Cleo St	City View Dr	Ladean St	E	12	0	0	0	0	0	0%	
	City View Dr	Nueva Vista Dr	W	11	1	0	0	0	1	9%	9:00:00 AM
	High St	Ladean St	W	7	1	0	0	0	1	14%	9:00:00 AM
	High St	Race St	E	5	3	0	0	0	3	60%	9:00:00 AM
	Ladean St	City View Dr	W	15	2	0	0	0	2	13%	9:00:00 AM
	Ladean St	High St	E	6	3	0	0	0	3	50%	5:00:00 PM
	Nueva Vista Dr	City View Dr	E	8	4	0	0	0	4	50%	9:00:00 AM
	Race St	High St	W	4	0	0	0	0	0	0%	
	Race St	Vine St	S	6	2	0	0	0	2	33%	9:00:00 AM
	Vine St	Race St	N	6	2	0	0	0	2	33%	9:00:00 AM
Coronado Pkwy	Downing Dr	Downing St	S	17	8	0	0	0	8	47%	9:00:00 AM
	Downing Dr	Ogden St	N	22	6	0	0	0	6	27%	11:00:00 AM
	Downing St	Lafayette St	S	8	6	0	0	0	6	75%	1:00:00 PM
	Driveway	Downing Dr	S	8	6	0	0	0	6	75%	1:00:00 PM
	Driveway	Driveway	S	0	0	0	0	0	0	0%	9:00:00 AM
	E 81st Pl	Explorador Calle	S	6	0	0	0	0	0	0%	9:00:00 AM
	Emerson St	Clarkson St	N	8	0	0	0	0	0	0%	9:00:00 AM
	Explorador Calle	Downing Dr	N	37	6	0	0	0	6	16%	5:00:00 PM
	Explorador Calle	Nueva Vista Dr	N	16	14	0	0	4	14	88%	11:00:00 AM
	Explorador Calle	York St	S	38	15	0	0	11	15	39%	7:00:00 PM
	Lafayette St	Niver Ct	S	5	0	0	0	0	0	0%	
	Niver Ct	E 81st Pl	S	10	0	0	0	0	0	0%	
	Ogden St	Emerson St	N	9	3	0	0	0	3	33%	9:00:00 AM
	City View Dr	Mid Block	E	27	4	0	0	0	4	15%	11:00:00 AM
Dawson Dr	Essex Dr	Mid Block	N	13	10	0	0	0	10	77%	7:00:00 PM
	Mid Block	City View Dr	W	20	5	0	0	0	5	25%	5:00:00 PM
	Mid Block	Essex Dr	S	15	11	0	0	0	11	73%	7:00:00 PM
	84th Ave	E 83rd Pl	W	6	0	0	0	0	0	0%	
Downing Dr	E 83rd Pl	84th Ave	E	4	0	0	0	0	0	0%	
	E 83rd Pl	Nueva Vista Dr	W	14	4	0	0	0	4	29%	9:00:00 AM
	Nueva Vista Dr	E 83rd Pl	E	21	0	0	0	0	0	0%	

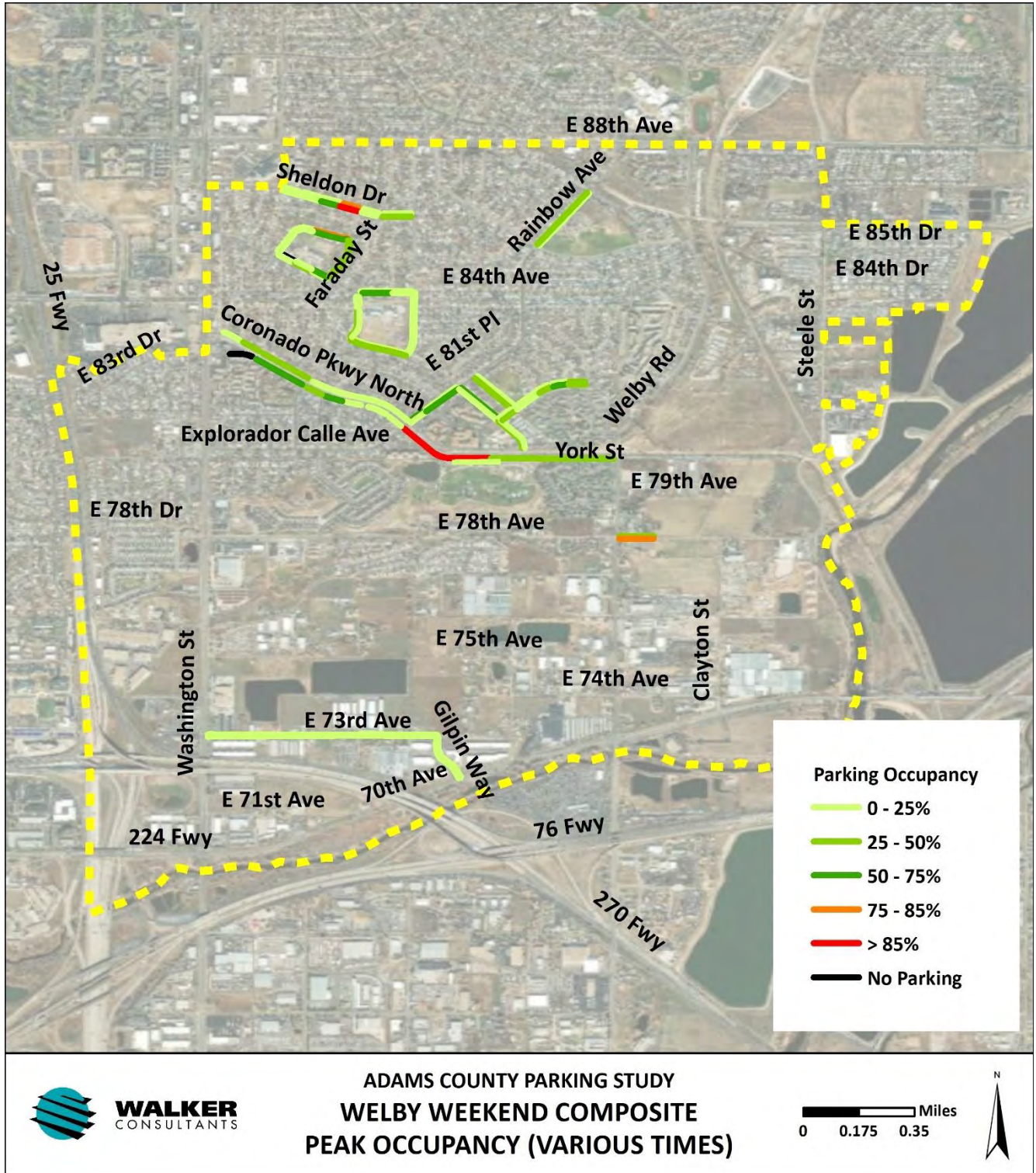


E 73rd Ave	Gilpin Way	Lafayette St	N	35	0	0	0	0	0	0%	
	Lafayette St	Gilpin Way	S	30	0	0	0	0	0	0%	
	Lafayette St	Washington St	N	49	1	0	0	0	1	2%	11:00:00 AM
	Washington St	Lafayette St	S	41	0	0	0	0	0	0%	
E 78th Ave	Mid Block	York St	N	15	5	0	0	0	5	33%	9:00:00 AM
	York St	Mid Block	S	5	4	0	0	1	4	80%	9:00:00 AM
Essex Dr	City View Dr	Dawson Dr	E	19	7	0	0	0	7	37%	11:00:00 AM
	Dawson Dr	City View Dr	W	19	8	0	0	0	8	42%	11:00:00 AM
Explorador Calle	Coronado Pkwy	Solana Dr	E	9	2	0	0	0	2	22%	9:00:00 AM
	Nueva Vista Dr	Quivira Dr	W	8	0	0	0	0	0	0%	
	Quivira Dr	Nueva Vista Dr	E	8	0	0	0	0	0	0%	
	Quivira Dr	Solana Dr	W	7	4	0	0	0	4	67%	11:00:00 AM
	Solana Dr	Coronado Pkwy	W	6	6	0	0	0	6	75%	11:00:00 AM
	Solana Dr	Quivira Dr	E	8	2	0	0	0	2	25%	9:00:00 AM
Franklin St	84th Ave	E 83rd Pl	W	9	0	0	0	0	0	0%	
	E 83rd Ave	E 83rd Dr	E	9	0	0	0	0	0	0%	
	E 83rd Ave	Nueva Vista Dr	W	12	0	0	0	0	0	0%	
	E 83rd Dr	E 83rd Ave	W	12	0	0	0	0	0	0%	
	E 83rd Dr	E 83rd Pl	E	9	0	0	0	0	0	0%	
	E 83rd Pl	84th Ave	E	11	0	0	0	0	0	0%	
	E 83rd Pl	E 83rd Dr	W	9	0	0	0	0	0	0%	
	Nueva Vista Dr	E 83rd Ave	E	8	2	0	0	0	2	25%	5:00:00 PM
	E 70th Ave	E 73rd Ave	E	27	0	0	0	0	0	0%	
Gilpin Way	E 73rd Ave	E 70th Ave	W	30	2	0	0	0	2	7%	9:00:00 AM
	Cleo St	Coronado Pkwy	S	19	6	0	0	0	6	32%	9:00:00 AM
Nueva Vista Dr	Cleo St	Explorador Calle	N	23	12	0	0	0	12	52%	9:00:00 AM
	Coronado Pkwy	Cleo St	N	16	3	0	0	0	3	19%	9:00:00 AM
	Downing Dr	Franklin St	S	21	7	0	0	0	7	33%	9:00:00 AM
	Explorador Calle	Cleo St	S	33	1	0	0	0	1	3%	5:00:00 PM
	Franklin St	Downing Dr	N	27	0	0	0	0	0	0%	
Rainbow Ave	McDougal St	McElwain Blvd	W	30	8	0	0	1	8	27%	7:00:00 PM
	McElwain Blvd	McDougal St	E	37	1	0	0	0	1	3%	11:00:00 AM
Sheldon Dr	Corona St	Dawson Dr	S	8	1	0	0	0	1	13%	9:00:00 AM
	Dawson Dr	Corona St	N	8	0	0	0	0	0	0%	
	Dawson Dr	De Soto Dr	S	8	0	0	0	0	0	0%	
	De Soto Dr	Dawson Dr	N	9	1	0	0	0	1	11%	11:00:00 AM
	De Soto Dr	Edison St	S	7	4	0	0	0	4	57%	9:00:00 AM
	Edison St	De Soto Dr	N	10	1	0	0	0	1	10%	5:00:00 PM
	Edison St	Essex Dr	S	8	7	0	0	0	7	88%	5:00:00 PM
	Essex Dr	Edison St	N	10	8	0	0	0	8	80%	5:00:00 PM
	Essex Dr	Faraday St	S	10	0	0	0	0	0	0%	
	Faraday St	Essex Dr	N	10	0	0	0	0	0	0%	
Faraday St	Franklin St	S	12	6	0	0	0	6	50%	9:00:00 AM	
Franklin St	Faraday St	N	12	3	0	0	0	3	25%	9:00:00 AM	
Total (Weekend Composite)				1,234	263	0	0	18	263	21%	9:00 AM

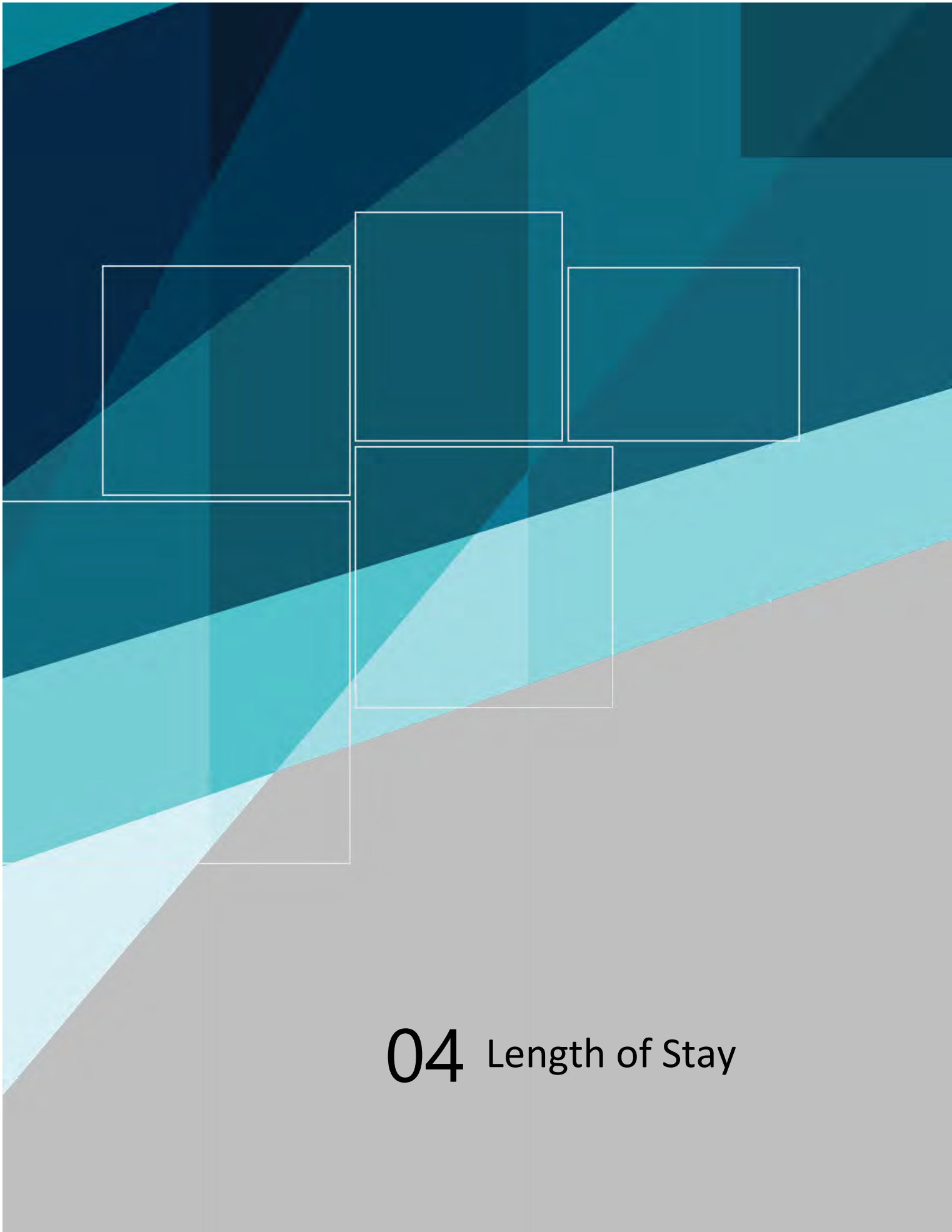
Source: Walker Consultants

The following heat map is a composite of peak parking demand per block face across all collection periods.

Figure 82: Welby Heat Map of Localized Weekend Peak Occupancy by Block Face (Varying Times of Day)



Source: Walker Consultants



04 Length of Stay

SECTION 4 – LENGTH OF STAY

ALL NEIGHBORHOODS

Across all neighborhoods, the average percentage of total vehicles that were parked all day was 20% on weekdays and 25% on weekends, as shown in the following table.

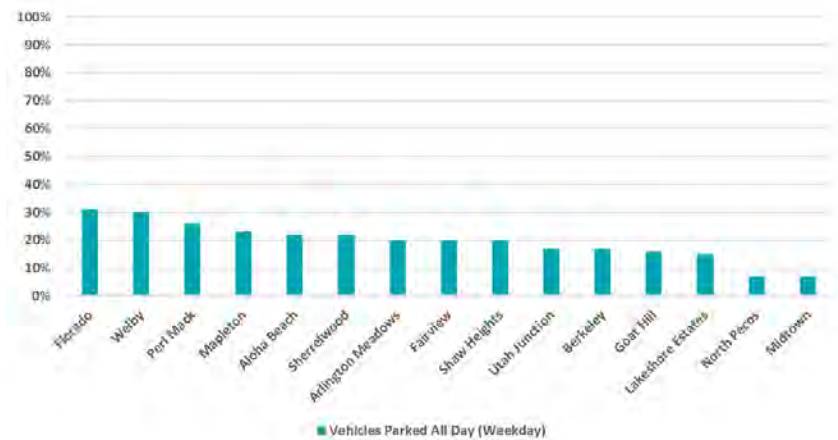
Table 125: Length of Stay Across All Neighborhoods

Neighborhood	Vehicles Parked All Day (Weekday)	Vehicles Parked All Day (Weekend)	Percent Difference Between Weekday and Weekend
Mapleton	23%	12%	-11%
Florado	31%	23%	-8%
North Pecos	7%	0%	-7%
Aloha Beach	22%	18%	-4%
Welby	30%	28%	-2%
Lakeshore Estates	15%	18%	3%
Arlington Meadows	20%	27%	7%
Utah Junction	17%	25%	8%
Peri Mack	26%	35%	9%
Midtown	7%	17%	10%
Fairview	20%	31%	11%
Berkeley	17%	31%	14%
Goat Hill	16%	30%	14%
Sherrelwood	22%	39%	17%
Shaw Heights	20%	45%	25%
Average	20%	25%	6%

Source: Walker Consultants

The percentage of total vehicles parked all day on weekdays ranged from 31% in Florado to 7% in Midtown, as shown in the following figure.

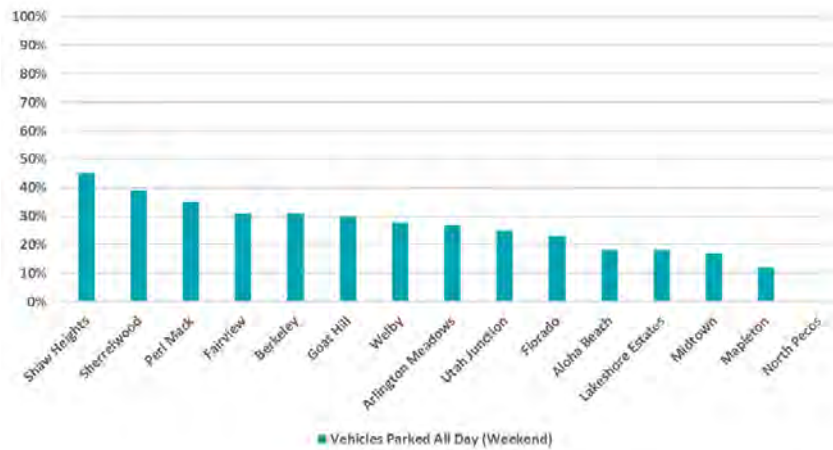
Figure 83: Percentage of Vehicles Parked All Day by Neighborhood on Weekdays



Source: Walker Consultants

The percentage of total vehicles parked all day on weekends ranged from 45% in Shaw Heights to 0% in North Pecos, shown in the following figure.

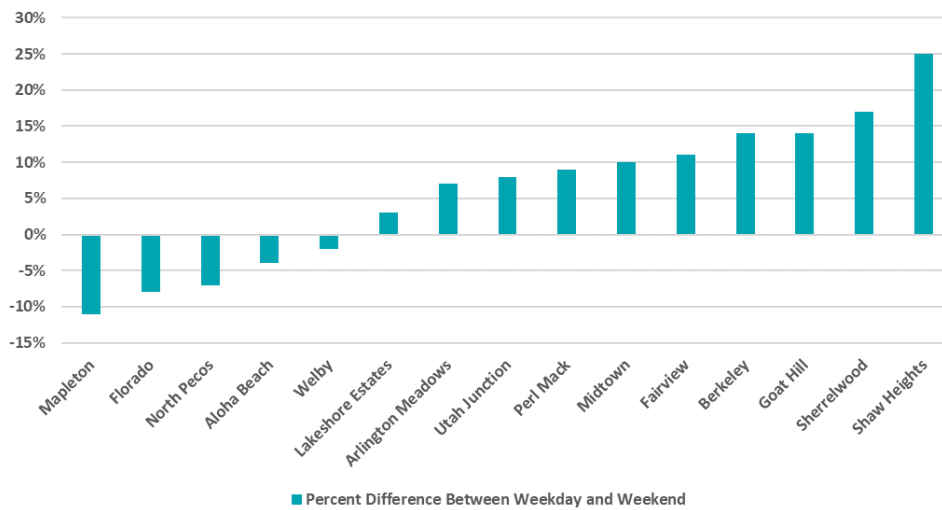
Figure 84: Percentage of Vehicles Parked All Day by Neighborhood on Weekends



Source: Walker Consultants

The percent difference between the percentage of vehicles parked all day on weekdays versus weekends ranged from - 11% in Mapleton (fewer all day vehicles on weekends) to + 25% in Shaw Heights (more all day vehicles on weekends). This data is shown in the following figure.

Figure 85: Difference between Weekdays and Weekends for All Day Parkers by Neighborhood



Source: Walker Consultants

ALOHA BEACH

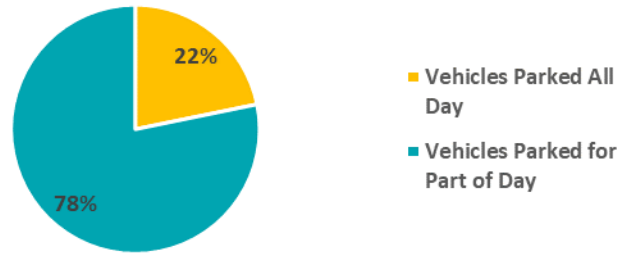
WEEKDAY

Table 126: Aloha Beach Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	17	22%	17	22%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	61	78%	61	78%	0	0%	0	0%	1	1%
Total Unique Vehicles Observed	78	100%	78	100%	0	100%	0	100%	1	100%

Source: Walker Consultants

Figure 86: Aloha Beach Total Weekday Length of Stay



Source: Walker Consultants

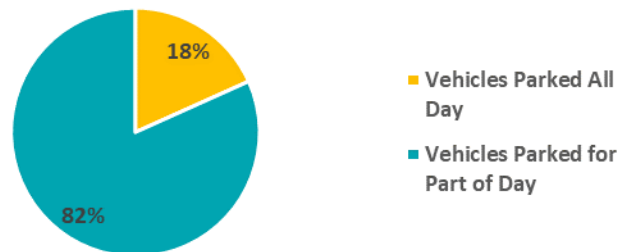
WEEKEND

Table 127: Aloha Beach Weekend Length of Stay by User Group

Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	15	18%	15	18%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	67	82%	67	82%	0	0%	0	0%	0	0%
Total Unique Vehicles Observed	82	100%	82	100%	0	100%	0	100%	0	100%

Source: Walker Consultants

Figure 87: Aloha Beach Total Weekend Length of Stay



Source: Walker Consultants

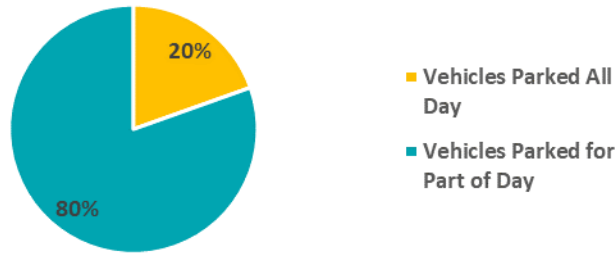
ARLINGTON MEADOWS
WEEKDAY

Table 128: Arlington Meadows Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	9	20%	9	20%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	37	80%	37	80%	0	0%	0	0%	1	2%
Total Unique Vehicles Observed	46	100%	46	100%	0	100%	0	100%	1	100%

Source: Walker Consultants

Figure 88: Arlington Meadows Total Weekday Length of Stay



Source: Walker Consultants

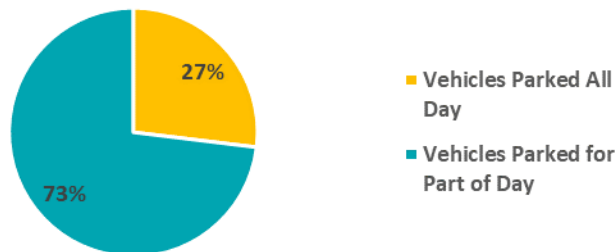
WEEKEND

Table 129: Arlington Meadows Weekend Length of Stay by User Group

Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	14	27%	14	27%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	38	73%	38	73%	0	0%	0	0%	1	2%
Total Unique Vehicles Observed	52	100%	52	100%	0	100%	0	100%	1	100%

Source: Walker Consultants

Figure 89: Arlington Meadows Total Weekend Length of Stay



Source: Walker Consultants

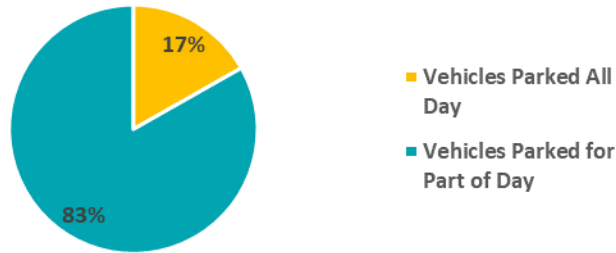
BERKELEY
WEEKDAY

Table 130: Berkeley Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	63	17%	63	17%	0	0%	0	0%	3	1%
Vehicles Parked for Part of Day	312	83%	312	83%	0	0%	0	0%	12	3%
Total Unique Vehicles Observed	375	100%	375	100%	0	100%	0	100%	15	100%

Source: Walker Consultants

Figure 90: Berkeley Total Weekday Length of Stay



Source: Walker Consultants

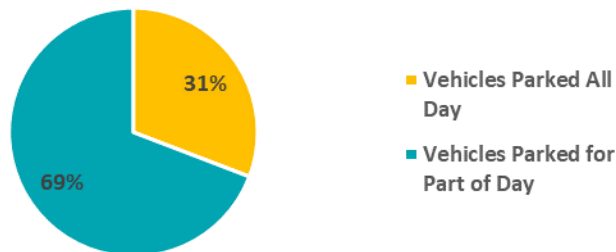
WEEKEND

Table 131: Berkeley Weekend Length of Stay by User Group

Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	61	31%	61	31%	0	0%	0	0%	2	1%
Vehicles Parked for Part of Day	137	69%	137	69%	0	0%	0	0%	2	1%
Total Unique Vehicles Observed	198	100%	198	100%	0	100%	0	100%	4	100%

Source: Walker Consultants

Figure 91: Berkeley Total Weekend Length of Stay



Source: Walker Consultants

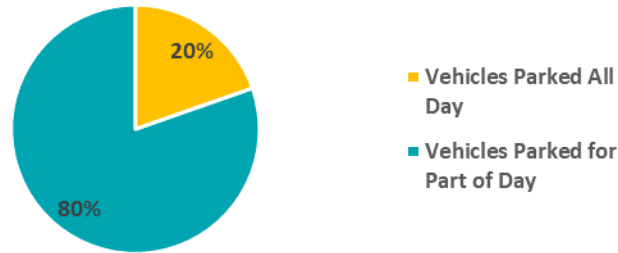
FAIRVIEW
WEEKDAY

Table 132: Fairview Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	23	20%	23	20%	0	0%	0	0%	1	1%
Vehicles Parked for Part of Day	94	80%	92	79%	1	1%	1	1%	1	1%
Total Unique Vehicles Observed	117	100%	115	100%	1	100%	1	100%	2	100%

Source: Walker Consultants

Figure 92: Fairview Total Weekday Length of Stay



Source: Walker Consultants

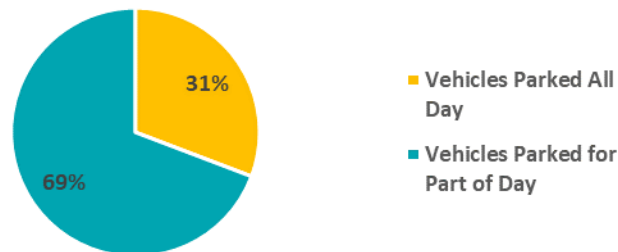
WEEKEND

Table 133: Fairview Weekend Length of Stay by User Group

Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	40	31%	40	31%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	90	69%	90	69%	0	0%	0	0%	0	0%
Total Unique Vehicles Observed	130	100%	130	100%	0	100%	0	100%	0	100%

Source: Walker Consultants

Figure 93: Fairview Total Weekend Length of Stay



Source: Walker Consultants

FLORADO

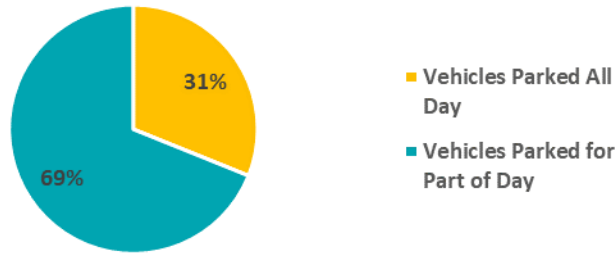
WEEKDAY

Table 134: Florado Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	37	31%	35	29%	0	0%	2	2%	0	0%
Vehicles Parked for Part of Day	82	69%	81	68%	0	0%	1	1%	3	3%
Total Unique Vehicles Observed	119	100%	116	100%	0	100%	3	100%	3	100%

Source: Walker Consultants

Figure 94: Florado Total Weekday Length of Stay



Source: Walker Consultants

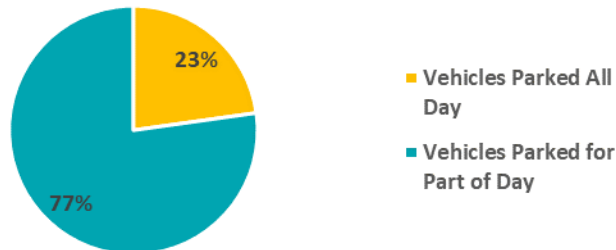
WEEKEND

Table 135: Florado Weekend Length of Stay by User Group

Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	41	23%	40	22%	0	0%	1	1%	0	0%
Vehicles Parked for Part of Day	139	77%	137	76%	0	0%	2	1%	3	2%
Total Unique Vehicles Observed	180	100%	177	100%	0	100%	3	100%	3	100%

Source: Walker Consultants

Figure 95: Florado Total Weekend Length of Stay



Source: Walker Consultants

GOAT HILL

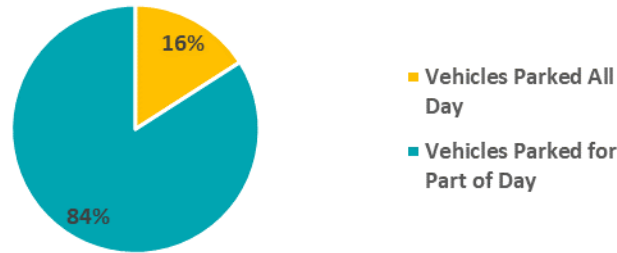
WEEKDAY

Table 136: Goat Hill Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	11	16%	11	16%	0	0%	0	0%	3	4%
Vehicles Parked for Part of Day	58	84%	58	84%	0	0%	0	0%	6	9%
Total Unique Vehicles Observed	69	100%	69	100%	0	100%	0	100%	9	100%

Source: Walker Consultants

Figure 96: Goat Hill Total Weekday Length of Stay



Source: Walker Consultants

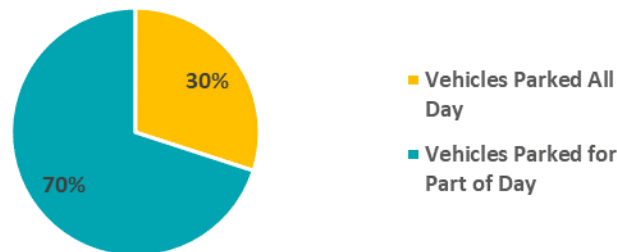
WEEKEND

Table 137: Goat Hill Weekend Length of Stay by User Group

Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	27	30%	27	30%	0	0%	0	0%	3	3%
Vehicles Parked for Part of Day	63	70%	63	70%	0	0%	0	0%	11	12%
Total Unique Vehicles Observed	90	100%	90	100%	0	100%	0	100%	14	100%

Source: Walker Consultants

Figure 97: Goat Hill Total Weekend Length of Stay



Source: Walker Consultants

LAKESHORE ESTATES

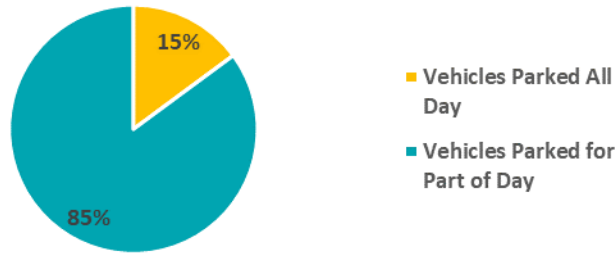
WEEKDAY

Table 138: Lakeshore Estates Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	7	15%	7	15%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	40	85%	40	85%	0	0%	0	0%	0	0%
Total Unique Vehicles Observed	47	100%	47	100%	0	100%	0	100%	0	100%

Source: Walker Consultants

Figure 98: Lakeshore Estates Total Weekday Length of Stay



Source: Walker Consultants

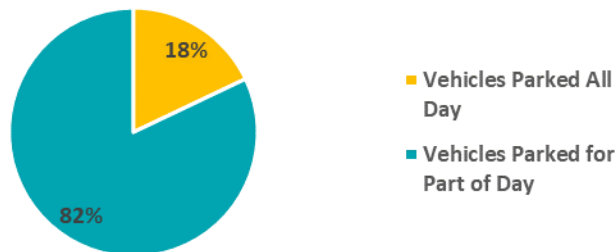
WEEKEND

Table 139: Lakeshore Estates Weekend Length of Stay by User Group

Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	7	18%	7	18%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	32	82%	32	82%	0	0%	0	0%	1	3%
Total Unique Vehicles Observed	39	100%	39	100%	0	100%	0	100%	1	100%

Source: Walker Consultants

Figure 99: Lakeshore Estates Total Weekend Length of Stay



Source: Walker Consultants

MAPLETON

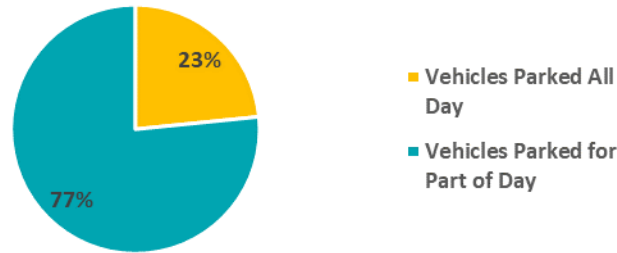
WEEKDAY

Table 140: Mapleton Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	22	23%	22	23%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	72	77%	72	77%	0	0%	0	0%	0	0%
Total Unique Vehicles Observed	94	100%	94	100%	0	100%	0	100%	0	100%

Source: Walker Consultants

Figure 100: Mapleton Total Weekday Length of Stay



Source: Walker Consultants

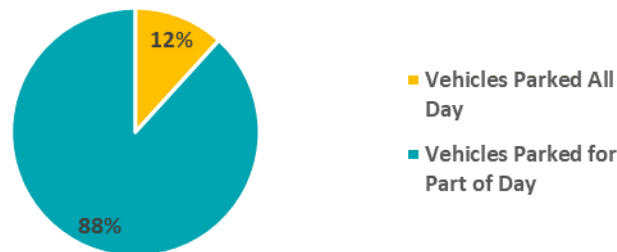
WEEKEND

Table 141: Mapleton Weekend Length of Stay by User Group

Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	8	12%	8	12%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	60	88%	60	88%	0	0%	0	0%	0	0%
Total Unique Vehicles Observed	68	100%	68	100%	0	100%	0	100%	0	100%

Source: Walker Consultants

Figure 101: Mapleton Total Weekend Length of Stay



Source: Walker Consultants

MIDTOWN

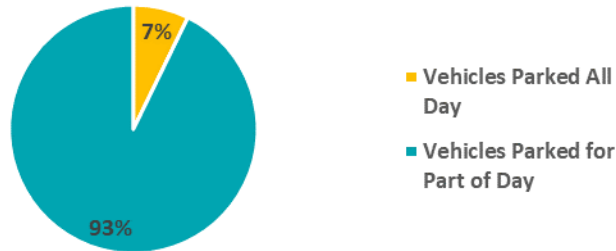
WEEKDAY

Table 142: Midtown Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	5	7%	5	7%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	64	93%	62	90%	2	3%	0	0%	0	0%
Total Unique Vehicles Observed	69	100%	67	100%	2	100%	0	100%	0	100%

Source: Walker Consultants

Figure 102: Midtown Total Weekday Length of Stay



Source: Walker Consultants

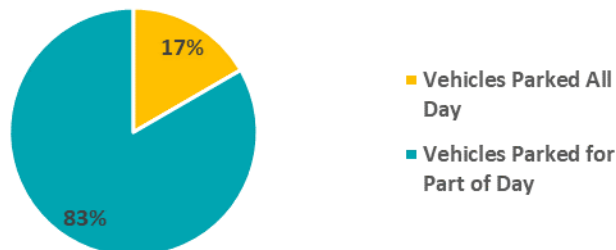
WEEKEND

Table 143: Midtown Weekend Length of Stay by User Group

Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	15	17%	15	17%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	75	83%	75	83%	0	0%	0	0%	0	0%
Total Unique Vehicles Observed	90	100%	90	100%	0	100%	0	100%	0	100%

Source: Walker Consultants

Figure 103: Midtown Total Weekend Length of Stay



Source: Walker Consultants

NORTH PECOS

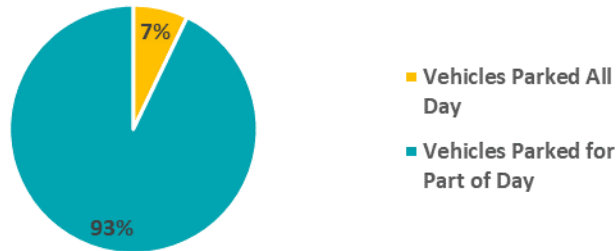
WEEKDAY

Table 144: North Pecos Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	1	7%	1	7%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	13	93%	13	93%	0	0%	0	0%	0	0%
Total Unique Vehicles Observed	14	100%	14	100%	0	100%	0	100%	0	100%

Source: Walker Consultants

Figure 104: North Pecos Total Weekday Length of Stay



Source: Walker Consultants

WEEKEND

Table 145: North Pecos Weekend Length of Stay by User Group

Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	0	0%	0	0%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	2	100%	2	100%	0	0%	0	0%	0	0%
Total Unique Vehicles Observed	2	100%	2	100%	0	100%	0	100%	0	100%

Source: Walker Consultants

Figure 105: North Pecos Total Weekend Length of Stay



Source: Walker Consultants

PERL MACK

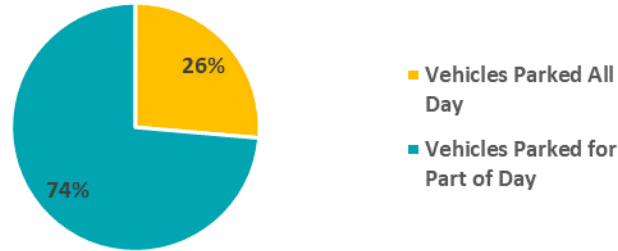
WEEKDAY

Table 146: Perl Mack Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	216	26%	216	26%	0	0%	0	0%	3	0%
Vehicles Parked for Part of Day	603	74%	602	74%	0	0%	1	0%	8	1%
Total Unique Vehicles Observed	819	100%	818	100%	0	100%	1	100%	11	100%

Source: Walker Consultants

Figure 106: Perl Mack Total Weekday Length of Stay



Source: Walker Consultants

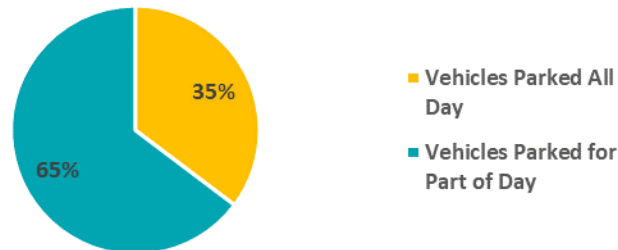
WEEKEND

Table 147: Perl Mack Weekend Length of Stay by User Group

Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	303	35%	303	35%	0	0%	0	0%	5	1%
Vehicles Parked for Part of Day	556	65%	556	65%	0	0%	0	0%	28	3%
Total Unique Vehicles Observed	859	100%	859	100%	0	100%	0	100%	33	100%

Source: Walker Consultants

Figure 107: Perl Mack Total Weekend Length of Stay



Source: Walker Consultants

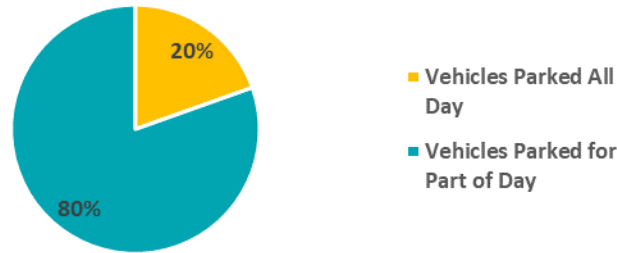
SHAW HEIGHTS
WEEKDAY

Table 148: Shaw Heights Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	36	20%	36	20%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	148	80%	148	80%	0	0%	0	0%	1	1%
Total Unique Vehicles Observed	184	100%	184	100%	0	100%	0	100%	1	100%

Source: Walker Consultants

Figure 108: Shaw Heights Total Weekday Length of Stay



Source: Walker Consultants

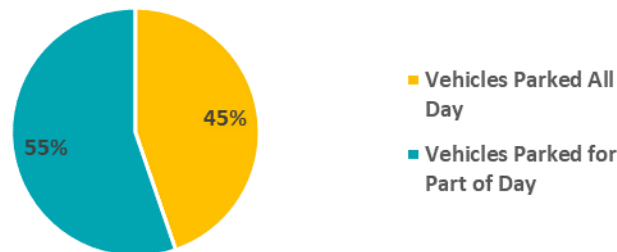
WEEKEND

Table 149: Shaw Heights Weekend Length of Stay by User Group

Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	68	45%	68	45%	0	0%	0	0%	1	1%
Vehicles Parked for Part of Day	84	55%	84	55%	0	0%	0	0%	3	2%
Total Unique Vehicles Observed	152	100%	152	100%	0	100%	0	100%	4	100%

Source: Walker Consultants

Figure 109: Shaw Heights Total Weekend Length of Stay



Source: Walker Consultants

SHERRELWOOD

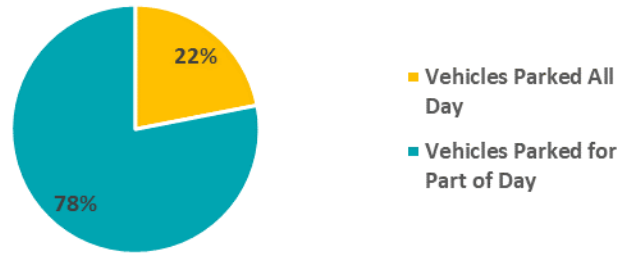
WEEKDAY

Table 150: Sherrelwood Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	71	22%	71	22%	0	0%	0	0%	1	0%
Vehicles Parked for Part of Day	253	78%	253	78%	0	0%	0	0%	3	1%
Total Unique Vehicles Observed	324	100%	324	100%	0	100%	0	100%	4	100%

Source: Walker Consultants

Figure 110: Sherrelwood Total Weekday Length of Stay



Source: Walker Consultants

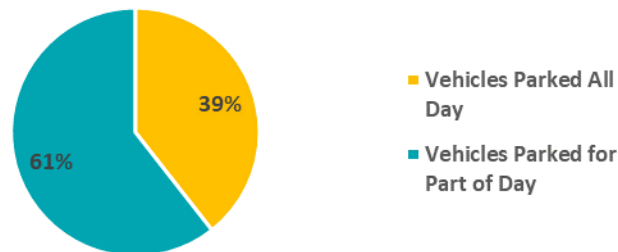
WEEKEND

Table 151: Sherrelwood Weekend Length of Stay by User Group

Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	121	39%	121	39%	0	0%	0	0%	2	1%
Vehicles Parked for Part of Day	186	61%	186	61%	0	0%	0	0%	13	4%
Total Unique Vehicles Observed	307	100%	307	100%	0	100%	0	100%	15	100%

Source: Walker Consultants

Figure 111: Sherrelwood Total Weekend Length of Stay



Source: Walker Consultants

UTAH JUNCTION

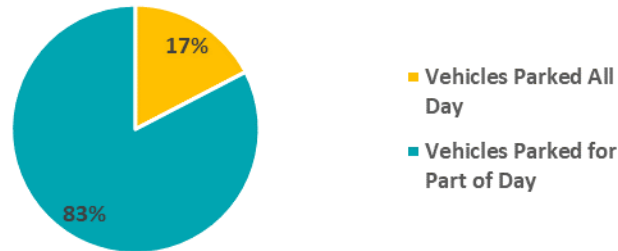
WEEKDAY

Table 152: Utah Junction Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	12	17%	12	17%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	57	83%	57	83%	0	0%	0	0%	2	3%
Total Unique Vehicles Observed	69	100%	69	100%	0	100%	0	100%	2	100%

Source: Walker Consultants

Figure 112: Utah Junction Total Weekday Length of Stay



Source: Walker Consultants

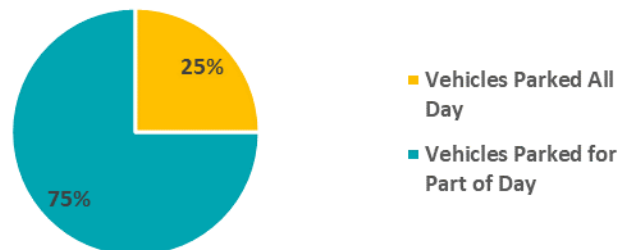
WEEKEND

Table 153: Utah Junction Weekend Length of Stay by User Group

Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	19	25%	19	25%	0	0%	0	0%	0	0%
Vehicles Parked for Part of Day	57	75%	57	75%	0	0%	0	0%	4	5%
Total Unique Vehicles Observed	76	100%	76	100%	0	100%	0	100%	4	100%

Source: Walker Consultants

Figure 113: Utah Junction Total Weekend Length of Stay



Source: Walker Consultants

WELBY

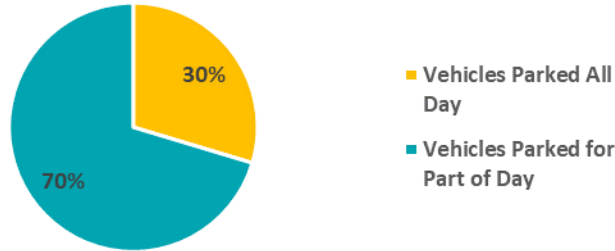
WEEKDAY

Table 154: Welby Weekday Length of Stay by User Group

Weekday Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	107	30%	107	30%	0	0%	0	0%	1	0%
Vehicles Parked for Part of Day	254	70%	254	70%	0	0%	0	0%	23	6%
Total Unique Vehicles Observed	361	100%	361	100%	0	100%	0	100%	24	100%

Source: Walker Consultants

Figure 114: Welby Total Weekday Length of Stay



Source: Walker Consultants

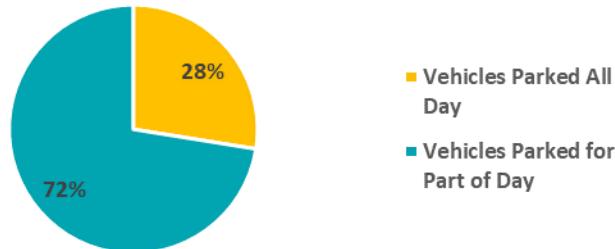
WEEKEND

Table 155: Welby Weekend Length of Stay by User Group

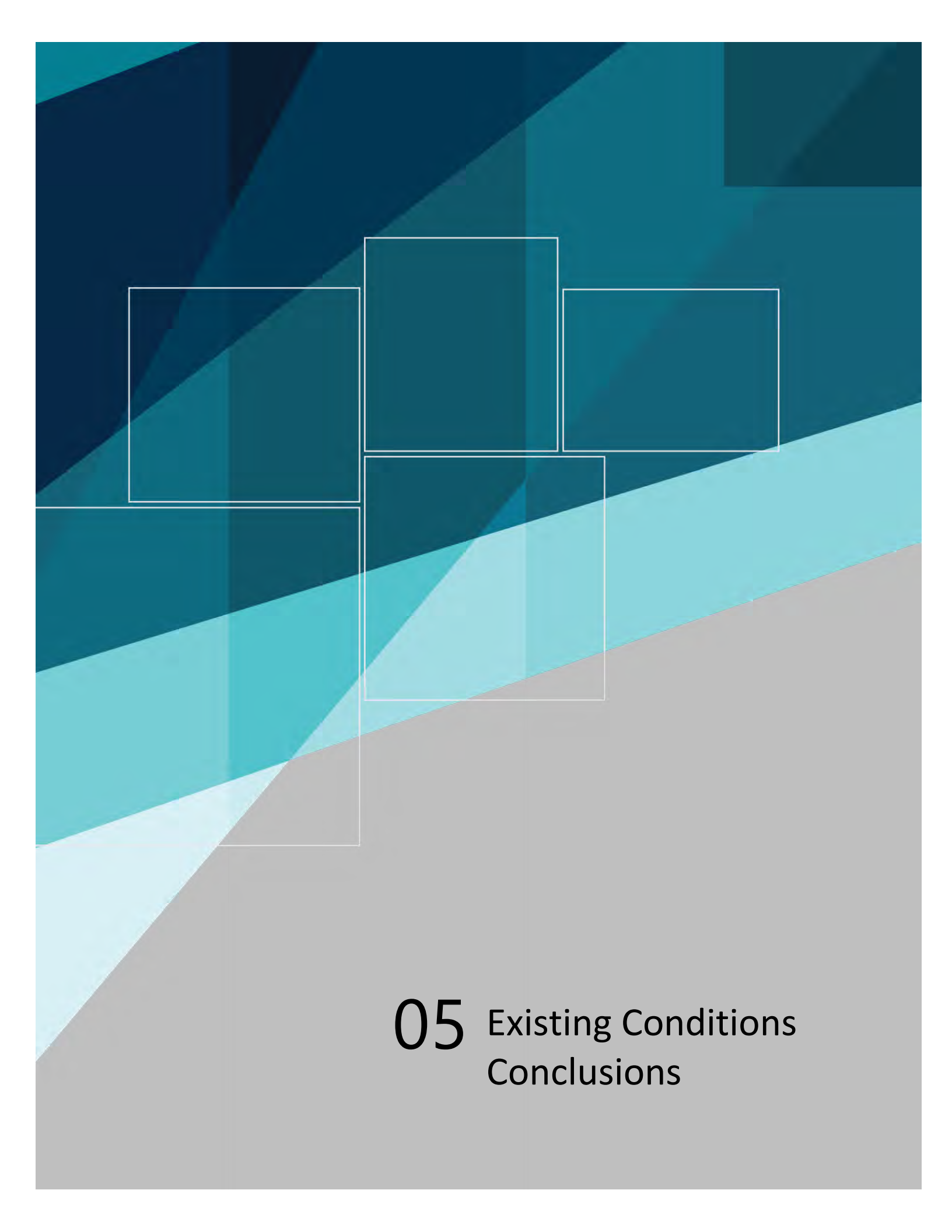
Weekend Length of Stay	All Vehicles		Unrestricted		Time Limited		ADA		Illegal	
	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage	Total	Percentage
Vehicles Parked All Day	98	28%	98	28%	0	0%	0	0%	9	3%
Vehicles Parked for Part of Day	258	72%	258	72%	0	0%	0	0%	17	5%
Total Unique Vehicles Observed	356	100%	356	100%	0	100%	0	100%	26	100%

Source: Walker Consultants

Figure 115: Welby Total Weekend Length of Stay



Source: Walker Consultants



05 Existing Conditions
Conclusions

SECTION 5 – EXISTING CONDITIONS SUMMARY

ALL NEIGHBORHOODS

Parking occupancy across all 15 neighborhoods reached an average of 25% peak occupancy during weekdays and 22% peak occupancy during weekends.

1. **Parking restrictions**

Time-limited parking was found in one neighborhood, Fairview. On-street ADA parking was found in Florado and Midtown. Off-street ADA parking was found in Fairview, Goat Hill, Lakeshore Estates, Perl Mack, and Welby. Note that this only represents restricted parking found within the corridors or lots identified for study in each neighborhood.

2. **Long-term and illegal parking**

Illegal parking activity was observed in seven neighborhoods. In order of prevalence, these are: Welby, Perl Mack, Berkeley, Goat Hill, Sherrelwood, Fairview, and Florado. Illegal parking was mostly in the form of parking along corridors where no parking is allowed at any time.

Across all neighborhoods, the average percentage of vehicles parked all day was 20% on weekdays and 25% on weekends. All-day parking during weekdays was highest, in terms of percent of total parkers, in Florado, Welby, Perl Mack, Mapleton, and Aloha Beach. All-day parking during weekdays was highest, in terms of percent of total parkers, in Shaw Heights, Sherrelwood, Perl Mack, Fairview, and Berkeley. Mapleton experienced the largest negative difference between weekday and weekend all-day parkers, and Shaw Heights experienced the largest positive difference.

3. **Spillover parking**

Though there were many individual block faces that experienced high parking demand during peak times in most neighborhoods, most of that high demand did not constitute spillover parking from nodes of parking demand other than immediate adjacent land uses. However, based on comparing composite peak occupancy data to system-wide peaks for each neighborhood, and analyzing surrounding land uses, spillover parking demand was likely occurring in Berkeley, Perl Mack, Welby, and potentially Florado.

The problem was most prevalent in Perl Mack, with multiple block faces along 76th Avenue, Samuel Drive, Dakin Street, Del Norte Street, and Bronco Road affected with high demand at peak hours on both the weekday and weekend. Prevalent weekday spillover parking in Berkeley was also observed, with the block faces closest to the Regis University campus (segments of King Court, 53rd Avenue, Irving Street, Meade Street, and Lowell Boulevard) affected. In Welby, block faces along Coronado Parkway near the intersection with Explorador Calle, Downing Drive, and Sheldon Drive were affected.

Spillover parking demand likely related to church activity may have occurred along block faces near church front entrances during Sunday mornings and afternoons in Welby and Florado, though this parking activity may be related to the relative convenience of on-street parking in front of those churches, as opposed to actual spillover demand from mostly full church parking lots.

ALOHA BEACH

Parking occupancy in Aloha Beach peaks at 34% during the evening on the weekend.

1. **Parking restrictions**

There was no time-limited, ADA, or other restricted parking found within the areas identified for study in Aloha Beach.

2. **Long-term and illegal parking**

No illegal parking activity was observed along Lowell Blvd, and no pattern of frequent or habitual illegal parking activity on any other corridor identified for study was observed. Aloha Beach's percentage of all-day, on-street parkers suggests that long-term parking or personal or derelict vehicles are not an issue within the corridors identified for study. 4% fewer vehicles were parked all day on the weekend versus the weekday.

3. **Spillover parking**

According to the representative sample studied, there is ample parking available in the Aloha Beach neighborhood. Several block faces of Julian Street between 63rd Avenue and 64th Avenue exceeded 85% occupancy during the weekday. Based on the time of day that demands peak in this area, this parking demand is likely generated by residents who live along the street.

A difference of 1% between the system-wide and composite weekend peak suggests that parking demands are relatively consistent throughout the day.

4. **Other**

It should be noted that the majority of homes in Aloha Beach front 62nd Avenue, which is a public street that is access controlled. 62nd Avenue was not studied for this reason, and it is thus unknown whether these conclusions could be reasonably extrapolated to 62nd Avenue.

ARLINGTON MEADOWS

Parking occupancy in Arlington Meadows peaked during the evening at 34%, on both the weekday and weekend.

1. **Parking restrictions**

There was no time-limited, ADA, or other restricted parking found within the corridors or areas identified for study in Arlington Meadows.

2. **Long-term and illegal parking**

Only one illegal parker was observed during the weekend afternoon/evening along Tennyson Street. No pattern of frequent or habitual illegal parking activity on any other corridor identified for study was observed. Arlington Meadows's percentage of all-day, on-street parkers suggests that long-term parking and derelict vehicles are not an issue within the corridors identified for study. 7% more vehicles were parked all day on the weekend versus the weekday.

3. **Spillover parking and parking demand hot spots**

According to the representative sample chosen for the study, there is ample parking available in the Arlington Meadows neighborhood. Parking along Stuart Street and Two block faces of 62nd Place exhibited high parking demand on weekends and weekdays, but this is highly localized and likely being generated by residents who live in the immediate area.

A difference of 7% between the system-wide and composite weekend peak suggests that there some localized demand "hot spots" that occur at times outside the system-wide peak. There is a nearby pedestrian/bike path easement connecting to Tennyson Knolls Elementary School, however, spillover demand from the school, would have been anticipated along Tennyson during weekday morning and afternoon collections. Therefore, demand peaks are likely attributable to single family homes adjacent to studied corridors.

BERKELEY

Parking occupancy in Berkeley peaked during the morning at 33% on the weekday.

1. Parking restrictions

There was no time-limited, ADA, or other restricted parking found within the corridors or areas identified for study in Berkeley.

2. Long-term and illegal parking

Nine illegal parkers were observed along Lowell Boulevard, Meade Street, Newton Street, Osceola Street, 52nd Avenue, and 53rd Avenue during the weekday system peak (9 AM). Illegal parking activity was evenly dispersed, and not concentrated on any particular street segment or block face. Frequent or habitual illegal parking activity was observed to occur throughout the day along those corridors, particularly in the morning and afternoon.

The percentage of all-day, on-street parkers observed was far below average for all neighborhoods on weekdays and above the average on weekends, suggesting that long-term parking and derelict vehicles are not an issue within the corridors identified for study. However, 14% more vehicles were parked all day on the weekend versus the weekday. This suggests that a high number of weekday parkers are short-term, non-residents.

3. Spillover parking and parking demand hot spots

According to the representative sample chosen for this study, there is ample parking available in the Berkeley neighborhood. However, on-street parking supply on corridors immediately surrounding Regis University experience high demand loads.

A difference of 8% between the system-wide and composite weekday peak occupancies suggests that there several localized demand “hot spots” that occur at times outside the system-wide peak. In Berkeley, most block faces studied experienced their demand peaks in the evening, except those closest to the Regis University campus along 52nd Avenue, King Street, 53rd Avenue, Irving Street, Meade Street, and Lowell Boulevard. This demand pattern is consistent spillover likely attributable to commuter students and faculty from the University, parking along those street segments during the morning and afternoon.

Other corridors with high peak occupancy, such as 54th Avenue, experienced their peaks during the evening. Peak occupancy for those corridors is not likely resident driven and not due to spillover parking from non-residents.

FAIRVIEW

On-street parking occupancy in Fairview peaked at 17% during the afternoon on the weekend. Off-street parking occupancy peaked at 2% during the weekend in the Waddell Street Lot.

1. Parking restrictions

There are 11 time-limited parking spaces (15-minute parking) observed on the east side of Elmwood Lane between Cottonwood Drive and Fairview Avenue. These spaces are adjacent to Fairview Elementary School, and likely intended for student pick-up/drop-off. No other restricted or ADA on-street parking was noted. There were 4 ADA spaces located in the Waddell Park Lot.

2. Long-term and illegal parking

Though only one illegally parked vehicle was observed during the system peak, there were between one and two illegally parked vehicles at all observation times along Cottonwood Drive between Valley View Drive and Federal Boulevard during the weekday. This may constitute a pattern of frequent or habitual illegal parking along that corridor during the weekday.

The percentage of all-day, on-street parkers observed was consistent with other neighborhoods on weekdays and above the average on weekends. This suggests that long-term parking and derelict vehicles are not an issue within the corridors identified for study. However, 11% more vehicles were parked all day on the weekend versus the weekday, suggesting that a higher number of weekday parkers were short-term and non-residents. This activity is likely attributable to the park, where park goers may prefer to park on-street instead of in the surface lot.

No parkers along the time-limited block face of Elmwood were observed to be all-day parkers, and no parkers were observed for more than one observation period along this block face.

3. Spillover parking and parking demand hot spots

According to the representative sample chosen for the study, there is ample parking available in the Fairview neighborhood. No corridor was observed to experience greater than 50% peak occupancy at any time throughout the weekday or weekend collections.

Spillover parking did not appear to be an issue, there or anywhere else in Fairview. Perceptions of spillover parking may be attributable to increased vehicle traffic during peak pick up and drop off times.

FLORADO

On-street parking occupancy in Florado peaked at 40% during the morning on the weekend and exhibited the second-highest overall weekend peak occupancy observed in any neighborhood.

1. **Parking restrictions**

There are six ADA-restricted on-street spaces along Valleyview Drive and Zuni Street adjacent to the two churches in the neighborhood. No other restricted parking was observed.

2. **Long-term and illegal parking**

One illegal parker was observed on each of the weekday and weekend. These observed violations occurred along two different block faces at different times in unrestricted spaces. There was no pattern of frequent or habitual illegal parking activity observed in the Florado neighborhood.

The percentage of all-day, on-street parkers observed was the highest observed among all neighborhoods on weekdays. Based on this, long-term storage of personal or derelict vehicles in the public right-of-way may be an issue within the corridors identified for study. However, 8% fewer vehicles were parked all day on the weekend versus the weekday, suggesting that a high number of weekend parkers were short-term and non-residents, compared to the weekday. This is likely attributable to parking activity generated by the two churches located in the neighborhood.

3. **Spillover parking and parking demand hot spots**

According to the representative sample chosen for the study, there is ample parking available in the Florado neighborhood. Florado experienced a difference of 8% between the weekend system peak occupancy and the weekend composite peak occupancy; only Midtown experienced a greater difference. Given that the weekday system-wide and composite peak were both 39% occupied, it can be determined, along with length of stay data, that significant parking demand is generated by short-term activity occurring on weekends. Streets immediately adjacent to the two churches in the neighborhood experienced their individual peaks in the morning or afternoon and were noticeable “hot spots” of demand activity.

Despite several individual hot spots of peak demand, the only corridor segment to exceed 60% occupancy during the study was Elmwood Lane between Umatilla Street and Florado Street. This segment of Elmwood Lane is directly in front of the LDS Church and reached 90% occupancy at 11 AM on Sunday. Spillover parking is this likely an issue but highly localized to this street segment during this time.

GOAT HILL

On-street parking occupancy in the Goat Hill neighborhood peaked at 13% during morning on the weekend.

1. **Parking restrictions**

There is no time-limited, ADA, or other restricted parking found within the corridors identified for study in Goat Hill. There were two ADA spaces located in the Baker Community Center off-street lot.

2. **Long-term and illegal parking**

There were nine illegal parkers observed in Goat Hill during the weekend peak. A pattern of frequent or habitual illegal parking activity was observed on both the weekday and weekend along 63rd Avenue between Federal Boulevard and Decatur Street with eight of nine illegally parked vehicles recorded along that corridor during the weekend collection.

The percentage of all-day, on-street parkers observed was slightly lower than the average for other neighborhoods on weekdays, but slightly above the average on weekends, suggesting that long-term parking/derelict vehicles are not an issue within the corridors identified for study. 14% more vehicles were parked all day on the weekend versus the weekday. This length of stay pattern for the Goat Hill neighborhood is likely attributable to the mix of residential and non-residential uses in the neighborhood, with more short-term parking activity occurring on weekdays due to employees and business patrons in parts of the neighborhood, particularly along Federal Boulevard and south of 64th Avenue.

3. **Spillover parking and parking demand hot spots**

According to the representative sample chosen for the study, there is ample parking available in the Goat Hill neighborhood with the 3rd lowest weekend peak occupancy of the neighborhoods studied. There was a 4% difference between the system-wide peak and the composite peak for both the weekday and weekend. This suggests that there were not any significant localized “hot spots” of demand in the neighborhood that occurred outside the system-wide peak.

No block face during either the weekday or weekend collections exceeded 75% occupancy.

LAKESHORE ESTATES

On-street parking occupancy in Lakeshore Estates peaked at 7% during the weekday, the lowest peak observed for any neighborhood. Off-street parking demand in the Hidden Lake Park Lot peaked at 4% at 1 PM on the weekday.

1. **Parking restrictions**

There is no time-limited, ADA, or other restricted parking found within the corridors identified for study in Lakeshore Estates. There are five ADA spaces located in the Hidden Lake Park Lot.

2. **Long-term and illegal parking**

There was one illegally parked vehicle observed on the weekend. This occurred along Raleigh Street between 64th Avenue and 65th Avenue. There was no pattern of frequent or habitual illegal parking activity observed.

The percentage of all-day, on-street parkers observed was lower than the average for all neighborhoods on both weekdays and weekends. This suggests that long-term parking/derelict vehicles are not an issue within the corridors identified for study. The percentage of all-day parkers was among the lowest of any neighborhood studied on both weekdays and weekends, with only a 3% difference. Most residents in the neighborhood were not observed to park on-street.

3. **Spillover parking and parking demand hot spots**

According to the representative sample chosen for the study, there is ample parking available in the Lakeshore Estates neighborhood. The neighborhood was observed to have the lowest peak occupancy of any neighborhood observed on both weekdays and weekends. There was a 4% difference between the system-wide peak and the composite peak for both the weekday and weekend, suggesting that there were not significant localized “hot spots” of demand in the neighborhood that occurred outside the system-wide peak.

Only one block face exceeded 50% occupancy at any time studied.

4. **Other**

Staff did not observe any issues with recreational vehicles, boat trailers, or other vehicles belonging to non-residents parking in the cul-de-sacs immediately south of Hidden Lake during any collection period the study.

MAPLETON

On-street parking occupancy in Mapleton peaked at 23% during the morning on the weekday.

1. Parking restrictions

There is no time-limited, ADA, or other restricted parking found within the corridors or areas identified for study in Mapleton.

2. Long-term and illegal parking

There were no illegally parked vehicles observed during the weekday or weekend. Therefore, no pattern of frequent or habitual illegal parking activity was observed.

The percentage of all-day, on-street parkers observed was slightly higher than the average for all neighborhoods on weekdays. However, it was much lower than the average on weekends. Despite the lower-than-average percentage on weekends, the size of Mapleton and industrial nature of the neighborhood suggest that the 12% of vehicles parked all day on weekends is likely to at least partially consist of long-term parked/derelict vehicles, including semi-trucks and trailers. Not many vehicles would otherwise have a reason to be parked all day on a weekend. The difference in the percentage of all-day parkers observed on the weekday versus the weekend was – 11%.

3. Spillover parking and parking demand hot spots

According to the representative sample chosen for the study, there is ample parking available in the Mapleton neighborhood. There was a 2% difference between the system-wide peak and the composite peak for the weekday, suggesting that there weren't any significant localized "hot spots" of demand in the neighborhood that occurred outside the system-wide peak. This is anticipated due to the low number of non-industrial properties within the study area.

No block faces adjacent to residential areas in Mapleton exceeded 50% occupancy during data collections. The only peak occupancies to exceed 50% for any corridor during the weekday or weekend collections, were along Logan Court and Pearl Street. This area has a higher density of smaller-sized businesses that typically lack off-street parking and disproportionately contribute to on-street parking demands.

MIDTOWN AT CLEAR CREEK

On-street parking occupancy in Midtown at Clear Creek (“Midtown”) peaked in the evening at 27% during the weekend.

1. Parking restrictions

There is no time-limited parking found within the corridors or areas identified for study in the Midtown neighborhood. Two ADA spaces were recorded along 67th Avenue, which acts as the “main street” of Midtown.

2. Long-term and illegal parking

No illegally parked vehicles were observed on the weekday or weekend. Therefore, there was no pattern of frequent or habitual illegal parking activity observed.

The percentage of all-day, on-street parkers observed was well below the average for all neighborhoods on both the weekday and the weekend. This suggests that long-term parking/derelict vehicles are not an issue in the corridors of this neighborhood identified for study. The Midtown neighborhood had the lowest percentage of all-day parkers for any non-industrial neighborhood for both the weekday and weekend. This may suggest a lower rate of car ownership for residents in the Midtown neighborhood or that residents do not park on-street as frequently as in other neighborhoods. The difference in the percentage of all-day parkers observed on the weekday versus the weekend was 10%.

3. Spillover parking and parking demand hot spots

According to the representative sample chosen for the study, there is ample parking available in the Midtown at Clear Creek neighborhood. However, there was a significant (9%) difference between the system-wide peak and the composite peak weekend demand, suggesting the existence of localized “hot spots” of demand in the neighborhood that occurred outside the system-wide peak during the weekend. In contrast, the weekday difference was only 4%. Analysis showed that localized hot spots were located along 67th Avenue, scattered throughout the weekend day.

Despite peak weekend demand being on the higher end of all neighborhoods studied, and the large difference between system-wide peak demand and composite peak demand, only two block faces, both along 67th Avenue between Pecos Street and Raritan Drive exceeded 70% occupancy, both occurring at different times. Parking demands ranged between 10% and 50% occupied along all other corridors throughout all collection periods on both the weekday and weekend.

With mixed-use development located along 67th Avenue between Pecos Street and Raritan Drive, higher weekend peak occupancy would be expected for that segment. Overall, no spillover parking issues were observed, and existing on-street supply along 67th Avenue and immediately adjacent segments appear to accommodate existing mixed-use parking demands effectively.

NORTH PECOS

On-street parking occupancy in North Pecos peaked at 50% during the weekday, representing the highest peak weekday occupancy observed for any neighborhood studied. It should be noted, however, that there are only approximately 20 public on-street spaces located in North Pecos.

1. **Parking restrictions**

There is no time-limited, ADA, or other restricted parking found within the corridors or areas identified for study in the North Pecos neighborhood.

2. **Long-term and illegal parking**

No illegally parked vehicles were observed. Therefore, there was no pattern of frequent or habitual illegal parking activity observed.

The percentage of all-day, on-street parkers observed was the lowest of all neighborhoods studied for both the weekday and weekend. There were no all-day parkers observed on the weekend. Because of the very small size of North Pecos's on-street parking system, and industrial nature of the neighborhood, length of stay data for this neighborhood is statistically insignificant. 52nd Avenue may be used for long-term parking/derelect vehicle storage, including semi-trucks and trailers.

3. **Spillover parking and parking demand hot spots**

According to the representative sample chosen for the study, there is ample parking available in the North Pecos neighborhood. There was no difference between the system-wide peak and the composite peak for the weekday, and thus no localized "hot spots" of demand in the neighborhood that occurred outside the system-wide peak.

PERL MACK

Parking occupancy in Perl Mack peaked at 41% during the evening on the weekend. With a weekday peak of 40% occupied, the Perl Mack neighborhood exhibited the second highest overall parking demand of any neighborhood studied, with higher than average utilization observed during all observation periods compared to the other neighborhoods studied. Off-street parking occupancy at the Perl Mack Library peaked at 39% at 9 AM on the weekday, the highest off-street peak observed in any neighborhood.

1. Parking restrictions

There is no time-limited, ADA, or other restricted parking found within the corridors identified for study in the Perl Mack neighborhood. There are two ADA spaces in the Perl Mack library off-street parking lot.

2. Long-term and illegal parking

Fifteen illegal parkers were observed during the weekend system peak, and nine were observed during the weekday peak. Illegal parking activity was concentrated along a few key corridors. This activity constituted a pattern of frequent and habitual illegal parking, particularly in the late evening.

Illegal parking was observed along Cortez Street, Cragmore Street, Cuchara Street, Samuel Drive, and 71st Place during the weekday. On the weekend illegal parking was observed along Alan Drive, Cuchara Street, Jordan Drive, Quivas Street, Raritan Street, Samuel Drive, and 76th Avenue. Illegal parking was particularly prevalent along 76th Avenue, with six illegally parked vehicles observed between Shoshone Street and Zuni Street. Illegal parking occurred in and around the vicinity of four areas: Boulder Crossroads Apartments, Villas on 76th Apartments, the commercial retail development along both sides of Pecos Street south of 72nd Avenue, and Francis Day Elementary School.

The percentage of all-day, on-street parkers observed was far above the average for all neighborhoods on both the weekday and weekend. Along with the mixed nature of land uses in the neighborhood, as well as the large sample size of corridors studied, it can be determined that long-term parking/derelict vehicles may be an issue within the corridors identified for study. 9% more vehicles were parked all day on the weekend versus the weekday. This suggests that a higher number of weekday parkers were visitors or commuters, compared to the weekend.

3. Spillover parking and parking demand hot spots

Though there may be ample parking availability within the overall Perl Mack study area, on-street parking experiences localized areas of high parking demands, straining available supplies in some areas.

A difference of 7% between the system-wide and composite weekend peak suggests that some localized demand “hot spots” occur at times outside the system-wide peak. These hot spots are located in and around the same areas discussed above that experience significant illegal parking activity.

Peak parking demands approached and exceeded 85% occupied along 76th Avenue on most block faces between Zuni Street and Raritan Street on both days studied, as well as along Samuel Drive between 72nd Avenue and 73rd Avenue, Dakin Street west of the Boulder Crossroads Apartments, and Bronco

Road between Cortez Street and Cragmore Street. On the weekends, in addition to the weekday corridors that exceeded 75%, Samuel north of 73rd, 71st Place between Lipan Street and Mariposa Street, Bronco Road between Cuchara Street and Del Norte Street, Del Norte Street between Bronco Road and Greenwood Boulevard, and Alan Drive between 67th and 68th Avenues, all exceeded 75% occupancy at the peak as well.

The data suggests that significant spillover parking activity is occurring from both the Villas on 76th Apartments as well as the Boulder Crossroads Apartments. The commercial retail along Pecos Street are not the perceived cause of spillover parking demand along Samuel Drive. Rather, the multi-family apartment complex to the west of Samuel Drive between 72nd Avenue and 73rd Avenue are the perceived cause.

SHAW HEIGHTS

On-street parking occupancy in Shaw Heights peaked at 20% midday on the weekday as well as during the early afternoon and evening on the weekend.

1. Parking restrictions

There is no time-limited or ADA parking found within the corridors or areas identified for study in the Shaw Heights neighborhood.

2. Long-term and illegal parking

There were two illegally parked vehicles observed during the weekend and one during the weekday during collections. There was no pattern of frequent or habitual illegal parking activity observed, though one vehicle was observed to be parked illegally during both the weekday and weekend collections along 88th Way.

The percentage of all-day, on-street parkers observed was approximately equal to the average for all neighborhoods on the weekday but was the highest observed for any neighborhood during the weekend. Additionally, the difference between the percentage of weekday and weekend all-day parkers of 25% was also the highest observed among the areas studied. Long-term parking/derelict vehicles does not appear to be an issue within the corridors identified for study, despite the high weekend all-day parkers percentage.

3. Spillover parking and parking demand hot spots

According to the representative sample chosen for the study, there is ample parking available in the Shaw Heights neighborhood. However, there was a 7% difference between the system-wide peak and the composite peak weekday demand, suggesting the existence of a few localized “hot spots” of demand in the neighborhood that occurred outside the system-wide peak. The difference on the weekend, however, was only 4%.

Peak demand during the weekday exceeded 75% on three streets: Wagner Drive from N Wiley Circle to S Wiley Circle, all studied block faces of 88th Avenue, and along 88th Way, immediately north of the Flynn Marzano Academy. Demand on the first three segments exceeded 75% on the weekend as well as the weekday. It is unlikely that high demand along 88th Avenue and Wagner Drive represent spillover parking from other areas. The only accessible land uses along those corridors are single-family homes. However, the Flynn Academy may be generating spillover demand during the weekend along 88th Way. It is possible the school was hosting a special event on the weekend day studied that would be responsible for the high demand along 88th Way that was observed.

SHERRELWOOD

On-street parking occupancy in the Sherrelwood neighborhood peaked in the early evening at 22% during the weekend collection, just one percent higher than the weekday peak occurring in the evening at 21% occupied.

1. Parking restrictions

There is no time-limited parking or ADA parking found within the corridors or areas identified for study in Sherrelwood.

2. Long-term and illegal parking

There were 10 illegally parked vehicles observed during the weekend peak and four during the weekday peak. There was a pattern of frequent or habitual illegal parking activity observed, particularly along Sherman Street between Del Norte Street and Grace Place, and along Grace Place east of Del Norte Street. There were an additional seven illegally parked vehicles on the weekend along 78th Circle between Mariposa and Osage on both sides, indicating habitual or frequent illegal parking activity along that street on weekends.

Shaw Heights's percentage of all-day, on-street parkers observed was slightly above the average for all neighborhoods on the weekday and was the second highest observed for any neighborhood during the weekend. The difference between the percentage of weekday and weekend all-day parkers of 25% was also the second-highest observed. Thus, long-term parking/derelict vehicles may not be an issue in this neighborhood, at least within the corridors identified for study, due to the large difference between the weekday and weekend percentages of all-day parkers observed, along with the near average percentage observed on weekdays.

3. Spillover parking and parking demand hot spots

According to the representative sample chosen for the study, there is ample parking available in the Sherrelwood neighborhood. There was a 5% difference between the system-wide peak and the composite peak weekend demand, suggesting that there are very few significant localized "hot spots" of demand in the neighborhood that occurred outside the system-wide peak. The difference on the weekday was also 5%.

Peak weekend demand exceeded 75% occupied on Linda Lane between Erie Street and Dakin Street, Del Norte Street between Grace Place and Sherman Street, Conifer Road between Linda Lane and Ellen, and Greenwood Boulevard between Delta Street and Drake Street. These segments also experienced high demand on the weekday. The high peak demand along Del Norte Street, Conifer Road and Greenwood Boulevard are attributable to adjacent single-family homes and are not perceived to be spillover from other areas. However, the Adventure Academy may be responsible for the high demand that was observed along the section of Linda Lane immediately in front of the school's main entrance.

UTAH JUNCTION

On-street parking occupancy in the Utah Junction neighborhood peaked early in the afternoon at 19% during the weekend.

1. Parking restrictions

There is no time-limited, ADA, or other restricted parking found within the corridors or areas identified for study in Utah Junction.

2. Long-term and illegal parking

There was one illegally parked vehicle along Zuni Street between 52nd Avenue and 53rd Avenue. This vehicle was observed to be parked in this location across all weekend observation times. It is likely that this did not constitute a larger pattern of frequent or habitual illegal parking activity, as no illegally parked vehicles were observed on the weekday.

The percentage of all-day, on-street parkers observed was slightly lower than the average for all neighborhoods on weekdays and equal to the average on weekends. This suggests that long-term parking/derelict vehicles are not an issue within the corridors identified for study. 8% more vehicles were parked all day on the weekend versus the weekday. This length of stay pattern for Utah Junction is likely due to the mix of residential and non-residential uses in the neighborhood. This contributes to more short-term parking activity occurring on weekdays due to part-time employees and business patrons in parts of the neighborhood.

3. Spillover parking and parking demand hot spots

According to the representative sample chosen for the study, there is ample parking available in the Utah Junction neighborhood. There was a 3% difference between the system-wide peak and the composite peak for both the weekday and weekend, suggesting that there were not any significant localized “hot spots” of demand in the neighborhood that occurred outside the system-wide peak.

Only one block face, along Zuni Street between 52nd Avenue and 53rd Avenue, was observed to exceed 50% occupancy throughout collections during the weekend. This block face is directly across from the AT&T/CenturyLink switchboard facility, and likely represents demand from weekend employees of that facility who chose not to park off-street.

This portion of Zuni Street drove weekend peak demand slightly higher than weekday peak demand. On the weekday, demand was above 50% along Columbine Lane between Primrose Lane and 54th Avenue, and along 54th Avenue, between Rosemary Lane and Columbine Lane. Demand along several block faces of Zuni Street between 52nd Avenue and 54th Avenue was also approaching 50%. This demand potentially represents spillover parking generated by the Sisters of Saint Francis Convent, Berkeley Hills Park, and AT&T/CenturyLink facility respectively.

WELBY

On-street parking occupancy in the Welby neighborhood peaked midday at 18% during the weekday. Off-street parking at the Rotella Park Lot peaked at 8% at 9 AM on the weekend.

1. Parking restrictions

There is no time-limited parking or ADA on-street parking found within the corridors identified for study in Welby. There are six ADA spaces found in the Rotella Park Lot.

2. Long-term and illegal parking

14 vehicles were observed to be illegally parked during the weekday peak and 17 during the weekend peak. There was a pattern of frequent or habitual illegal parking activity observed occurring predominantly along Coronado Parkway between Explorador Calle Avenue and York Street and between Explorador Calle Avenue and Nueva Vista Drive on both the weekday (seven vehicles) and weekend (14 vehicles), particularly in the morning. A second “hot spot” of illegally parked vehicles was Downing Drive between 83rd Place and Nueva Vista Drive on the weekday. Other illegally parked vehicles were distributed throughout the neighborhood.

3.

The percentage of all-day, on-street parkers observed was above the average for all neighborhoods on both the weekday and weekend. The weekday percentage of all-day parked vehicles was one of the highest percentages observed. Thus, long-term parking/derelict vehicles may be an issue in this neighborhood, at least within the corridors identified for study. The percentage of all-day parkers was 2% lower on the weekend than the weekday, further supporting the possible existence of higher than average long-term parking activity.

4. Spillover parking and parking demand hot spots

According to the representative sample chosen for the study, there is ample parking available in the Welby neighborhood. There was a 6% difference between the system-wide peak and the composite peak weekend demand. The difference on the weekday, for reference, was 5%.

Peak weekday demand exceeded 75% on only two block faces: Downing Drive between 83rd Place and Nueva Vista Drive, and Coronado Parkway west of Downing Drive. This likely represents spillover demand resulting from The Shiloh Condominiums and Coronado Hills Elementary School, respectively.

On the weekend, demand was heaviest along Coronado Parkway between York Street and Nueva Vista Drive, a small segment of Sheldon Drive, and along 78th Avenue just east of York. This demand is likely generated by the multi-family housing southwest and southeast of Coronado Parkway south of Explorador Calle, from employees of the commercial retail center containing Walmart on 88th and Washington north of Sheldon, which is located in the City of Thornton, and from the Assumption of the Blessed Virgin Catholic Church, respectively.



06 Parking Policy

SECTION 6 – PARKING POLICY

This section focuses on the review of the existing parking policy as well as identify opportunities for updates and changes that can help to streamline and otherwise modernize parking policy. Concerns over potential spillover parking demand in certain neighborhoods, illegal parking activity along with long-term parking of vehicles and parking of derelict vehicles along public right-of-way, have been the impetus for this parking study, and such conditions were observed in the field and identified in previous sections of this report. Modernization of the code and refinement of parking policies will be the catalyst to constructing the framework that will allow the county to address these concerns as they relate to both existing and future development.

EXISTING POLICY

This section presents a summary of existing parking related policies taken from the Adams County Development Standards and Regulations. This summary is not intended to replace the use of the full Standards and Regulations for development review purposes.

PARKING AS A LAND USE

Parking as a stand-alone land use is permitted only within specific zoning districts under either a conditional use permit (CUP) or under a Planned Unit Development (PUD). Parking as a standalone use is not permitted within Transit Orientated Developments (TODs).

The following list summarizes where standalone parking as a land use is allowed:

- Conditional Use Permit (CUP)
 - A-2
 - A-3
 - C-1
 - C-2
 - C-3
- Planned Unit Development (PUD)
 - C-4
 - C-5
 - I-1
 - I-2
 - I-3

STORAGE AND OUTDOOR PARKING

Storage of personal and commercial vehicles within the public right-of-way contributes to issues of equitable access to a public resource along with limiting accessibility for public works and utilities to potential easements along the curb. While the County does not have time restrictions on the duration a personal vehicle may remain parked on-street, there are prohibitions against parking vehicles over 7,000 pounds gross vehicle weight (GVW) in residentially zoned areas.

RECREATIONAL AND INHABITED VEHICLES

The County does not currently have restrictions in place that limit the duration a recreational or oversized vehicle (under 7,000 pounds GVW) may remain parked within the public right-of-way. However, if the recreational vehicle is inhabited, the vehicle may remain parked on-street for a maximum of 30 days. The location must be in conjunction with a primary residence and is limited to only once per calendar year.

PARKING MINIMUM REQUIREMENTS

Minimum parking requirements for supply based on land use category are documented in several sections throughout Chapters 3 and 4 of the Adams County Development Standards and Regulations Document. The authority for the County to adopt and enforce development standards and regulations is provided via the Colorado Constitution and the Revised Colorado Statutes. Chapter 4 provides a highly detailed prescription of parking supply requirements based on land use category. These are outlined in Section 12 as well as discussed individually for select land uses in Sections 3 through 11 of the Standards and Regulations.

The table below provides a condensed summary of parking requirements per non-residential land use category, with residential land use parking minimum requirements summarized in the following table.

Table 156: Summarized Parking Minimum Requirements per Land Use (Non-Residential)

Land Use	Minimum Parking
Art Galleries, museums, offices, radio, TV recording studio	1.0 per 300 SF GFA
Auditoriums, assembly halls, theaters, meeting rooms and exhibit halls	Greater of: 1.0 per 4.0 fixed seats OR 1.0 per 40 SF where seats are moveable
Auto repair shops	2.0 per 1.0 service bay
Banks and other financial institutions, clubs and lodges	1.0 per 200 SF GFA used by general public AND each 600 SF not used by general public
Bed and breakfast inns	1.0 per 1.0 rental unit AND 1.0 per 4.0 employees
Boarding houses, hotels, commercial dormitory, motels	1.0 per 1.0 rental unit
Campgrounds, tent campgrounds, travel trailer and RVs	1.0 per 1.0 camp site AND 1.0 per 10.0 trailers
Car wash	5.0 per 1.0 automatic bay OR 2.0 per 1.0 self-service bay
Carnival or circus	300 per 1.0 acre of area used for amusement purposes
Contractor's offices, yards, building, laboratories, research, manufacturing, industrial	1.0 per 10,000 SF
Convalescent centers and nursing homes	1.0 per 4.0 beds AND 1.0 per 2.0 staff
Correctional facilities	1.0 per 1.0 employee per shift AND 1.0 per 5.0 occupants
Day care facility	1.0 per 1.0 employee on a major shift PLUS 2 drop off spaces for patrons
Fairgrounds	1.0 per 600 SF GFA
Hospitals	1.0 per 2.0 beds AND 1.0 per 1.0 doctor and nurse AND 1.0 per 4.0 additional employees
Junk yards	1.0 per 10,000 SF yard PLUS 1 space

Medical, dental or similar offices, veterinary establishments	4.0 per 1.0 doctor or nurse AND 1.0 per 1.0 additional employee
Night clubs, restaurants, cafeterias, dining rooms, including drive-up windows	1.0 per 3.0 seats
Places of worship, churches	1.0 per 5.0 seats
Service establishments and retail	1.0 per 600 SF GFA, low frequency public interface (i.e., furniture store) OR 1.0 per 200 SF GFA, higher frequency public interface (i.e., salon, grocery)
Elementary school	1.0 per 1.0 classroom AND 1.0 per 300 SF of administrative office space
High school, college, vocational/business school	6.0 per 1.0 classroom AND 1.0 per 300 SF of administrative office space
Transportation terminals	1.0 per 1,000 SF GFA freight terminal AND 1.0 per 200 SF GFA passenger terminal
Vending and produce stands, Christmas tree sales	5.0 per 200 SF of stand area
Warehouses	1.0 per 5,000 SF GFA
Wholesale establishments	1.0 per 900 SF GFA
Colleges and universities, golf courses, government buildings, recreation (indoor/outdoor)	As determined by Planning Commission or Dir. Of Community and Economic Development

Source: Adams County

Table 157: Summarized Parking Minimum Requirements per Land Use (Residential)

Land Use	Minimum Parking
Single-family & two-family	
Single family	2.0 per 1.0 dwelling unit
Two family	2.0 per 1.0 dwelling unit PLUS Planning Commission may require up to 1 space per 2 units for visitor parking
Multi-family	
Studio	0.75 per 1.0 dwelling unit
1 BR	1.0 per 1.0 dwelling unit
2 BR	1.5 per 1.0 dwelling unit
3+ BR	2.0 per 1.0 dwelling unit
Visitor	15% of required resident parking
Efficiency	1.0 per 1.0 dwelling unit
Accessory Dwelling	1.0 per 1.0 accessory dwelling unit in addition to requirements of primary dwelling unit
Retirement	1.0 per 2.0 Multifamily dwelling unit OR 1.0 per single family dwelling unit AND 1.0 per 4 staff members
Other	
Mobile Home	2.0 per 1.0 mobile home site
Dormitories, fraternity, and sorority houses	1.0 per 1.0 per 2.0 beds

Source: Adams County

ON-STREET PARKING

Regardless of zoning classification, parking is prohibited on all arterial roadways throughout unincorporated Adams County.

ADA REQUIREMENTS

Of the minimum parking requirements, the Standards and Regulations prescribe how many of the total spaces must be signed as ADA restricted and the associated sizing and location of those spaces. The table below summarizes the number of accessible spaces required by total capacity of the parking facility.

Table 158: Minimum Number of ADA Spaces Required

Total Parking Spaces in Facility	Minimum Number of Accessible Spaces Required
≤ 25	1
26 – 50	2
51 – 75	3
76 – 100	4
101 – 150*	5
151 – 200	6
201 – 300	7
301 – 400	8
401 – 500	9
501 – 1,000	2% of total spaces
> 1,000	20 spaces plus 1 for every 100 spaces or fraction thereof over 1,000

Source: Adams County

ADA spaces are required to be a minimum of 13 feet wide, unless they are located parallel to a pedestrian walkway.

Accessible parking supplies are to be located as close as possible to the nearest accessible entrances using the shortest accessible route of travel. When possible, this route should not cross vehicular travel. Each accessible space shall be clearly marked for reserved use with signage and/or pavement markings

PARKING SUPPLY REDUCTIONS

The County allows for developments to accommodate parking needs at an off-site location within 600 feet of the development. At less than an eighth of a mile, 600 feet is the standard maximum walking tolerance used by many shopping centers. This is seen at the Outlets at Castle Rock and Denver Premium Outlets, as shown in the figures below. In the diagrams, each red circle represents a 600-foot buffer from popular entry points.

Figure 116: Example Walking Distances



Source: Walker Consultants

SHARED PARKING

County Standards and Regulations currently allow for shared parking where conditions meet the following requirements:

1. The proposed land uses do not have overlapping principal hours of parking demand by more than 30 minutes.
2. The shared use parking facility(s) must provide an equal number of parking stalls to those required for each participating land use.
3. The shared site is located within 600 feet each participating land use, and if the parking facility is the primary use on its site, that it be located within a zone that permits parking as a use.
4. The shared parking facility(s) provide a pedestrian connection between the shared uses and the parking facility(s) with included lighting and safety considerations.
5. A contract signed by all participating land owners and the County providing:
 - a. The land comprising the shared parking facility(s) is not encroached upon or in any manner transferred that changes the use of the facility for as long as the shared parking facility(s) is needed to meet the need of any participating land use(s);
 - b. The prime hours of operation for participating land uses;
 - c. Assigns maintenance provisions for the facility(s) and landscaping;
 - d. Enforcement by the County;

- e. Final approval of the contract by the Director filed with the deed of the parcels involved so that it is binding on any and all successors;
- f. Changes to or termination of the contract is based upon the review and approval of the Director

ELECTRIC VEHICLE CHARGING STATIONS

Parking stalls serving electric vehicle charging station may comprise up to 5 percent of the required parking supply.

OTHER PARKING REDUCTIONS & MEASURES

County Standards and Regulations provide the Director final approval on other, alternative methods of meeting the intent of the parking requirements. Examples cited in the regulations include mechanical stackers, tandem parking stalls, motorcycle parking, and Family Friendly parking allocations.

PARKING ADJUSTMENTS

While the above Parking Flexibility options provide for alternative means of providing parking supply requirements, they do not provide parking reductions. Regulations do provide an opportunity to apply for a parking reduction of up to 25 percent with Parking and Transportation Demand Management Study approved by the Director. Such a study must be completed by a parking or traffic professional and account for the following:

1. Reviews multiple comparable projects in the region
2. Demonstrable alternative transportation mode options provided that reduce single occupant vehicle usage, such as
 - a. Car and bike sharing services located on site
 - b. Car and van pool services
 - c. Shuttle services
 - d. Transit passes
 - e. Transit supportive site design elements such as immediately adjacent transit stops, provision of loading spaces on site, transit information centers, or enhanced pedestrian routes (see sidebar) within one-half mile of a Park & Ride or rail station
3. Demonstrates a density of more than 14 dwelling units per acre;
4. The proposed site for reduced supply is located within 1,300 feet, or just under a quarter mile, of basic daily uses (i.e., grocery or convenience store, child care) or weekly uses (i.e., bank, restaurant);
5. Other criteria as approved by the Director and support by the study that meets the intent of the Standards and Regulations.

ENHANCED PEDESTRIAN ROUTES

Enhanced pedestrian routes connecting land uses to the Park & Rides or rail stations provide:

- continuous weather protection of at least 50% of the property frontage;
- continuous, direct walkways to and from the Park & Ride or rail station;
- generally continuous street lighting and minimized or enhanced pedestrian crossings at vehicle routes

TRANSIENT ORIENTED DEVELOPMENTS

Transit oriented developments (TOD), small communities designed to be compact, walkable, pedestrian-oriented mixed-use projects developed around transit activity centers, typically experience reduced parking demands based on the nature of their design. In addition to the reduced parking minimums summarized below, TODs are afforded additional potential parking reductions.

REDUCED NEED POPULATIONS

Several populations are recognized as consistently utilizing less parking as recognized by their common characteristics. Multifamily developments that restrict at least 80 percent of dwellings for those aged 60 years or older are eligible for a reduction up to 33 percent. Developments that restrict at least 80% of dwellings for those meeting the definition of “handicapped” as defined by the Federal Fair Housing Act are eligible for a reduction of up to 50 percent.

SHARED PARKING

Although shared parking arrangements in other zoning districts do not provide a reduction in the minimum parking requirement, within areas zoned for TOD, participating land uses may document their agreement with the County and are eligible for a reduction factor based on the land use mix as summarized in the next table. The reduction factor is applied by dividing the cumulative minimum requirement of the participating land uses by the shared parking factor.

Table 159: Shared Parking Reduction Factors by Land Use Mix

Property Use	Multi-Family	Institutional	Restaurants, Recreation or Hotel	Retail Sales	Office or Commercial Services
Multi-family	-	1.1	1.1	1.2	1.3
Institutional	1.1	-	1.2	1.3	1.3
Restaurants, recreation, or hotel	1.1	1.2	-	1.3	1.7
Retail sales	1.2	1.3	1.3	-	1.2
Office or commercial services	1.3	1.5	1.7	1.2	-

Source: Adams County

In addition to general minimum parking requirements, Section 27 of Chapter 3 prescribes parking minimum and maximum requirements per land use category for properties located within a transit-oriented district (TOD). Within a TOD district, non-residential properties comprising fewer than 2,000 square feet are exempted from parking minimum requirements. Additionally, on-street parking located adjacent to the site may be counted to meet up to 25 percent of the minimum parking requirements.

The table below summarizes parking minimum and maximum requirements for TOD districts.

Table 160: Parking Minimum and Maximum Requirements per Land Use in TOD Districts

Use Type	Minimum Parking (Spaces per GSF Unless Otherwise Specified)	Maximum Parking (Spaces per GSF Unless Otherwise Specified)
Residential		
All residential	1.0 per unit	2.0 per unit
Office		
General office and financial services	1.0 per 400	1.0 per 300
Medical offices	1.0 per 300	1.0 per 200
Institutional	See Section 4-12-04-03	
Commercial		
Commercial sales and services	1.0 per 500	1.0 per 400
Restaurant	1 per 4 persons of maximum occupancy capacity of customer service	
Industrial		
Accessory office or admin area		1.0 per 500
Accessory indoor sales area		1.0 per 400
Indoor storage, distribution, warehousing, assembly, vehicular service, or manufacturing area		1-3,000 SF GFA 1 per 400
		3,001 – 5,000 SF GFA 1 per 500
		5,001 – 10,000 SF GFA 1 per 750
		10,001+ SF GFA 1 per 1,250

Source: Adams County

NEIGHBORHOOD PLANS

Neighborhood plans and planning documents provide valuable insights into the desired parking environment and community character of the constituents of the various neighborhoods that comprise Adams County. While these documents are included here for the valuable context they can provide, it should be noted that action items provided in these plans typically are recommendations or suggestions. The county may or may not have taken action on implementing or codifying provisions from these plans in terms of official policies and standards.

BERKELEY NEIGHBORHOOD PLAN, 2008

The Berkeley Neighborhood Plan was adopted in 2008 as an Amendment to the Adams County Comprehensive Plan. The Berkeley Neighborhood Plan portrays a strong desire in the community to limit on-street parking and encourage off-street parking. Action items noted within the Plan to follow up with the County on included requesting a residential parking permit program in the area near Regis University and the installation of “No Parking” signage in select areas. While it is unclear if the permit program request was made to the County, a program is currently not in place.

CLEAR CREEK VALLEY TOD PLAN, 2009

The Clear Creek Valley TOD Plan reads as though it was specifically developed for the area of Federal Station. The Plan notes that RTD has budgeted surface parking at the station through 2030, although a recommendation is noted for a retail wrapped parking structure in its place. The Plan also presents alternative TOD minimum parking requirements by land use, as summarized and compared to the Adams County TOD parking requirements in the next table.

Table 161: Summarized Comparison of Clear Creek Valley TOD Plan and Adams County TOD Parking Requirements

Land Use Category	TOD Min/Max	Clear Creek Valley TOD	Unit of Measure
Multifamily	1.0 / 2.0	1.5	per unit
Office	2.2 / 3.33	2.5	per 1,000 ft ²
Retail	2.0 / 2.5	3	per 1,000 ft ²
Light Industrial	NA	2.5	per 1,000 ft ²

ADAMS COUNTY REGIONAL PARK FAIRGROUNDS MASTER PLAN, 2009

The Adams County Regional Park Fairgrounds Master Plan was developed as an addition to the Adams County Regional Park Mater Plan. In the Fairgrounds Master Plan, it is noted that “though the fairgrounds sees year-round activity, parking areas often stand empty.” This observation, along with reports that parking facilities experience significant ponding and excessive mud during inclement presents a strong opportunity to improve parking facilities and explore opportunities to reduce parking supplies through parking reduction strategies and promotion of alternative modes of transportation.

OPEN SPACE AND TRAILS PLAN, 2012

The 2012 Open Space and Trails Plan was performed as an update to the 1998 Master Plan. A significant focus of the Plan was concerned with improving the fishing and equestrian experiences through improving their associated parking experience. Specific action items provided in the Plan include conducting an inventory of parks' parking facilities and providing parking areas for horse trailers at suitable trailhead locations.

WELBY: WHERE DEEP ROOTS GROW – A NEIGHBORHOOD PLAN FOR FUTURE DEVELOPMENT, 2014

In WELBY: Where Deep Roots Grow, parking issues related to overflow and illegal parking occurring near the Springbrook Apartments were noted several times throughout the Plan. A concern for this issue is the impacts on traffic circulation and specifically how it relates to site lines of vehicles and the resulting safety impacts. Additionally, the Plan reports a desire for added supply to serve the Clear Creek Trailhead.

MAKING CONNECTIONS: SOUTHWEST ADAMS COUNTY PLANNING AND IMPLEMENTATION PLAN, 2016

Making Connections identifies updated parking regulations as the second greatest priority of the Top 40 projects identified for the community. However, other than updating parking requirements to provide reduced parking requirements for affordable housing developments, no other specific regulation updates and related issues are identified. Within the plan, a desire is noted to move parking supplies along Federal Boulevard behind buildings. While Standards and Regulations already support this requirement for future developments, existing properties or those that change land use categories without a significant impact to the parcel layout present little opportunity to comply.

CLEAR CREEK CORRIDOR MASTER PLAN, 2017

In the Clear Creek Corridor Master Plan, parking supplies are identified as a top concern throughout resident surveys. Specifically, the Plan notes a lack of parking near trailheads and recommends the County explore shared parking opportunities with private businesses in these areas, as well as expanding existing facilities where possible.

OPPORTUNITIES TO MODERNIZE PARKING POLICIES

Modernization of parking policies can provide simple language updates to incorporate new technologies that were not available at the time standards and regulations were originally drafted or last updated. It can also provide flexibility to allow future technologies and strategies, not yet identified, to support an efficient parking system serving the public and private demands of the community at varying levels unique to the characteristics and needs of the neighborhoods that make up unincorporated Adams County. The following identified opportunities to modernize the standards and regulations governing parking within the County aim to provide community planners and decision makers tools that are flexible and scalable to apply throughout the County equitably and with appropriate flexibility.

STORAGE OF VEHICLES WITHIN THE PUBLIC RIGHT-OF-WAY

Other than storage of oversized and inhabited vehicles, there are currently no restrictions of parking within the public right-of-way. Regulation of the curb lane is essential in maintaining access for Public Works and reinforcing the public nature of the resource. Often, especially in areas without curb and gutter, property owners view and treat the curb lane(s) immediately adjacent to their property as an extension of their property.

This has escalated in some instances to vandalism and violence when neighbors go to extreme lengths to protect what they perceive to be theirs. In 2015 several instances of carshare vehicles were reported in which branded vehicles parked at the curb in Denver residential areas were tipped over. In 2018 one Denver neighborhood resident vandalized a neighbor's car for parking in front of his home when space in front of her own was not available. Other reports include residents painting the curb in front of their home or using homemade or purchase signage to block off and otherwise discourage others from the space.

Implementation of requirements to discourage long-term storage of personal vehicles in the public right-of-way is a relatively small, initial step in managing the curb lane. Not only does management of the curb lane increase equitable access to the resource, but also reinforces its status as public property. It is not uncommon to find damage, litter, and illegal parking along unmanaged and undeveloped public rights-of-way without curb and gutter. The images below demonstrate damage to the right-of-way caused by vehicles.

Many communities require vehicles to move every 72 hours to one week. Currently in Adams County, only inhabited vehicles must be moved, and only after 30 days have elapsed. Extending this 30-day time restriction to all derelict vehicles that are parked along the curb lane would be in line with supporting community goals to decrease prevalence of such vehicles parked along public streets. 30 days provides vehicle owners ample opportunity to resolve compliance issues with their vehicles.

EXAMPLE LANGUAGE

It shall be unlawful for any owner or operator of a vehicle to leave that vehicle parked in the same place on a public street continuously for a period in excess of 30 calendar days. A vehicle shall be considered in violation of this regulation if it has not been moved at least one hundred (100) feet during the 30-day period of time.

It shall be unlawful for the owner of a derelict vehicle to leave it parked on any public street for a period in excess of 30 days, regardless of location. The 30-day time limit includes the cumulative time spent on any public street. For the purposes of this regulation, a derelict vehicle is defined as a vehicle which is:

1. *Apparently inoperable; and*
2. *Extensively damaged, such as damage including but not limited to any of the following: broken windows, windshield, or both; missing wheels, tires, motor, or transmission.*

PARKING SUPPLY REDUCTIONS AND ADJUSTMENTS

Within the current regulations, the section providing “Parking Supply Reductions” does not provide any actual reduction in parking requirements. Rather, this section provides for alternative parking supply provisions. For instance, the shared parking option requires the shared parking resource to “provide parking spaces equal to those required for each use,” while also prohibiting the land uses from having overlapping parking supplies. It would appear the intent would be to share one smaller parking resource used at different times by patrons of two or more land uses. Instead, it is worded such to encourage large parking supplies that sit underutilized for significant portions of the day.

Similarly, “supply reductions” are afforded to those who supply electronic vehicle or off-site parking. As these are worded, no reduction in required supply is provided. Electric vehicle charging stations are providing on a 1:1 basis for standard parking stalls. Off-site parking provides the same parking supply require at an alternative location up to 600 feet away.

Along the lines of off-site and shared parking, where allowed, the distance provided is very restrictive. According to the Institute of Parking and Mobility Institute, research indicates that walking tolerances related to retail, restaurant, office, employee parking, and entertainment is typically as far as 1,200 feet¹, or just under a quarter mile. Extending the distance shared parking facilities may be located from their associated land use(s) provides greater flexibility in shared parking opportunities and locating parking behind buildings, away from the curb.

Much of the Parking Adjustments section of the standards addresses actual parking reduction opportunities. Here, specific strategies to redistribute transportation demands are provided, with a requirement of completion of a Transportation Demand Management study by a parking or traffic professional. Potential restrictive elements of this regulation include the requirement of a residential component to the mixed-use project and use of comparable land uses from within the region. Land uses that are well established in areas outside the region can provide adequate comparable analysis when appropriately screened for similar parking demand influence characteristics (walkability of and availability of transit serving the site, vehicle ownership and mode split statistics for the populations, etc.).

Additionally, there are several examples of highly effective shared parking arrangements that do not include a residential component. Religious centers typically present ideal parking behaviors for shared opportunities with commercial land uses. Also, hotels typically experience peak parking demands late in the evening and overnight, providing complimentary parking demands typically exhibited by offices and retail stores.

Within the Transit Oriented Developments section of the regulations, parking reduction factors are outlined for mixed use developments based on two land uses. These factors do not account for additional reductions possible through land uses mixes including three or more categories.

¹ Bier, Leonard T. and Vergene, Mark A., *Identifying an Area of Influence, A Guide to Parking*, International Parking and Mobility Institute, 2018

These sections can be significantly simplified by replacing them with updated language that requires a Mobility Study performed by parking or transportation professional and approved by the Director. Generalizing the regulation provides for greater flexibility in applying future strategies that support the intent of the original language. Use of a parking and mobility study would replace parking flexibility, adjustment, and reduction options for general developments and TODs, including the potential to utilize adjacent on-street to satisfy a portion of a subject site's parking requirements.

EXAMPLE LANGUAGE

Based on the following criteria, the Director may grant a parking reduction, not to exceed twenty-five (25) percent of the required parking, for commercial, institutional and mixed-use development:

- 1. The nature of the proposed use(s), the size of the site or other physical constraints, or location of the site that may result in a situation where the parking requirements cannot be met on site without unnecessary hardship, practical difficulties or negative impact on the visual character of the site or neighborhood; and*
- 2. The applicant can satisfactorily demonstrate by means of a parking and mobility study completed by a parking or transportation professional that the parking needs of the use will be adequately served, and, if the reduction is due to the nature of the use, the applicant shall provide assurance that the nature of the use will not change; and*
 - a. A mix of uses is proposed and the parking needs of all the uses can be accommodated through joint use of parking spaces with varying time periods of use; or*
 - b. The applicant provides an acceptable proposal for an alternate mode of transportation program, including assurances that the use of alternate modes of transportation will adequately supplant the need for on-site parking on an ongoing basis.*

REDUCED NEED POPULATIONS

Reduced parking requirements are provided in the Standards for properties predominantly serving individuals aged 60 and over and those that meet the definition of "handicapped" as defined by the Federal Fair Housing Act. However, consideration should be given to properties or proposed residential developments that will house low income individuals as well, with such developments qualifying for a reduced minimum number of parking spaces required. Recent studies have demonstrated that affordable housing generates less parking demand than typical market rate multi-family housing developments.

In 2019, Walker conducted a parking study for a proposed affordable housing project located in Adams County, within the City of Westminster. The goal of the study was to determine what an appropriate parking reduction ratio would be for the proposed development in question, based on Westminster's existing default parking minimums for multi-family housing. This determination would be made by quantifying the difference between the average number of vehicles owned per household for the expected median household income bracket for the development and the average number of vehicles owned per household for the greater area.

It should be noted that the study area for Walker's analysis was approximately half a mile east of the Shaw Heights neighborhood, one of the neighborhoods examined in this report.

For data on average number of vehicles owned per household by income bracket, Walker referenced a model for automobile ownership relative to household income that was established by the National Center for Smart

Growth Research and Education (NCSGR) at the University of Maryland. The study examined data collected for approximately 70,000 households across the United States based on a Nationwide Household Travel Survey (NHTS) that was conducted by the Federal Highway Administration. Because auto ownership for Millennials has fallen in more recent years, according to more recent NHTS data, it should be noted that number of vehicles owned per household for most income brackets has likely decreased in the years since the NCSGR model was established.

The table below summarizes vehicle ownership by percent for various selected income brackets as determined by the NCSGR’s research and derived from Federal Highway Administration data. The income bracket containing the weighted average 60% Area Median Income (AMI) value for the proposed development Walker studied, which was the maximum household income threshold proposed for this development, is highlighted in yellow and the row containing the median household income bracket for the greater area is highlighted in grey.

Table 162: Average Percent Distribution of Vehicle Ownership by Median Income Bracket

Median Household Income Bracket	Number of Vehicles Owned				
	0	1	2	3	4
\$25,000 - \$29,999	12.87%	45.15%	33.58%	5.97%	2.43%
\$30,000 - \$34,999	11.66%	50.18%	22.26%	13.43%	2.47%
\$35,000 - \$39,999	7.89%	42.57%	34.50%	9.54%	5.50%
\$40,000 - \$44,999	5.23%	34.30%	40.70%	15.12%	4.65%
\$45,000 - \$49,999	4.89%	31.06%	41.91%	17.23%	4.91%
\$50,000 - \$54,999	1.90%	26.03%	54.29%	14.92%	2.86%
\$55,000 - \$59,999	1.76%	21.15%	49.34%	18.94%	8.81%
\$60,000 - \$64,999	0.44%	13.78%	50.67%	22.67%	12.44%
\$65,000 - \$69,999	0.22%	13.65%	53.24%	22.60%	10.29%
\$70,000 - \$74,999	0.40%	13.04%	60.87%	22.92%	2.77%
\$75,000 - \$79,999	1.83%	13.32%	56.66%	20.37%	7.82%
\$80,000 - \$99,999	0.55%	7.65%	57.92%	23.17%	10.71%

Source: National Center for Smart Growth Research (University of Maryland)

In order to make a direct comparison of vehicle ownership between income brackets possible, Walker calculated a weighted average number of vehicles owned for each income bracket using the above data. These calculations are shown in the following table. In the table, the column on the far right shows how the total weighted average number of vehicles owned per income bracket differs from the average number for the income bracket that contains the median household income for the greater area, shown in grey.

Table 163: Weighted Average Number of Vehicles Owned per Household per Income Bracket

Median Household Income Bracket	Number of Vehicles Owned					Total Weighted Average Number of Vehicles Owned	Difference in Percent from Weighted Average Number of Vehicles for Greater Area Income Bracket
	0	1	2	3	4		
\$25,000 - \$29,999	0	0.45	0.67	0.18	0.1	1.4	-41%
\$30,000 - \$34,999	0	0.5	0.45	0.4	0.1	1.45	-39%
\$35,000 - \$39,999	0	0.43	0.69	0.29	0.22	1.62	-31%
\$40,000 - \$44,999	0	0.34	0.81	0.45	0.19	1.8	-24%
\$45,000 - \$49,999	0	0.31	0.84	0.52	0.2	1.86	-21%
\$50,000 - \$54,999	0	0.26	1.09	0.45	0.11	1.91	-19%
\$55,000 - \$59,999	0	0.21	0.99	0.57	0.35	2.12	-10%
\$60,000 - \$64,999	0	0.14	1.01	0.68	0.5	2.33	-1%
\$65,000 - \$69,999	0	0.14	1.06	0.68	0.41	2.29	-3%
\$70,000 - \$74,999	0	0.13	1.22	0.69	0.11	2.15	-9%
\$75,000 - \$79,999	0	0.13	1.13	0.61	0.31	2.19	-7%
\$80,000 - \$99,999	0	0.08	1.16	0.7	0.43	2.36	0%

*Note that the total vehicle ownership data applies to both single-family and multi-family housing and does not differentiate based on total household size. It can be assumed that actual vehicle ownership ratios per household for multi-family housing units (which typically have fewer people /unit) are less than the overall ownership ratios shown above.

Source: National Center for Smart Growth Research, Walker Consultants

According to the above table, the average number of vehicles owned per household in the proposed development would be at least 21% lower than the average number of vehicles owned per household in the greater area.

According to the study and Walker’s analysis, a negative correlation exists between household income bracket and vehicle ownership, with households in each successive lower household income bracket owning fewer vehicles, on average.

This study provides a framework for how Adams County could move forward with allowing for parking reductions for affordable housing in its development standards that would be appropriate for and tailored to the county. It could accomplish this either by establishing a single reduction percentage or by establishing a graduated table of reductions that would be allowed based on the expected average housing income of a proposed affordable development and maximum household AMI threshold for the development

Another approach for establishing parking reductions for affordable housing developments could be to look towards peer Front Range jurisdictions that have existing language in their parking ordinances and codes that allows for parking reductions for affordable housing. The table below shows parking reductions for affordable housing allowed for three other selected Front Range jurisdictions.

Table 164: Peer Community Comparison of Affordable Housing Parking Reductions

Fort Collins, CO	Longmont, CO	Denver, CO
50% reduction	1.0 space/DU	25% reduction

When communities require off-street parking with residential construction, the cost of driving is shifted to be included in the cost of housing, which artificially increases the cost of housing. Reduced parking requirements for residential properties that typically experience lower vehicle ownership supports reduced construction costs. They also support increased use of transit and other alternative modes of transportation as well as greater reliance on shared mobility. Lower development and construction costs can translate into reduced housing costs across the board, but particularly for low-income individuals and families who would benefit the most from reduced costs.

EXAMPLE LANGUAGE

The City of Seattle lists affordable housing as a separate land use category within the residential portion of their parking minimum requirements, as shown in the following table. Relevant text is highlighted in yellow.

Table 165: City of Seattle Example Language for Affordable Housing Parking Requirements

Table B for 23.54.015 REQUIRED PARKING FOR RESIDENTIAL USES	
Use	Minimum Parking Required
III. Multifamily residential use requirement with rent and income criteria	
P. For each dwelling unit rent and income-restricted at or below 80 percent of the median income	No minimum requirement

Source: City of Seattle

Similarly, Milwaukee presents the reduction option as a note to the multifamily land use category:

Table 166: City of Milwaukee Example Language for Affordable Housing Parking Requirements

Table 295-403-2-a NUMBER OF PARKING SPACES, BY USE	
Uses	No. of Parking Spaces Required
Residential Uses	
Multi-family dwelling: ...	
*Note: In RM6, RM7, C9A and IM districts, a private elderly housing project shall have one parking space for every 2 dwelling units; in other zoning districts, a private elderly housing project shall have 2 parking spaces for every 3 dwelling units. Public housing for low-income families and public or federally-assisted low-income elderly housing projects shall provide 1 parking space for every 2 dwelling units.	

Source: City of Milwaukee



06 Technology & Operations Strategies

SECTION 7 – TECHNOLOGY & OPERATIONS STRATEGIES

Neighborhoods that were observed to not display characteristics indicative of issues of concern include:

- Aloha Beach
- Arlington Meadows
- Lakeshore Estates
- Mapleton
- Midtown at Clear Creek
- North Pecos
- Shaw Heights
- Utah Junction

For these eight neighborhoods, Walker is recommending that unmanaged parking be maintained as-is, with the option of infrequent, active enforcement for illegally parked vehicles or other right-of-way infractions.

Some patterns of illegal parking, but not spillover parking or areas of consistently high demand, were found in the following neighborhoods:

- Goat Hill
- Fairview
- Sherrelwood

For these neighborhoods, Walker is recommending that unmanaged parking be maintained as-is, with the option for semi-regular enforcement or targeted enforcement for illegally parked, derelict vehicles, or other right-of-way infractions.

The remaining four neighborhoods were found to have both illegal parking and possible spillover parking demand and/or areas of consistently high parking demand. These are:

- Berkeley
- Perl Mack
- Welby
- Florado

For these neighborhoods, Walker is recommending that the County explore various options for parking management and enforcement, particularly in areas where potential problems have been identified and confirmed by study.

Management for each of these neighborhoods, depending on management options chosen, could be exclusive to each neighborhood independently, or be unified under a common enforcement scheme that employs use of license plate reader (LPR) technology, discussed in the last section of this memo.

PARKING MANAGEMENT OPTIONS

ALL NEIGHBORHOODS

At this time, Walker is not recommending that the county consider implementing paid or metered parking in any neighborhood. It should be noted that the only area or corridor where paid or metered parking may be a tenable option in the future, out of all the corridors or areas identified in our study, is along 67th Avenue in the Midtown neighborhood. This is due to the unique mixed-use and higher-density nature of the corridor, which may eventually lead to consistently high on-street parking demand at peak times. However, an established pattern of very high on-street parking utilization would first need to be observed in order for metered parking to be useful as a parking management tool. This level of parking demand is likely years away, pending the build-out of Midtown, if it ever occurs at all.

Long-term parking of derelict vehicles is understood to be an issue in semi-urbanized, unincorporated areas in many counties. Such activity is especially prevalent in low-density and industrial areas, where such activity is unlikely to be easily noticed or the subject of active complaints. Due to the limited nature of data collection, restricted to one weekday and one weekend, data is insufficient to quantify multiday and long-term storage of vehicles occurring on-street.



Typical on-street parking in the Perl Mack neighborhood on a weekday afternoon

If Adams County procures LPR technology for parking enforcement purposes in the neighborhoods outlined above, technology and staff resources could be extended into conducting enforcement of code or updated ordinance language that prohibits long-term storage of vehicles in the public right-of-way in all 15 neighborhoods. For instance, if the county wished to enforce a 30-day limit for on-street parking, the parking enforcement officer (PEO) or parking ambassador (PA) could conduct a full passthrough of all streets in each neighborhood once a month. License plates that occur with the same GPS coordinates across many months could have the corresponding vehicle context photos and other metadata reviewed by PEOs and flagged as potential long-term parking ordinance violators. Further investigation would be necessary in order to further differentiate between habitual parkers and truly derelict vehicles, such as comparing wheel placement and tire pressure valve stem location. This evidence would suggest whether a vehicle has been moved at all within the month and put on a hotlist of vehicles earmarked for further investigation.

In relation to other forms of illegal parking, such as parking in restricted zones, targeted patrolling is an option in neighborhoods with observed patterns of illegal parking activity, such as Goat Hill and Sherrelwood. Targeted patrols could be conducted by either the PEO/PA (with or without LPR technology) or by Sheriff's deputies. Such enforcement activity would not need to be conducted frequently or regularly to have a meaningful degree of efficacy. Note that the PEO/PA would have more limited authority to enforce unpaid citations than Sheriff's deputies unless the ordinances were changed accordingly.

Adams County is considering implementation of a street sweeping program for at least some neighborhoods, such as Berkeley and Shaw Heights. With or without active parking enforcement, the implementation of street sweeping would have a positive effect on enforcement of potential regulations prohibiting long-term parking, derelict vehicle parking, or otherwise illegal parking. Because vehicles must be moved for street sweeper operations, street sweeping can be a tangible, physical deterrent for such behavior. However, this benefit would only occur if citations are actually issued and violating vehicles reported for removal by tow, which would require coordination with enforcement staff and PEOs. Also, it would be limited to the warmer months, when sweeping operations are conducted.

In the future, it is likely that continued mixed-use development in the Midtown Neighborhood, as well as new transit-oriented development along or near the B Line, the G Line, and the N Line, will necessitate some sort of parking management and enforcement. This may include time limited parking or paid parking. When such development occurs, and the need for parking management in these areas becomes apparent, LPR technology could be expanded for use in enforcement in these other areas.

BERKELEY

Out of all the neighborhoods, Berkeley was identified by Adams County staff as having the most immediate and significant potential need for parking management, which was confirmed by Walker through data collection and analysis. The Berkeley neighborhood features the clearest example of likely spillover parking demand, where the land use node generating the spillover demand is unrelated to the land uses where demand is spilling over to. In this case, parking demand generated by students and staff of Regis University, located immediately to the south of the neighborhood within the City and County of Denver, is spilling over across 52nd and 53rd Avenues into the adjacent low-density residential areas.

Denver has implemented a restricted parking program within a limited radius of the University to the southeast, south, and southwest. The program limits student access along block faces where on-street parking is allowed.

Only residents are permitted to park along those streets in an unrestricted manner, via Denver’s resident parking permit program. Without a residential parking permit, parking is prohibited between 8 AM and 6 PM on weekdays between September and April.

The unique jurisdictional nature of the area surrounding Regis University, with unincorporated area within a different county immediately to the north, combined with Denver’s restricted on-street parking, funnels students and staff who do not wish to pay the University’s fees for on-campus parking permits in the unmanaged residential neighborhoods of the Berkeley neighborhood. Furthermore, parking along 52nd Avenue, within Adams County, prohibits parking at any time, providing no relief for the spillover demand generated by the University.

In order to address parking concerns in the Berkeley neighborhood, Walker recommends the following options for Adams County to consider:

- Passive parking management with no enforcement
- Active parking management with enforcement provided by County staff
- Active parking management with enforcement provided by contract

In all three options, a special parking zone would be established within a recommended boundary of 52nd Avenue to the south, Osceola Street to the west, 54th Avenue to the north, and Federal Boulevard to the east. The restricted period is recommended between 8 AM and 6 PM on weekdays between the months of September and April, matching Denver’s restrictions in its neighboring zone.

PASSIVE MANAGEMENT

Under a passive parking management scenario, posted sign restrictions would be installed on each block face within the parking zone. Signage would specify that on-street parking is for residents and guests of residents only. There would be limited deterrence anticipated by posting “Resident Parking Only” signage. Under this scenario, costs would be restricted primarily to the cost of acquiring and installing signs, with minimal ongoing maintenance costs. Without also establishing a resident parking permit program, to be able to distinguish between vehicles belonging to residents/guests and those belonging to non-residents, there would be no active enforcement. Adams County Sheriff’s deputies would continue to be responsible for enforcement of regulations prohibiting illegal parking or derelict vehicle parking when reported, if the county wishes to make such enforcement more of a priority without establishing its own parking enforcement team separate from the Sheriff’s Department.

With such a plan, Regis University may agree, in the interest of being a good neighbor, to ask that students refrain from parking on-street along block faces that have been designated for residents only. This information could potentially be included as part of parking informational materials provided to students.

ACTIVE MANAGEMENT WITH COUNTY ENFORCEMENT

In order to execute any degree of active enforcement of “Residents Only” parking restrictions, a resident parking permit program (RPPP) must be considered. Such programs are used by many jurisdictions across Colorado in areas of high spillover parking demand. Most cities that contain universities or colleges, such as Colorado

Springs, Fort Collins, and Boulder, all have established RPPPs in the neighborhoods immediately surrounding their respective campuses.

Costs to establish such a program are now lower than they would have been in previous years due to the fact that programs can be created and maintained electronically. There is no need to print and issue physical placards for residents. In this case, considering the unincorporated context of the Berkeley neighborhood, permits could be issued either free of charge to residents or at minimal fee to cover the cost of the program's administration on behalf of the neighborhood, with no maximum vehicle limit and no limit on guest permits. A database of permit holders and corresponding permitted license plates would be established. If Adams County chose to implement LPR technology, the LPR database and software would be integrated with the list of permit holders and their registered vehicles. With LPR enforcement, the system could be extended to any other areas or neighborhoods that Adams County would wish to implement parking management and enforcement, both now and in the future.

A soft launch of such a program would be recommended, with a 60-day or 90-day grace period following installation of restricted parking signs along corridors that fall within the proposed restriction zone. During this time, the county could work with Regis University in order to create an informational campaign that transparently and explicitly notify Regis students and staff of changes to come for on-street parking north of 52nd, as well as to allow residents within the zone to register their vehicles for permits. For a further 30-day or 60-day period, warnings only could be issued to all user groups for alleged violations within the zone.

Optionally, parking by non-permit holders could be allowed during restricted times for a limited duration of time, such as up to two hours. Residential parking demand typically does not peak until late evening, while University-generated parking demand peaks during the morning and early afternoon. Such a time limit would allow short-term parkers, including commuter students and staff, as well as resident guests, to park during the day while prohibiting all-day vehicle storage by University commuters. It should be noted Denver does not allow non-resident parking at any time within the restricted period.

Under this scenario, Adams County PEOs/PAs should maintain a somewhat regular and active enforcement schedule for RPP zone, especially within the first six months of implementation. If parking is to be limited at all times during the enforcement window to permit holders only, enforcement routes through the neighborhood would only be necessary once or twice per day to achieve the intended affect. With two-hour parking allowed, enforcement would be needed more frequently, at least four times per day, occurring every two hours per day within the enforcement window.

During enforcement, the PEO/PA would also separately notate and cite instances of illegally parked vehicles, with or without resident permits. This would include vehicles parked in areas where parking is prohibited at all times, such as along curbs marked red, near fire hydrants, driveways, etc., as well as vehicles that have been parked on sidewalks that have recently been installed along many Berkeley neighborhood streets. Walker staff observed many instances of sidewalk parking during its site visits.

ACTIVE MANAGEMENT WITH CONTRACT ENFORCEMENT

If the preferred overall management strategy chosen is to maintain unmanaged and unenforced parking in all neighborhoods other than Berkeley, or if the county declines to purchase LPR technology or establish its own enforcement team independent of the Sheriff's Office, another option would be to contract enforcement of an

RPPP to a third party operator. In the context of the Berkeley neighborhood, the most cost-effective and efficient third-party management solution may be to contract enforcement of on-street parking restrictions in Berkeley to Regis University's parking enforcement staff. Under this solution, Adams County would still need to establish and maintain an active database on its own of resident permits and permitted vehicles. However, the database of registered resident license plates could be made available to Regis parking enforcement staff so that Regis's parking enforcement could differentiate permitted vehicles from unpermitted ones. The University has an established and mature parking enforcement program, and the limited size of the zone and relatively small number of on-street parking spaces to be restricted in Adams County would make enforcement north of 52nd Avenue by University staff quite feasible, with modest additional staff resources and additional infrastructure needed, even for frequent enforcement.

The likelihood that Regis administration would agree to such a contract is currently undetermined. Further investigation and study would be needed in order to determine what additional staffing, resources, and technology Regis would need to extend enforcement. Also, other details relating to how Regis's parking technology would integrate with Adams County, and to what extent Regis would have access to permit holders' information, would have to be resolved. However, Walker does estimate that overall active enforcement costs for Berkeley would likely be lower overall under this option than the other two.



Restricted/permit parking signage currently used south of Regis by Denver

PERL MACK

After Berkeley, multiple problematic areas were identified by Adams County staff and confirmed by Walker's study and analysis of current parking behaviors in the Perl Mack Neighborhood. In this case, three problem areas featured spillover parking demand likely generated by three large multi-family housing areas in the neighborhood: The Boulder Crossroads Apartments, the Villas on 76th / Parkside Apartments, and multiple, smaller apartment complexes along Samuel Drive.

Most of the likely spillover parking happened along streets that were generally adjacent to the apartment complexes. However, some spillover parking, particularly along Del Norte Street, occurred along streets within the vicinity of the apartment complexes comprised predominantly of single-family detached homes. Such spillover parking is especially problematic due to parking demands for both nodes generating peak parking demands at the same time with one node's parking demands exceeding its available supply and spilling over into the other node's supply (overnight during weekdays).

Walker recommends that the County explore the option of employing parking management in a limited fashion in the Perl Mack neighborhood's problem areas. Specifically, enforcement could help to address and alleviate frequent illegal parking activity that occurs along key corridors with high peak parking demand, such as 76th Avenue, Dakin Street, and Samuel Drive. If using LPR technology, enforcement routing could take place by the LPR-equipped PEO/PA when time allows; a regular enforcement schedule would not need to be maintained and sporadic enforcement would aid in preventing area scofflaws from identifying any enforcement patterns. Without LPR technology, or if the County elects not to implement managed parking, targeted enforcement could be provided by the Sheriff similar to the option for passive management proposed for the Berkeley neighborhood.

Also, a resident parking permit program could be established, similar to the scheme described above for the Berkeley neighborhood (active management with County enforcement), albeit with restrictions in place year-round, 24 hours a day. Such a program would only apply to areas where parking demand is spilling over into adjacent areas of single-family homes. Walker has identified two areas as candidates to establish permit zones. One is an area bounded by Bronco Road to the west and south, Campo Street to the southeast, Greenwood Boulevard to the east, and Dakin Street to the north (between Hilltop Circle and Greenwood Boulevard.) The other is an area along 76th Avenue, between Zuni Street and Raritan Street, with the zone extending approximately one block north along each street between Zuni Street and Raritan Street. RPPPs in the Perl Mack neighborhood would require an enforcement program to be established and implemented by the County, with or without LPR technology.

Walker recommends that further study be conducted within the proposed zones described above, to further establish extents of spillover parking and if it warrants establishment of a permit program. Note that streets within one block to the north of 76th Avenue between Zuni Street and Raritan Street are within the Fairview neighborhood and were not examined in this study. This is a potential area for expanded study.

With regards to high resident parking demand around the Boulder Crossroads Apartments and along Samuel Drive, Walker recommends that further study be conducted to determine how utilized private, off-street parking lots are during peak times (overnight on a weekday) for the apartment complexes in question. If the lots have significant excess capacity, it would indicate that high on-street demand is due to fees and policies of the apartment complex that apply to its parking, and not necessarily to a lack of supply in the complex. If it is found

that there is excess capacity during peak times, it is recommended that the County work with apartment complex owners to modify and relax their fee structure for residents' vehicles using its own parking. However, if it is found that off-street parking for the complexes is indeed near or at capacity at peak times, shared parking opportunities could be explored.

Specifically, the County might facilitate a dialogue on potential shared parking opportunities between Boulder Crossroads management and the landowner for the Pima Medical Institute office building, located across Dakin Street to the west, as well as between management for the apartment complexes along Samuel Drive and the owners of the Samuel Building, across Samuel Drive to the east. Such a dialogue would need to occur during or after further study was conducted to determine peak parking demand for the Pima Medical Institute facility and Samuel Building and whether a shared parking agreement would be feasible and in the best interest of all parties involved.



Potential locations for off-street shared parking agreements with private businesses in Perl Mack

WELBY

Two potential areas of concern were identified in the Welby neighborhood during weekdays and three during weekends. Frequent and habitual illegal parking activity was observed, particularly along Coronado Parkway and Downing Drive. Block faces with high peak parking demand were also observed along Sheldon Drive and 78th Avenue during Sundays, in addition to along Coronado Parkway and Downing Drive.

Most of the high parking demand happened along streets where the demand was adjacent to the land use(s) generating the high parking demand, and thus likely does not constitute true spillover parking. High parking demand along 78th Avenue occurred during Sundays, directly in front of the Assumption of the Blessed Virgin Mary church complex. This also does not likely constitute true "spillover parking" for the same reason.

Walker recommends that the County employ parking management in a limited fashion for the Welby neighborhood's problem areas, similar to what was recommended for the Perl Mack neighborhood. Specifically, enforcement could help to alleviate frequent illegal parking activity that occurs along key corridors with high peak parking demand. Enforcement routing could take place sporadically by PEO/PA staff if the County wished

to implement a unified management scheme independent of the Sheriff's Department; a regular enforcement schedule would not need to be maintained. Otherwise, the Sheriff would need to enforce on an as-needed basis when problems are reported.

Further verification of existing parking restrictions along Coronado Parkway is recommended to determine the exact nature of illegal parking along that corridor, and whether restrictions are clearly posted and delineated.

FLORADO

In the Florado neighborhood, two block faces that were directly fronting two churches located in the neighborhood featured high parking demand during the morning and early afternoon on Sunday. Specifically, these block faces were along Meadowbrook Drive between Zane Street and Wyandot Street, and Elmwood Lane between Umatilla Street and Florado Street.

Illegal parking was not found to be a significant problem in the Florado neighborhood. Walker recommends that County staff survey homeowners who live in the high demand block faces in question to determine if they perceive parking on Sundays to be an issue. If so, further study should be conducted to determine if peak on-street parking demand on Sundays along the block faces in question is the result of private, off-street parking lots for the two churches being at or near capacity.

If it is not a result of church parking lots filling to capacity, County staff could open a dialogue with church leaders to encourage churchgoers to fully utilize off-street parking before parking on-street. If homeowners indicate that spillover parking is a significant problem on Sundays, and church parking lots are not fully utilized, then the County could optionally install "Residents Only" parking signage along the high-demand block faces in order to further discourage non-resident parking.

Based on this study, Walker does not have other recommendations for management or enforcement specific to the Florado neighborhood at this time.



Churches located in Florado

OVERVIEW OF LPR TECHNOLOGY

LPR systems are data capture and collection platforms that utilize specialized hardware, cameras, and software to quickly and effectively capture the license plate numbers of vehicles. A computer mounted inside a motorized vehicle records license plates, associated metadata, geographic position via GPS, and can dynamically check data in real time to determine vehicle presence and length of stay, if more than one pass is made. Also, cameras will capture both a context shot, showing the vehicle’s surroundings as well as a photo of the license plate itself. Even without Department of Motor Vehicle (DMV) information, the color, make, and model of a vehicle can usually be determined from the context photos captured with the license plate read. As an option, some systems could capture the wheel’s valve stem location as the enforcement vehicle drives past the parked vehicle.

LPR technology could be used for enforcement of both existing and proposed parking restrictions in all neighborhoods. The costs and complexity of LPR technology have decreased within the last decade, making enforcement using this option cost-effective and reducing the amount of labor hours needed. Moreover, the US Court of Appeals for the 6th Circuit ruled in April 2019, in the case of *Taylor vs. City of Saginaw*, that the more traditional low-cost enforcement method of employing a parking enforcement officer to physically chalk tires in the field is unconstitutional. Though as of this writing the decision only applies to US states under the jurisdiction of the 6th Circuit, Walker advises you review your legal use of these method due to the potential that the unconstitutionality of chalking tires could be upheld by the US Supreme Court in the future. Even in cases of targeted enforcement of otherwise infrequent enforcement of specific areas of concern, LPR technology is considered to be a very effective and low-risk parking enforcement solution, particularly in the context of scalability.

Despite the geographically disparate nature of the neighborhoods and areas identified for enforcement, Walker believes that one FTE could effectively cover all areas that were identified as candidates for parking management and enforcement by this study. LPR technology could be outfitted to any existing county-owned passenger vehicle or light-duty truck, as some systems do not require permanent installation or vehicle modification. Many systems employ the use of a magnetically-mounted camera that could easily be attached and detached to the roof of a steel-bodied vehicle.

Note that LPR technology could be used within off-street parking facilities as well as for on-street parking.



Size of a Pelican case that contains portable LPR equipment

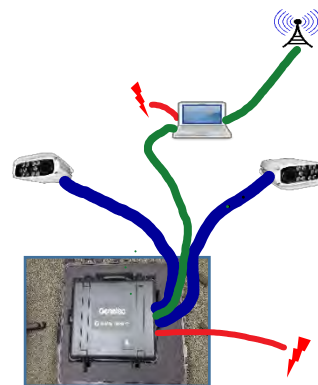
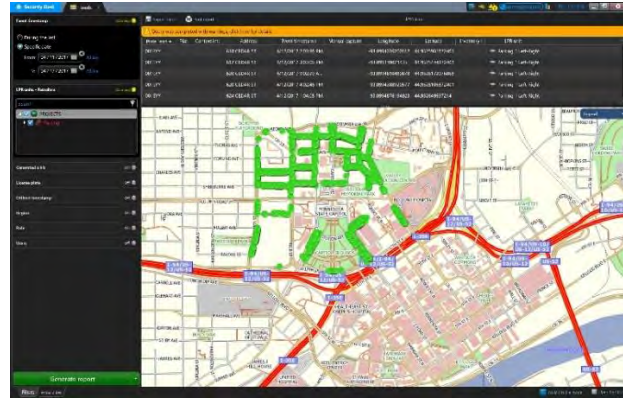


Diagram of how to set up an LPR unit



Inputs for portable LPR computer unit



Screenshot of typical user interface for LPR software

OPERATIONS

We understand the County is considering that the parking enforcement staff who would utilize LPR technology for enforcement be employed under the umbrella of the Department of Community Safety and Well-Being.

The enforcement officer would drive an LPR-equipped vehicle down corridors identified for parking enforcement, following a fixed, pre-defined route. If the corridors or areas have been included under a permit parking program, license plates captured using specialized software would then be compared to a dynamic database, based in the cloud, of vehicles permitted to park along a corridor. If there is time-limited parking for non-permit-holders allowed (e.g., vehicles could be parked for under two hours without a permit once per day), the database of parkers would be updated in real time during multiple passes per day along the fixed route.

During these passes, the system would track which license plates belong to vehicles that have been parked for either two hours or less and which have been parked for more than two hours, according to the above example given. Only vehicles whose license plates have been tracked for more than the time limit would be considered as potential/alleged violators. In most LPR systems, data is hosted and stored in the cloud, allowing any credentialed end user with permission and the applicable software to see, download, and analyze data.

For corridors where LPR technology would merely be used for enforcement along corridors where no parking is allowed at any time, the LPR operator would simply activate and deactivate the camera while passing by restricted segments. While activated, any license plate numbers recorded would be uploaded to a database containing likely illegally parked vehicles.

In all instances of recorded vehicle license plates, parking enforcement staff would be tasked with review of the license plate numbers of vehicles allegedly in violation that have been sent and are stored in the LPR platform's database. If a violation has been confirmed by analyzing context photos and metadata, either a warning or citation could be generated. The warning or citation could either be in the form of a ticket/notice placed under the windshield wiper or auto-generated at a central location and sent via US Mail. In-person issuance of a citation, once a violation has been confirmed, is the recommended method of delivery to maintain the citation's match to the vehicle rather than the individual, as required in many jurisdictions. Additionally, education of legal parking behaviors is diminished when a citation is received days or often weeks after the violation when the vehicle owner, who may not have been the driver, does not recall the situation.

A violation hierarchy and citation fine structure would need to be created, with potentially two different structures. There might be one hierarchy for illegally parked vehicles and one for vehicles in violation of established permit and/or time limited restrictions. The fine for each citation would depend on how many previous warnings/violations have been linked to a particular vehicle as well as whether established fines associated with a vehicle that has been cited multiple times have gone unpaid.

Given the currently unorganized, unmanaged nature of on-street parking within the unincorporated areas, Walker recommends that, for violation of time restrictions or unpermitted parking in permit zones, very nominal fines be implemented, always following an initial warning. For illegally parked vehicles, the fine structure should be stricter, and PEO/PAs would have the option of contacting Sheriff's deputies to follow up for particularly egregious violations or violations where safety is at risk.



A personal passenger vehicle outfitted with detachable LPR equipment



A PEO/PA attaching LPR cameras to a car roof with magnets

COSTS

The cost of LPR equipment depends on the vendor and on the specific package selected. For Adams County's purposes, the initial, up-front capital costs associated with procurement of a portable LPR system, not including a laptop computer and ongoing cellular/WiFi costs, would range between approximately \$20k and \$50k in 2019. This includes the cost of the proprietary hardware as well as the initial software purchase and back-end costs

associated with building a database; this figure does not include the price of a vehicle. Typically, ongoing annual maintenance costs for mobile LPR technology are about 10% of the initial purchase price. In this case, ongoing costs would be approximately \$2,000 to \$3,000 per year, including ongoing technical support and fees for cloud-based services.

Because no paid parking is being recommended at this time, and due to the limited quantity of potentially managed spaces, ongoing labor needs for enforcement across neighborhoods would be minimal. LPR duties could easily be integrated into the duties of existing Community Well-Being and Safety staff who perform work in the field. Depending on the preferred management strategies for the neighborhoods discussed below, Walker believes that reasonable enforcement coverage could be achieved with labor costs equivalent to a range of between .25 and 1.0 FTE's. Assuming a base hourly wage of \$15.00 per hour, with an additional approximately \$2.58 per hour in the form of part-time benefits, annual labor costs would thus range from approximately \$9,141 to \$36,566.

OTHER BENEFITS

During a kick-off meeting with Adams County staff, Walker learned that the Sheriff's Department currently does not use LPR technology for law enforcement purposes. Therefore, the procurement of an LPR system by the County could not only be useful for parking management purposes, but also be extremely useful for Sheriff's deputies in conducting their day-to-day operations and police investigations.

Sheriffs' departments typically make use of grant funding programs or county general funds in order to fund procurement of new equipment and vehicles, with budgeting taking place several years out. This makes LPR technology more difficult to obtain than it would be for a city police department. If LPR equipment can be used to generate a steady stream of revenue, however, it can make LPR technology politically and financially palatable. Active parking management using LPR achieves revenue generation. Also, in some jurisdictions, particularly rural ones, it can be politically difficult to allow law enforcement to use LPR equipment to generate and utilize data containing license plate numbers for its own use. However, if a non-law-enforcement department is the steward and operator of LPR equipment, with only limited enforcement ability that comes in the form of issuing parking citations, it can oftentimes circumvent political challenges and perceived privacy issues associated with use of LPR by law enforcement.

From the perspective of the Department of Community Well-Being and Safety, giving the Sheriff access to an LPR system and its data would also be beneficial. By law, only law enforcement agencies have access to certain databases, such as DMV records or stolen vehicle hotlists. When LPR data is shared with law enforcement, the sheriff can be notified and deployed to conduct enforcement and citation for serious violations outside the purview of parking enforcement. For instance, LPR data can be shared with the sheriff, who can cross-reference DMV records to identify vehicles with expired registration tags parked on-street. Sheriff Department data is therefore useful in enforcement of ordinances prohibiting long term vehicle storage on-street.

Modern LPR software platforms allow for seamless sharing and integration of data for analytics purposes, and many routines and the generation of hotlists can be automated on the back end. Some LPR vendors will set up data sharing accounts for other agencies at little to no cost once a system is deployed. By law, data generated by the sheriff cannot be shared with non-law-enforcement agencies, though there is an exception if parking enforcement is conducted directly by the law enforcement agency. However, this is an uncommon situation in

most jurisdictions, as PEOs/PAs have to undergo additional training and certification to allow them to act as officers of the law or be employed with a law enforcement agency.

The Adams County Sheriff currently employs a Brazos Technology platform in issuing tickets for moving violations. Any LPR solution chosen should be able to integrate and share data with the Sheriff's existing ticketing platform. In areas where less active, targeted patrols of illegal parking activity is preferred (e.g., in the neighborhoods not identified in this report as having areas of concern with regard to parking), Walker recommends that such enforcement continue to be conducted by Adams County Sheriff's deputies on an ad hoc/as needed basis. Parking lots, private or public, that may be identified as having issues with long-term/derelict parking could also be spot checked for such activity, if desired, with input and assistance from PEOs.



STUDY SESSION AGENDA ITEM

DATE:	March 10, 2020
SUBJECT:	Oil and Gas Update
FROM:	Jill Jennings Golich, Director Katie Keefe, Environmental Program Manager Greg Dean, Oil and Gas Liaison
AGENCY/DEPARTMENT:	Community & Economic Development
ATTENDEES:	Jill Jennings Golich, Ryan Nalty, Katie Keefe, Greg Dean, Keith Huck, Chane Finkenbinder
PURPOSE OF ITEM:	To provide the Commissioners with new, emerging and changing information pertaining to oil and gas activities within the County.
STAFF RECOMMENDATION:	Update and Direction

BACKGROUND:

Staff will provide the Board with a status update on new flowline data within unincorporated Adams County as a result of state rulemakings. Staff will additionally brief the Board on the status of proposed regulation rulemakings at the COGCC. Lastly, Staff will briefly update the Board on the status of oil and gas permits under evaluation with the COGCC and other emerging concerns pertaining to oil and gas activities in the County.

AGENCIES, DEPARTMENTS OR OTHER OFFICES INVOLVED:

ATTACHED DOCUMENTS:

Flowline Data Update
Flowline Viewing Request Form
Mineral Rights Info Sheet

FISCAL IMPACT:

Please check if there is no fiscal impact . If there is fiscal impact, please fully complete the section below.

Fund:

Cost Center:

	Object Account	Subledger	Amount
Current Budgeted Revenue:			
Additional Revenue not included in Current Budget:			
Total Revenues:			

	Object Account	Subledger	Amount
Current Budgeted Operating Expenditure:			
Add'l Operating Expenditure not included in Current Budget:			
Current Budgeted Capital Expenditure:			
Add'l Capital Expenditure not included in Current Budget:			
Total Expenditures:			

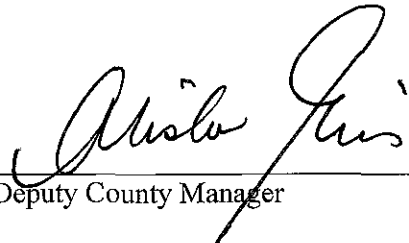
New FTEs requested: YES NO

Future Amendment Needed: YES NO

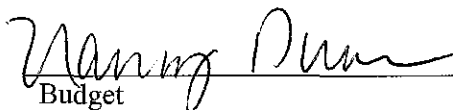
Additional Note:

APPROVAL SIGNATURES:

Raymond H. Gonzales, County Manager


Deputy County Manager

APPROVAL OF FISCAL IMPACT:


Budget

Flowline Mapping & Information

Scale: 1 : 7500

Removed flowline

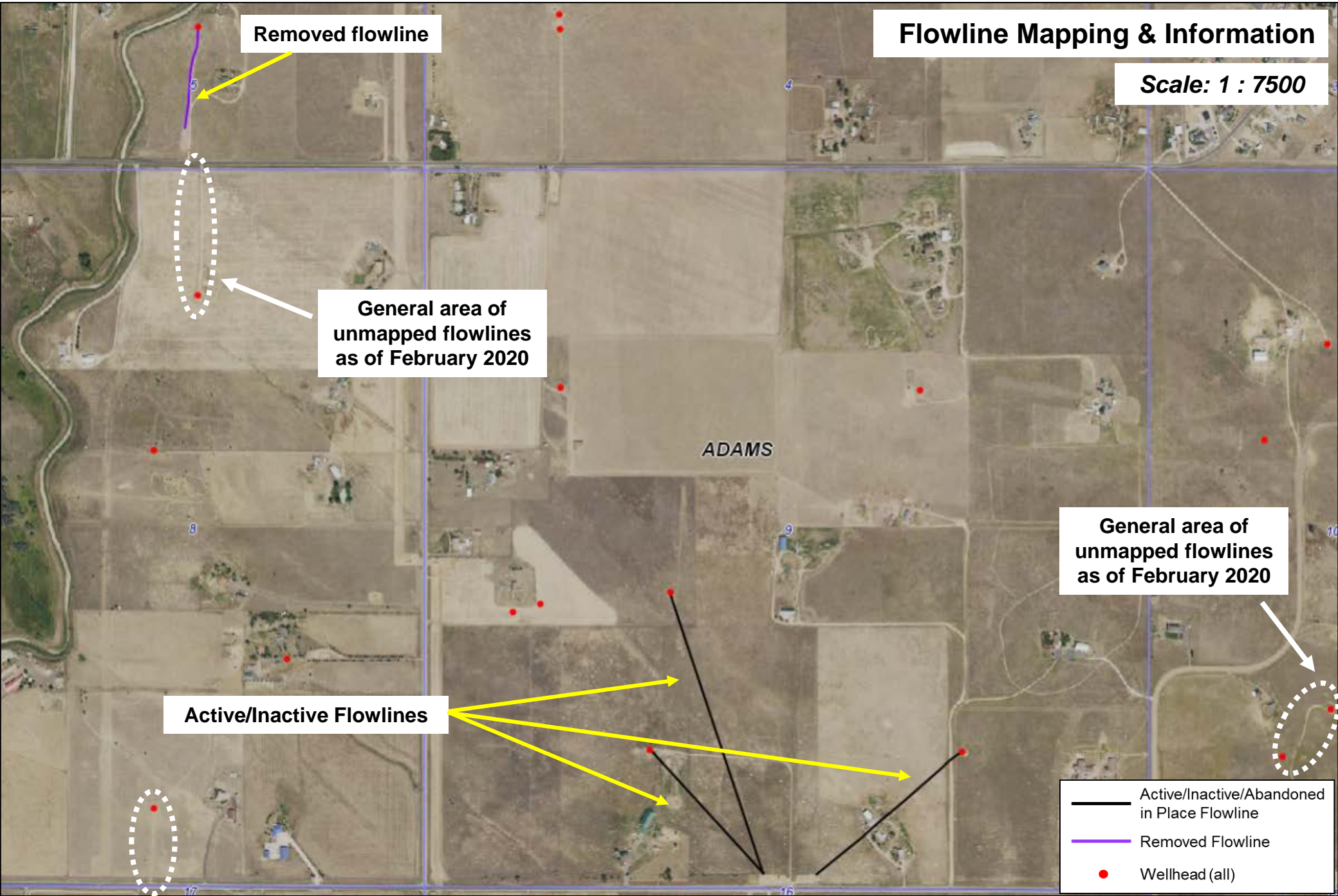
General area of unmapped flowlines as of February 2020

General area of unmapped flowlines as of February 2020

Active/Inactive Flowlines

ADAMS

- Active/Inactive/Abandoned in Place Flowline
- Removed Flowline
- Wellhead (all)



Flowline Abandonment Notices

Scale: 1 : 6000

Well: HSR-Wenzel 6-29
Requesting Abandonment in Place
per exception to COGCC R.1105.d

29

28

ADAMS

32

33

Well: HSR-Van Schaack 4-33
Requesting Abandonment in Place
per exception to COGCC R.1105.d

- Active/Inactive/Abandoned in Place Flowline
- Removed Flowline
- Wellhead (all)



Flowline Viewing Request Form

Date: _____

Name(s): _____

Contact Information: (phone and/or email address)

Requestor's Address: (Street/City/County)

Requested Flowline Viewing Address/Parcel Number(s):

Detailed flowline information is only available for locations within unincorporated Adams County. For other municipalities and/or counties please contact the appropriate local government or the Colorado Oil and Gas Conservation Commission (COGCC). Please keep requests to a specific location, not exceeding a three square miles radius.

Preferred Days/Times of Day:

Adams County Government Center is open Monday-Friday, 8:00 AM – 4:30 PM. The County aims to schedule all flowline viewings within 10 business days from the time the request is received.

Please Note:

- Flowline viewing appointments are for a maximum of one (1) hour. If your request takes longer than one hour, another viewing appointment must be scheduled for a different date/time.
- All flowline viewing appointments are conducted at the Adams County Government Center located at 4430 S. Adams County Parkway, Brighton, CO 80601. Please check-in at the One-Stop Permit Desk located on the 1st floor.
- No copies/facsimiles, photographs or printouts of detailed flowline information will be allowed. This appointment is solely for in-person viewing of flowline data at scales less than 1:6000. Maps at scales greater than 1:6000 are always available online through the COGCC website (http://cogcc.state.co.us/maps.html#/gis_flowlines).
- Flowline viewing appointments are for informational purposes only and DO NOT replace the need to call Colorado 8-1-1 before any ground disturbing or excavation activities commence. This information is provided as a public service and not intended to be used as an authoritative public record.
- Available flowline information has been vetted and approved by COGCC staff and does not necessarily represent all the flowlines that are currently in the ground. Operators have until December 1, 2020 to submit all GIS data to the COGCC for all flowlines in existence and not yet abandoned prior to May 1, 2018.

Please submit requests to Greg Dean, Oil & Gas Liaison at gdean@adcogov.org

◇◇◇◇◇◇◇◇◇◇ LEARN ABOUT YOUR ◇◇◇◇◇◇◇◇◇◇

MINERAL RIGHTS

DO YOU OWN THE MINERAL RIGHTS FOR YOUR PROPERTY?

That is a complex question in Colorado because there is a difference between ownership of the surface rights of the property and ownership of the minerals — oil and gas — beneath the property. You may own the surface rights of a property, but someone else may own the minerals beneath it. The process for determining who owns the mineral rights for a particular property is difficult and often time-consuming. You should consult an attorney or land use expert if you have questions about your mineral rights.



The State of Colorado through the Colorado Oil & Gas Conservation Commission (COGCC), not individual counties, handles all mineral rights, spacing, and pooling cases. Please contact the COGCC or visit their website for more information.

COLORADO OIL & GAS CONSERVATION COMMISSION

1120 Lincoln St., Suite 801 | Denver, CO 80203

303.894.2100 | cogcc.state.co.us

STATEWIDE COMPLAINT LINE: 888.235.1101

ADAMS COUNTY OIL & GAS LIAISON

Community & Economic Development Department

4430 S. Adams County Pkwy. | Brighton, CO 80601

720.523.6891



ADAMS COUNTY
COLORADO

LEARN ABOUT YOUR MINERAL RIGHTS



EXAMPLE 1

If you own the surface rights but not the mineral rights, you are not entitled to any royalties. However, you may submit public comments on proposals with Adams County about surface impacts and the COGCC about surface and subsurface impacts. You may also request a hearing with the State of Colorado about applications, pooling, and spacing.



EXAMPLE 2

If you own the mineral rights and do not share ownership with anyone else, you can choose to sign and negotiate private lease agreements with operators.



EXAMPLE 3

If you own your mineral rights and an oil and gas operator applies to develop a large area spreading over several parcels, including yours, you can negotiate and sign a private contract with the operator to pool your minerals into a spacing unit that also includes other mineral rights owners.

This is a more efficient way to drill with less surface impact while allowing all mineral owners to benefit from the proceeds. If some owners can't or won't work out agreements, the operator has the right to request "force pooling" from the COGCC, which ensures the owners receive reasonable compensation for their minerals. An operator may force pool if they own or lease at least 45% of the mineral interests in the proposed spacing unit.

WHAT IS "FORCED POOLING?"

Under Colorado state law, an oil and gas operator can apply to develop the minerals under several parcels and request to lease the mineral rights through a pooling application sent to all other owners. Owners have these options:

- ✓ Lease your minerals to an operator through a private agreement that entitles you to reasonable royalties from oil and gas production.
- ✓ Participate in the drilling, completion, and operation of the wells. If you choose this option, you share the costs of the drilling with the operator, but you will receive a greater share of the royalties.
- ✓ Take no action. You will be considered a "non-consenting party" through COGCC rules, and your minerals will be force pooled. You will still receive your share of the proceeds after the operator recoups its cost for drilling and completions. Non-consenting owners receive 13% royalty for gas wells and 16% for oil wells. You do have the right to protest the application directly with the COGCC.